

DATE



PIN 8780.49

KINGSTON CONNECTIVITY

BROADWAY STREETSCAPE PROJECT

CITY OF KINGSTON

AUGUST 2019

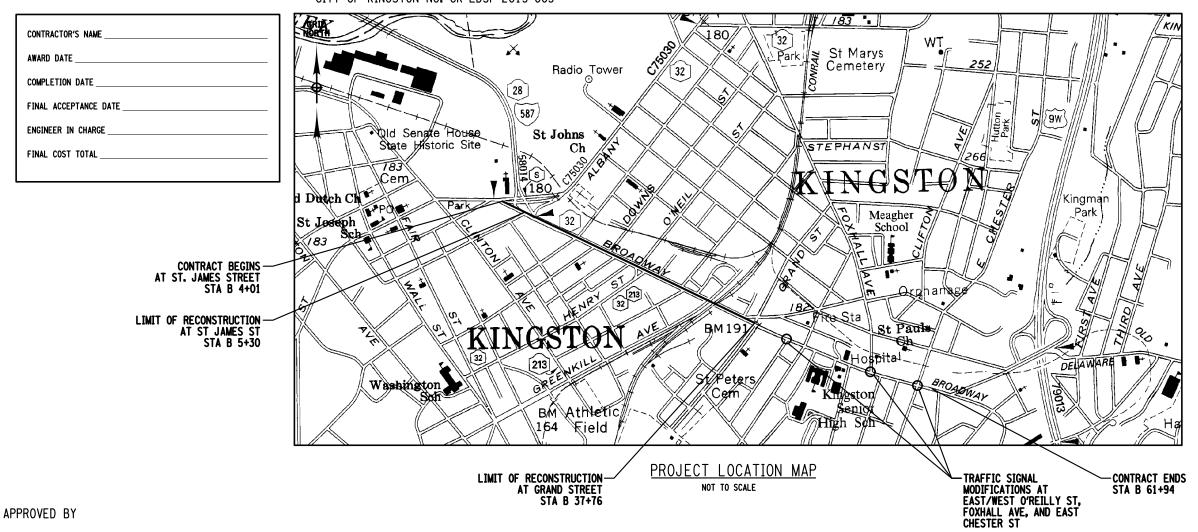
THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE DEPARTMENT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN THE CONTRACT PROJECT "PROPOSAL"

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYSDOT POLICIES AND GUIDELINES AND THE FINAL DESIGN REPORT APPROVED ON 12/14/2018

DL NO. 034880 GIGP NO. 1378 CITY OF KINGSTON NO. CK-EDSP-2015-003

ULSTER COUNTY



CHRISTOPHER C. CORNWELL, P.E. VICE PRESIDENT
GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS 80 WOLF ROAD, SUITE 300 ALBANY, NY 12205

PREPARED AND RECOMMENDED BY:

<u></u>	KINGSTON CONNE	CTIVITY	
_	BROADWAY STREE	TSCAPE PROJEC	Т
S	CITY OF KINGSTO	ON	
	ULSTER COUNTY		
	FED. ROAD REG. NO.	STATE	SHEET NO.
	8	N.Y.	1
	CAPITAL PROJECT IDENTIFICATION NO. 878	0.49	

INDEX ON SHEET NO. 2

IGN SUPERVISOR C. CORNWELL

STEVEN T. NOBLE MAYOR, CITY OF KINGSTON

TC

VCP

TOP OF CURB TOP OF GRATE

VITRIFIED CLAY PIPE

BB BOTTOM OF BANK (STREAM) BOTTOM OF CURB BO BOTTOM OF OPENING CAP CORRUGATED ALUMINUM PIPE СВ CATCH BASIN CAST IRON PIPE € STRM CENTERLINE OF STREAM CMP CORRUGATED METAL PIPE CP CONCRETE PIPE CSP CORRUGATED STEEL PIPE CULV CULVERT DIA DIAMETER DMH DRAINAGE MANHOLE DRAINAGE STRUCTURE PIPE DS D'XING DITCH CROSSING EHW EXTREME HIGH WATER EL ELEVATION ELEV ELEVATION ELW EXTREME LOW WATER ES END SECTION HW HEADWALL INV INVERT MH MANHOLE MEAN HIGH WATER MHW ORDINARY HIGH WATER OHW ORDINARY LOW WATER REINFORCED CONCRETE PIPE RCP SICPP SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE TB TOP OF BANK (STREAM)

ALIGNMENT

SUPERELEVATION RATE (CROSS SLOPE)

CENTER CORRECTION OF VERTICAL CURVE

DESCRIPTION

AHFAD

BACK BASELINE

AZIMUTH

BEARING

CENTERLINE

EQUALITY

MAIN LINE

RADIUS

STATION

POINT ON LINE

CURVE TO SPIRAL

HORIZONTAL CONTROL LINE

HEADLIGHT SIGHT DISTANCE

LENGTH OF SPIRAL

POINT OF CURVATURE

POINT OF TANGENT

SPIRAL TO CURVE

TANGENT LENGTH

VERTICAL CURVE

DESCRIPTION

TANGENT TO SPIRAL

SPIRAL TO TANGENT

POINT OF INTERSECTION

PASSING SIGHT DISTANCE

POINT OF VERTICAL CURVE

STOPPING SIGHT DISTANCE

THEORETICAL GRADE LINE

TOPOGRAPHY (DRAINAGE)

POINT OF VERTICAL TANGENT

POINT OF VERTICAL INTERSECTION

LENGTH OF CIRCULAR CURVE

LENGTH OF VERTICAL CURVE

ABBR.

ΔН

ΑZ

BK

BRG

CS

EQ

EXT

HCL

HSD

LS

PC

ΡI

P0L

PSD

PT

PVC

PVI

PVT

R

SSD

ST

STA

TGL

TS

ABBR.

VC

LVC

STANDARD SHEETS STANDARD SHEETS
203-01, 209-03, 402-01, 502-01, 502-02, 502-03, 502-04, 502-05, 502-06, 502-07, 502-08, 502-09, 502-10, 502-11, 502-12, 502-13, 502-14, 502-15, 603-01, 604-02, 605-01, 608-01, 608-02, 608-03, 608-04, 608-05, 608-06, 609-01, 609-02, 609-03, 611-01, 619-02, 619-10, 619-11, 619-12, 619-20, 619-21, 619-50, 619-51, 619-60, 619-61, 625-01, 645-01, 645-02, 645-03, 645-05, 645-06, 645-07, 645-03, 645-10, 645-11, 645-12, 645-14, 646-12, 646-13, 646-16, 649-01, 655-01, 655-02, 655-03, 655-04, 655-05, 655-06, 655-07, 655-08, 663-07, 663-02, 663-03, 663-04, 663-05, 663-06, 663-07, 664-01, 670-01, 670-02, 670-03, 680-01, 680-02, 664-01, 670-01, 670-02, 670-03, 680-01, 680-02, 680-03, 680-04, 680-05, 680-06, 680-07, 680-08, 680-11, 680-12, 680-13, 680-14, 680-15, 680-16, 680-17, 685-01

TOPOGRAPHY (MISCELLANEOUS)

ABBR.

ABUT

AOBE

ВМ

DESCRIPTION

AS ORDERED BY ENGINEER

ABUTMENT

BENCH MARK

CC CENTER TO CENTER

CMU CONCRETE MASONRY UNIT

DM DIRECT MEASUREMENT

EP EDGE OF PAVEMENT

ES EDGE OF SHOULDER

FEE WO/A FEE ACQUISITION WITHOUT ACCESS

IRON PIN OR IRON PIPE

NAIL AND WASHER

PERMANENT EASEMENT

ORIGINAL GROUND

FEE | FEE ACQUISITION

FENCE LINE

FP FENCE POST

FD FOUNDATION

GARAGE

GRAVEL

HIGHWAY

MAILBOX

MONUMENT

PAVEMENT

P PROPERTY LINE

PED POLE PEDESTRIAN POLE

PORCH

ROW RIGHT OF WAY

SH STATE HIGHWAY

RETAINING WALL

TE TEMPORARY EASEMENT

TO TEMPORARY OCCUPANCY

RR RAILROAD

RTE ROUTE

SHLDR SHOULDER

ST STREET

STK STAKE

STY STORY

SW SIDEWALK

U/G UNDERGROUND

WW WING WALL

SPK SPIKE

O/H OVERHEAD

P PARCEL

HOUSE

FL

GAR

GR

HWY

IP I

мв

MON

N&W

PAV'T

PE I

POR I

RW

OG |

ASPH ASPHALT

BDY BOUNDARY

BLDG BUILDING

CONC CONCRETE

CONST | CONSTRUCTION

DWY DRIVEWAY

CR COUNTY ROAD

D DEED DISTANCE

_						
	STANDARD Symbol (Plans)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)			
1	II .	-	INCHES			
	,	LF	LINEAR FEET			
	mi	MI	MILES			
	f†²	SF	SQUARE FEET			
	YD ²	SY	SQUARE YARD			
	AC	AC	ACRES			
	YD3	CY	CUBIC YARD			
	GAL	GAL	GALLON			
	lb	LB	POUND			
1	TON	TON	TON			

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

BROADWAY STREETSCAPE PROJECT

PIN 8780.49

RTE 32

REGION: 8

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

INDEX & ABBREVIATIONS

GREENMAN-PEDERSEN, INC

CONSULTING ENGINEERS

CONTRACT NUMBER

DL34880

CITY OF KINGSTON

DRAWING NO. IND-1 SHEET NO. 2

STANDARD Symbol (Plans)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
н	-	INCHES
•	LF	LINEAR FEET
mi	MI	MILES
f†²	SF	SQUARE FEET
YD ²	SY	SQUARE YARD
AC	AC	ACRES
YD ³	CY	CUBIC YARD
GAL	GAL	GALLON
Ь	LB	POUND
TON	TON	TON

TO BE USED IF ONE OF THE ABOVE CANNOT BE DEFINED AT THE TIME THE EXPLORATION IS MADE

UTILITIES

DESCRIPTION

EMH | ELECTRIC MANHOLE

GSB GAS SERVICE BOX (HOUSE LINE)

GV GAS VALVE (MAIN LINE)

LPG LOW PRESSURE GAS

E ELECTRIC

GP GUY POLE

HYD HYDRANT

LP LIGHT POLE

PP POWER POLE

SA SANITARY SEWER

TEL P | TELEPHONE POLE

W WATER

ABBR.

CP

FH

SMH SANITARY MANHOLE ST STORM SEWER
T TELEPHONE

TCB TRAFFIC CONTROL BOX TELBOX TELEPHONE BOX

TMH | TELEPHONE MANHOLE

WSB WATER SERVICE BOX (HOUSE LINE)

REPLACE ABBREVIATION "AB" WITH:

SUBSURFACE EXPLORATION

1 INCH SAMPLER (RETRACTABLE PLUG)

TO BE DEFINED AT THE TIME OF EXPLORATION

WV WATER VALVE (MAIN LINE)

CONE PENTROMETER

DA 21/4 INCHES CASED DRILL HOLE

DN 4 INCHES CASED DRILL HOLE

HOLLOW FLIGHT AUGER

PT PERCOLATION TEST HOLE

ABBREVIATION "C" IN CATEGORIES: DA, DM, DN, AND FH WITH:

DESCRIPTION

AH HAND AUGER

DM DRILLING MUD

PA POWER AUGER

SP SEISMIC POINT

TP TEST PIT

B BRIDGE

C CUT

F FILL

W WALL

K CULVERT

D DAM

PH PROBE

CTV CABLE TELEVISION

G GAS

ABBR.

DRAWING INDEX							
DESCRIPTION	DWG	SHEET	SHEET				
		QUANTITY	NO				
TITLE SHEET	CVR	1	1				
DRAWING INDEX	IND	1	2				
LEGEND SHEETS	LEG	2	3				
TYPICAL SECTIONS	TYP	2	5				
GENERAL NOTES	GNN	1	7				
WZTC NOTES	MPN	2	8				
WZTC PLANS	MPP	2	10				
BASELINE TIES	BLT	3	12				
MAINTENANCE JURISDICTION PLANS	MJP	2	15				
MISCELLANEOUS TABLES	MST	10	17				
MISCELLANEOUS DETAILS - GENERAL	MSD	5	27				
MISCELLANEOUS DETAILS - DRAINAGE	MSD	1	32				
MISCELLANEOUS DETAILS - BIO-SWALE	MSD	4	33				
MISCELLANEOUS DETAILS - PLANTINGS	MSD	3	37				
MISCELLANEOUS DETAILS - INTERPRETIVE SIGNS	MSD	1	40				
MISCELLANEOUS DETAILS - VAULTS	MSD	6	41				
GENERAL PLANS	GNP	7	47				
CURBING PLANS	CLP	6	54				
SIGN DATA SHEETS	SDS	2	60				
SIGNAGE PLAN	SLP	7	62				
TRAFFIC SIGNAL NOTES	TSN	1	69				
TRAFFIC SIGNAL DETAILS	TSD	4	70				
TRAFFIC SIGNAL PLANS	TSP	4	74				
LIGHTING DETAILS	UTD	2	78				
LIGHTING PLANS	UTP	7	80				
PAVEMENT MARKING PLANS	PMP	7	87				
DRAINAGE PLANS	DRP	7	94				
ESTIMATE OF QUANTITIES	EOQ	4	101				
	то	TAL SHEETS:	104				

ALIGNMENT LANDSCAPE ROADWAY TRAFFIC WORK ZONE STYLE NAME STYLE NAME DESCRIPTION STYLE NAME DESCRIPTION BARRIER, TEMPORARY DESCRIPTION BARRIER, TEMPORARY, W/ WARNING TW7BTWI CONTROL (CENTERLINE) LABL AREA, BRUSH LINE CZ RCZ_P CLEAR ZONE ~~~~~ TWZCD_P CHANNELIZING DEVICE AD_P LAHR AREA, HEDGE ROW -0-GUIDE RAIL, MISCELLANEOUS DETOUR PAVEMENT MARKING REMOVAL OR TWZPMRC_P ,,,,,,,,,, AT P TRANSITION CONTROL I APR AREA, PLANTING BED ___ __ RGB GUIDE RAIL, BOX BEAM **UTILITIES BRIDGE** LAWA AREA, WOODED AREA OUTLINE RGBM GUIDE RAIL, BOX BEAM, MEDIAN ___ ——— ___ LAWE AREA, WATERS EDGE RGC GUIDE RAIL, CABLE STYLE NAME DESCRIPTION RAIL -[0]-UC CONDUIT, UNDERGROUND **BSHT** SHEET PILING LCUT_P CUT LIMIT RGCB GUIDE RAIL, CONCRETE BARRIER -]c[CONDUIT, HANGING RGP_F CONTROL LFILL_F FILL LIMIT 0 GUIDE POST 0 OC UC0 CONDUIT, OVERHEAD GUIDE RAIL, W BEAM **-**⊠-RGW BASELINE LFNC **FENCE** UE ELECTRIC LINE, UNDERGROUND *********** LTRC TREE ROW, CONIFEROUS **RGWM** GUIDE RAIL, W BEAM, MEDIAN CBPR BASELINE, PROJECTION M UEH ELECTRIC LINE, HANGING LTRD TREE ROW, DECIDUOUS PARKING BUMPER DRAINAGE OF UE0 ELECTRIC LINE, OVERHEAD RRC LWH WALL, H PILE RAIL ROAD, CATENARY -ST-DCP CULVERT PIPE UETO ELECTRIC TRANSMISSION, OVERHEAD -3RRRER RAIL ROAD, 3RD RAIL WALL, RETAINING DCP_F CULVERT PIPE (DIR) $\times \times \times \times \times$ UESS **ELECTRIC. SUBSTATIONS** LWS WALL, STONE RRPLS_P RAIL, PHOTO, LARGE SCALE - F0 FIBER OPTIC, UNDERGROUND DDG_P DITCH, GRASS LINED **ROW MAPPING** –]F0[-FIBER OPTIC, HANGING RRPSS RAIL, PHOTO, SMALL SCALE DDP_P DITCH, PAVED INVERT DEED LINE UF00 FIBER OPTIC, OVERHEAD - OF O -RRS RUMBLE STRIP PΕ EASEMENT, EXISTING HG GAS, UNDERGROUND DDS_P DITCH, STONE LINED RRSLS_P RAIL, SURVEY, LARGE SCALE MEP_P EASEMENT, PERMANENT -]G[GAS, HANGING DFL_F FLOW LINE RRSSS EASEMENT, PERMANENT, APPROX. RAIL, SURVEY, SMALL SCALE APE UGO GAS, OVERHEAD OG DSSD SLOTTED DRAIN MET_P EASEMENT, TEMPORARY SIGNS · IC -UIC INFORM CABLE, UNDERGROUND DUD_P UNDERDRAIN - ATE -META_F EASEMENT. TEMPORARY, APPROX. **SBLB BILLBOARDS** -]*IC*[-INFORM CABLE, HANGING **ENVIRONMENTAL** MULTIPLE POST FEE ACQUISITION, W/ ACCESS FEE U0 OIL LINE, UNDERGROUND S **EBLHS** BALE, STRAW SS0 STRUCTURE, OVERHEAD AFEE MFA_P FEE ACQUISITION, APPROXIMATE ====0]0[UOH OIL LINE, HANGING CURTAIN, TURBIDITY MFS_P FEE ACQUISITION, SHAPE SSOC STRUCTURE, OVHD, CANTILEVER POLE, BRACE, PUSH BRACE 000000 **EDMC** DAM, COFFER MEWOA FEE ACQUISITION, W/O ACCESS **STRIPING** -FEE W/OA **>**------LIPGW POLE, GUY WIRE EDMEC_P DAM, EARTHEN CHECK MHA HISTORICAL, ACQUISITION STB* BROKEN LINE USA SANITARY SEWER, UNDERGROUND - SA MHB STDB* DOUBLE BROKEN LINE HIGHWAY BOUNDARY HR – ISA[-USAH SANITARY SEWER, HANGING EDMGSC_P DAM, GRAVEL BAG/SAND BAG CHECK STDL* MHBA HIGHWAY BOUNDARY, APPROX. - AHB -DOTTED LINE LONG - SAF-USAF SANITARY SEWER, FORCE MAIN, UGND EDMPC_P DAM, PREFABRICATED CHECK MHBW HWY BOUNDARY, FACE OF WALL STDS* DOTTED LINE SHORT –]S*AF*[– SANITARY SEWER, FORCE MAIN, HANG FULL BARRIER LINE MHBWOA HIGHWAY BOUNDARY, W/O ACCESS STFB* HR W/OA UT TELEPHONE. UNDERGROUND EDMSC_P DAM, STONE CHECK MJC JURISDICTION, CITY STH* HATCH LINE]7[-UTH TELEPHONE, HANGING MJCY JURISDICTION, COUNTY PARTIAL BARRIER LINE UT0 TELEPHONE, OVERHEAD **EFNSV** FENCE, SILT & VEGETATION MJHD JURISDICTION, HISTORIC DISTRICT STRCT ROUNDABOUT, CAT TRACKS IITV CABLE TV, UNDERGROUND - CTV **EFNV** FENCE, VEGETATION ************ MJLL JURIS., (GREAT, MILITARY) LOT LINE STRYL ROUNDABOUT, YIELD LINE –]CTV[− UTVH CABLE TV, HANGING AA EWAA_F WETLAND, ADJACENT AREA MJN JURISDICTION, NATION STSB STOP BAR UTVO CABLE TV, OVERHEAD -OCTV-WETLAND, FEDERAL **MJPB** JURISDICTION, PUBLIC LANDS STSE* SOLID, EDGE 11/1 UNKNOWN, UNDERGROUND **EWES** WETLAND, FEDERAL AND STATE MJS JURISDICTION, STATE - *]UU[* -STXL UNKNOWN, HANGING X WALK, LADDER LINE **EWM** WETLAND, MITIGATION AREA JURISDICTION, TOWN OUL-UUO UNKNOWN, OVERHEAD SW EWS WETLAND, STATE STXLB X WALK, LADDER BAR LINE M.IV JURISDICTION, VILLAGE UW WATER LINE, UNDERGROUND • = W (WHITE) OR Y (YELLOW) MPL PROPERTY LOT LINE WATER LINE, HANGING TRAFFIC CONTROL 1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED). MPLA PROPERTY LOT LINE, APPROXIMATE UWO WATER LINE, OVERHEAD OW 0 TCSW SIGNAL, SPAN WIRE 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.). MSL SUB LOT LINE FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS. PIN 8780.49 BRIDGES **CULVERTS** KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON 5. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE LEGEND, LINE AND POINT SYMBOLOGY DRAWING NO. LEG-1

COUNTY: ULSTER

REGION:

CORRESPONDING EXISTING FEATURES.

GREENMAN-PEDERSEN, INC CONSULTING ENGINEERS



CITY OF KINGSTON

CELL NAME DESCRIPTION CELL CELL NAME DESCRIPTION **CELL** CELL CELL NAME DESCRIPTION NAME DESCRIPTION NAME DESCRIPTION NAME DESCRIPTION \oplus CENTER OF CURVATURE DINV ₩ IANT_P 0 MDL1P DEED LINE, TYPE 1 \mathbb{E} UEB ELECTRIC, BOX **ANTENNAS** SINGLE POST **ACOGO** 2 S_P SINGLE POST, PROPOSED ELECTRIC, METER IASCTS ACCOU, SPEED/COUNT SNSR.S MDL2P DEED LINE. TYPE 2 UEM DS STRUCTURE. RECTANGULAR (<u>©</u>) ACS CURVE TO SPIRAL 3 SB F BACK TO BACK, PROPOSED Œ) UEMH ELECTRIC. MANHOLE TCARPAD CABINET & PAD MDL 3F DEED LINE. TYPE 3 DSI STRUCTURE, INVERT 4 Δ DETOUR, POINT OF INTERSECT \oplus ADPT_F ICCTV CCTV SITE MDL4P DEED LINE, TYPE 4 UEPT ELECTRIC, POLE, TRANS. DSM STRUCTURE, MANHOLE)CDPD(ADPL_P DETOUR, POINT ON LINE ICDPD **5** \bigoplus UGM GAS, METER CDPD TRANSCEIVER MDL5P DEED LINE, TYPE 5 SPM PARKING METER STRUCTURE, MANHOLE, DSMTXX_F **AEQN** EQUATION 0 **ICELL** CELL PHONE TOWER MEEP EASEMENT, EXISTING RFMSRM REFERENCE MARKERS **UGMH** GAS. MANHOLE "XX" = 48, 60, 72, 96 (A) **AEQNAHD EQUATION AHEAD** SRSC3 SHLD, CTY, 123 DIG **-⟨Ĝ>**-GAS, LINE MARKER **ICJB** CONDUIT JACK OR BORING (A) EASEMENT, PERM., APPROX. DSR STRUCTURE, ROUND (B) **AEQNBK EQUATION BACK** 0 SRSC4 SHLD, CTY, 4 DIG. FP UGP GAS/FUEL PUMP \boxtimes TONTI CAR MEPP F CONTROLLER CABINET EASEMENT, PERM., BACK LINE STRUCTURE, RECT., WITH CURB TYPE "X"
"X" = F, G, N, O, P, R \odot **AEVT EVENT STATION** \bigcirc 0 Ω **ICPB** COMMUNICATION PULL BOX MEPSP_ EASEMENT, PERM., SHAPE SRSCT2 SHLD, CTY TOUR, 1-2 DIG. UGV GAS, VALVE (o) APC POINT OF CURVATURE $-\otimes$ ICTD CONDUIT TURNING DOWN ♦ MFAP_F FEE ACQUISITION, APPROX. SRSCT4 SHLD, CTY TOUR, 3-4 DIG. (XIO) STRUCTURE, RECT., TYPE "X" POINT OF COMPOUND CURVATURE "X" = I, K, L, M, O, P, U APCC ULP \odot —⊙ ICTU CONDUIT TURNING UP **۞** MFP_P FEE ACQUISITION, BACK LINE SRSI SHLD. INTERSTATE LIGHTING. POLE API POINT OF INTERSECTION)Ó(**ICVTR1** COMM. VEH. ROAD TRANSCEIVER MFSP_ FEE ACQUISITION, SHAPE SRSN2 SHLD, NATIONAL, 2 DIG. **ULPM** LIGHTING, POLE, MEDIAN **ENVIRONMENTAL** APOB POINT OF BEGINNING IDEFAUL1 SHLD, NATIONAL, 3 DIG. ULPP LIGHTING, POLE, PED. MHBA HIGHWAY BNDRY., APPROX CULV EI0P_P STR., INLET, OUTLET PROT. \odot APOC POINT OF CURVATURE ΕZ • \bigcirc UMFC MISC. FILLER CAP SRSS2 SHLD. STATE, 2 DIG. **IEZR** E-ZPASS READER MHBCP HISTORICAL, BLDG. CORNERS AP0E POINT OF END Δ **IEZTR** TRANSMITTAL READER HIGHWAY BNDRY, PT SRSS3 SHLD, STATE, 3 DIG **-**ô> UOLM OIL, LINE MARKER (B) EIPGB_P STR., INLET PROT., GRAVEL BAG **APOL** POINT ON LINE \odot **IFOXCAB** 0 SRSS4 SHLD, STATE, 4 DIG. POLE, WITH UTILITY FIBER OPTIC X-CONNECT CABINET MJCF PT., JURIS, CITY H/S) EIPHS_P STR., INLET PROT., HAY/STRAW \odot APOS POINT ON SPIRAL **③** MPBC PT., BUILDING CORNER \odot LIPD POLE, DEAD (NO UTILITY) IFUSSPL FUSION SPLICE TRAFFIC CONTROL 0 AP01 POINT ON TANGENT **THARAD**\ HAR ADVISORY SIGN PT., CROSS CUT UPL POLE, WITH LIGHT (PRFB) EIPP_P STR., INLET PROT., PREFAB. **TCBJ** BOX. JUNCTION POINT ON VERTICAL CURVE 一位 **APOVO** IHARS1 HAR SITE MPDH PT., DRILL HOLE (S) USME SANITARY SEWER MANHOLE TCBP BOX. PULL BOX (SF) EIPSF_F STR., INLET PROT., SILT FENCE POINT ON VERTICAL TANGENT APOVT * P HTR Δ ILC LOAD CENTER MPF PT., FENCE LOCATION TELEPHONE, BOOTH TCBS BOX, SPLICE **APORC** POINT ON REVERSE CURVE 0 UTLM TELEPHONE, LINE MARKER IMECSPI MECHANICAL SPLICE MPIF PT., IRON PIPE **ERCB** RISER, CONCRETE BOX TCMC MICROCOMPUTER CABINET 0 APT POINT OF TANGENCY PM] \odot \overline{T} HTMH TELEPHONE, MANHOLE **TMSCS** PORT, SPEED & COUNT SENSOR MPTR PT., IRON ROD ETRS_F TRAP, SEDIMENT PED POLE POINT OF VERTICAL CURVATURE **(** APVC | M |] **IMSCTS** MICRO SPEED & COUNT SENSOR MPM PT., MONUMENT **-**♦> UTVLM CABLE TV, LINE MARKER WETLAND FLAG TCSH SIGNAL HEADS APVCC POINT OF VERT. CMPND CURVE \blacksquare CABLE TV, PULL BOX Δ (M): IMT MICROWAVE TRANSCEIVER MPMM PT., MONUMENT, MISC. UTVPB \odot **TCSP** SIGNAL POLE **GEOTECHNICAL** POINT OF VERT. INTERSECTION VMS Ø PT., NATI \Box TOVHVMS PERM. OVERHEAD VMS MPN UUB UNKNOWN, BOX TRAFFIC WORK ZONE • GDH DRILL HOLE APVRC POINT OF VERT. REVERSE CURVE Δ PA] **IPASCS** PORT. ACCOU. SPD & CNT. SENSOR * MPRS PT., RAILROAD SPIKE \boxtimes UNKNOWN, JUNCTION BOX UUJB APV1 POINT OF VERTICAL TANGENCY PEDESTRIAN SIGNAL HEAD 斑 PT. SPIKE TW7AP P ARROW PANEL **IPEDS** MPSE UNKNOWN, MANHOLE UUMH LANDSCAPE (<u>©</u>) ASC SPIRAL TO CURVE \Diamond **IPSS** PAVEMENT SURFACE SENSOR **MPST** PT., STAKE TW7APC P ARROW PANEL, CAUTION MODE UUPB UNKNOWN, PULL BOX LELS ELEVATION. SPOT SPIRAL POINT OF INTERSECTION ASP1 PVMS ••• TWZAPT_P ARROW PANEL, TRAILER OR SUPPOR **IPVMS** (×) PT., TREE W/ WIRE UUVL UNKNOWN, VALVE LFP FLAG POLE SPIRAL TO SPIRAL ASTS IRM + BARRICADE (TYPE III) PT., WALL LOCATION ∞ RAMP METER UUVT UNKNOWN, VENT MAILBOX \otimes AST SPIRAL TO TANGENT ∕\ RWI **IRWIS** RDWY WEATHER INFO. SENSOR TWZCMS_F CHANGEABLE MESSAGE SIGN (PVMS) 0 UUW UNKNOWN, WELL ROW ACQUISITION PAPER BOX LPB \otimes ATS TANGENT TO SPIRAL × ISF TWZFLG_P \odot WATER, FIRE HYDRANT -LIWE 0 **LPST** POST, SINGLE MFS_P_T FFF ACQUISITION AVEV1 VERTICAL EVENT POINT ISST SPREAD SPECT. TRANSCEIVER TWZFT_P FLAG TREE Δ W UWM WATER, METER (II) LRB ROCK, BOULDER IMPACT ATTENUATOR / AVHIGH VERTICAL HIGH POINT ITDB TELEPHONE DEMARCATION BLK TWZIA_P UWMH WATER, MANHOLE MEPS_P_T CRASH CUSHION (TEMPORARY) EASEMENT, PERMANENT LSHC SHRUB, CONIFEROUS \odot AVLOW VERTICAL LOW POINT ITF SUBSURFACE TEMP, PROBE LUMINAIRE (TEMPORARY) UWV WATER. VALVE () LSHD SHRUB. DECIDUOUS METS_P_T EASEMENT, TEMPORARY ➾ **W** HWW IVTRI TW7SDT P SYMBOL. DIRECTION OF TRAFFIC WATER. WELL VEHICLE TO RDWY TRANSCEIVER BRIDGE 쏬 LTC TREE, CONIFEROUS SYMBOL, DIRECTION OF TEMPORARY IWIMD TWZSDTD_F W/M WEIGHT IN MOTION DETECTOR METS_P. OCCUPANCY. TEMPORARY BSC BRIDGE, SCUPPER LTD TREE, DECIDUOUS)WVR TWZSGN_P SIGN (TEMPORARY TWVR WIRELESS VIDEO REPEATER \bigcirc LTS TREE, STUMP CONTROL FEE ACQUISITION W/O ACCESS SIGNAL, TRAFFIC OR PEDESTRIAN TWZSIG_P (V)**TWVRC** WIRELESS VIDEO RECEIVER (TEMPOŘARY) Ø TREE, WELL OR WALL CBP IWVTT WIRELESS VIDEO TRANSMITTER മ Δ BASELINE, POINT TWZWL_P WARNING LIGHT ROADWAY LUKP UNKNOWN POINT \odot CBPOL BASELINE, POINT ON LINE 113 WORK VEHICLE NOTES: ∅ RES_P ELEVATION, SPOT WORK VEHICLE WITH TRUCK CBSP BASELINE, SPUR POINT TWZWVA_P 1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED). \boxtimes RGA GUIDE RAIL, ANCHOR CBTP BASELINE, TIE POINT 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY \bigcirc GUIDE POST, SINGLE SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLÉ, ETC.). CPBM 3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CPH POINT, HORIZ, PHOTOGRAMMETRY PIN 8780.49 BRIDGES **CULVERTS** CORRESPONDING PROPOSED FEATURES. KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 **(** CPSM POINT, SURVEY MARKER, PERM. 4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED ROADWAY STREETSCAPE PROJECT CITY OF KINGSTON CPSV POINT, VERT., PHOTOGRAMMETRY FEATURES IS THICKER (0.015 IN ON B SIZE DRAWINGS). LEGEND, LINE AND POINT SYMBOLOGY DRAWING NO. LEG-2 MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF SHEET NO. 4 COUNTY: ULSTER REGION: TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.

6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES. GREENMAN-PEDERSEN, INC CITY OF KINGSTON CONSULTING ENGINEERS

ROW MAPPING

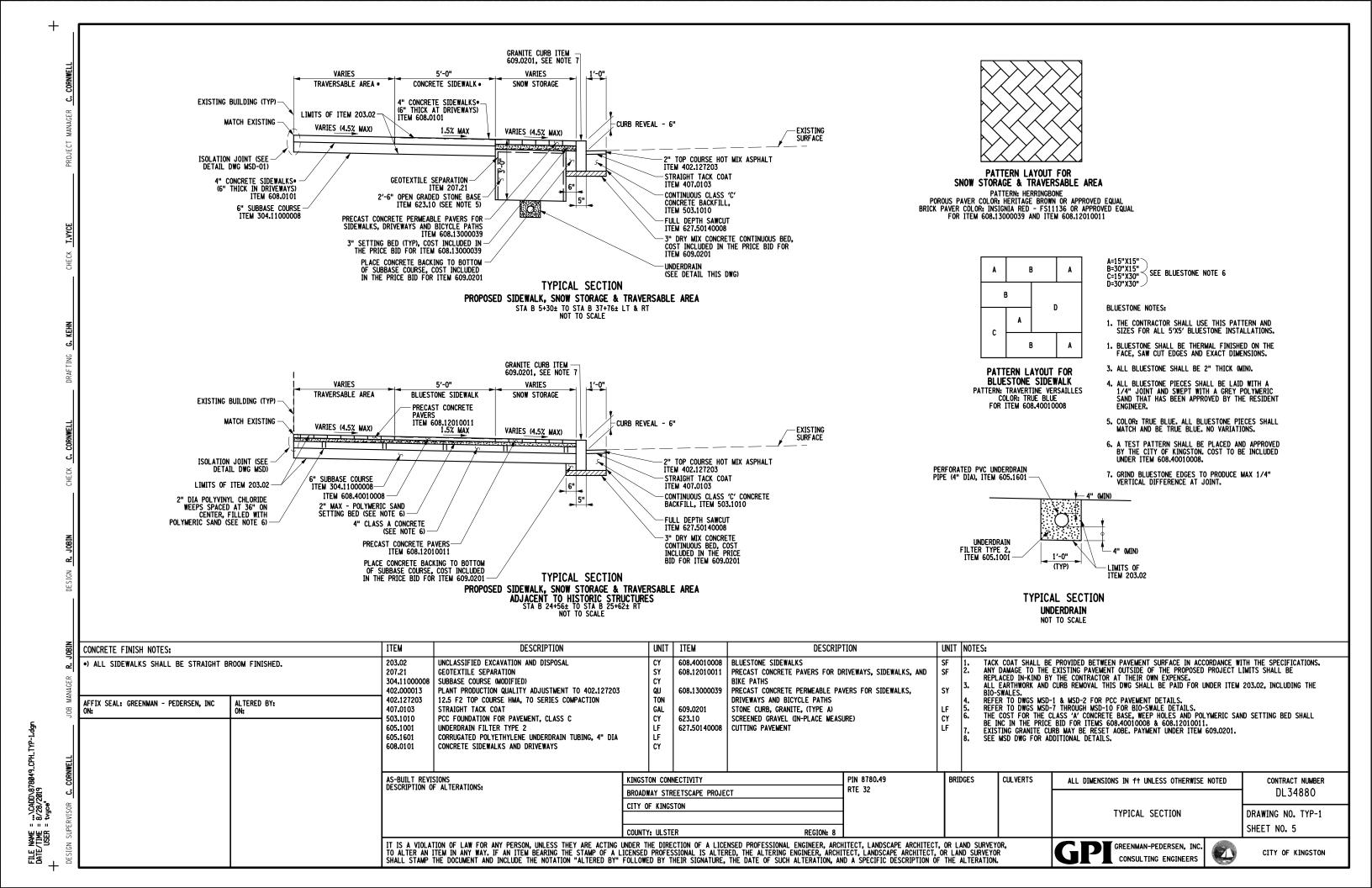
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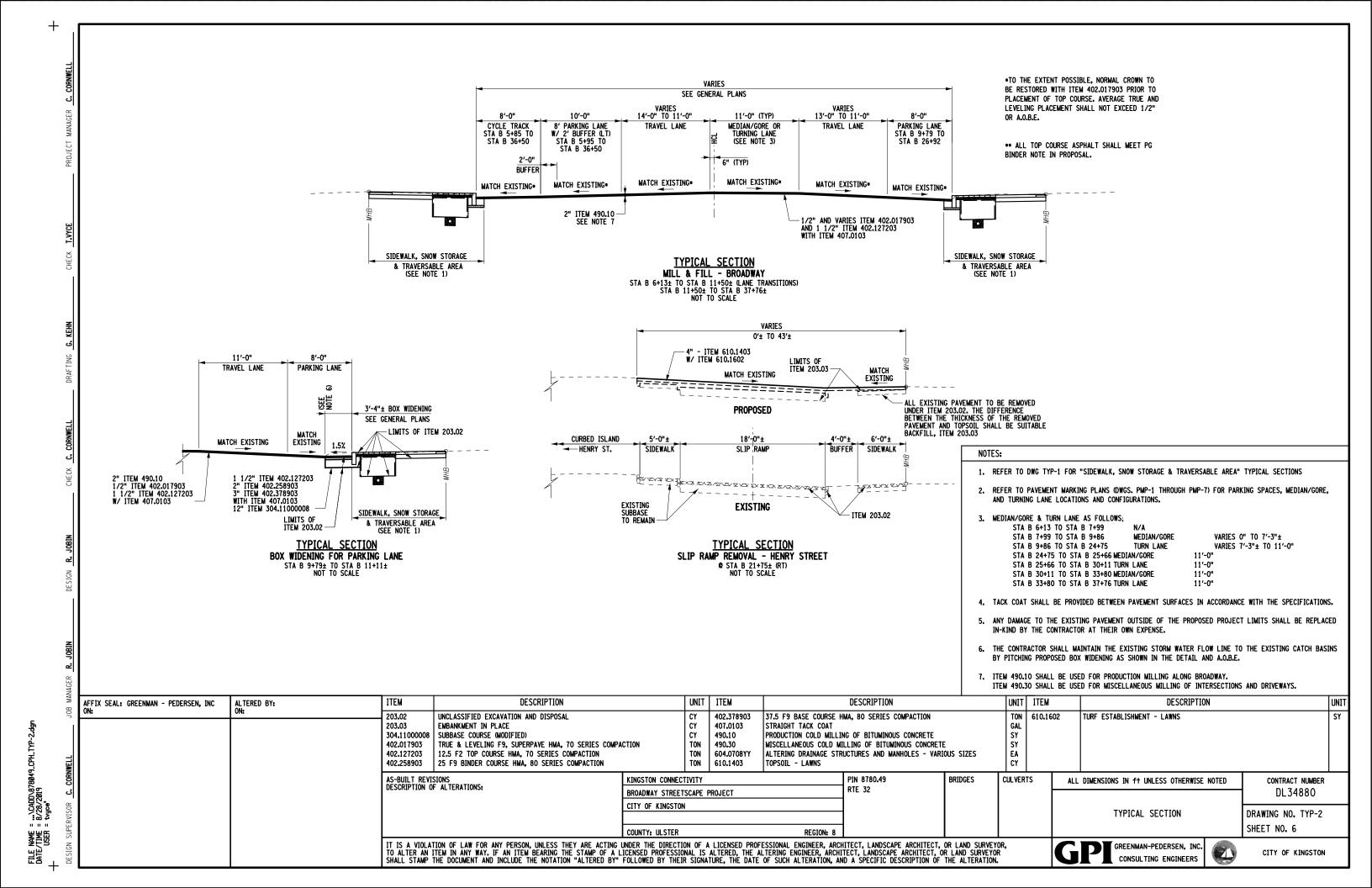
ITS

ALIGNMENT

DRAINAGE

UTILITIES





- 2. CURRENT NATIONAL "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) WITH NEW YORK STATE SUPPLEMENT SHALL BE IN EFFECT FOR THIS PROJECT.
- 3. ADDITIONAL NOTES MAY BE FOUND ON SUBSEQUENT DRAWINGS. SUCH NOTES, WHILE PERTAINING TO THE SPECIFIC DRAWING THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HEREIN.
- 4. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT DUE TO THE NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EXTENT OF THE WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH THE FIELD CONDITIONS AND A.O.B.E.
- THE CONTRACTOR SHALL EXAMINE AND VERIFY IN THE FIELD ALL EXISTING CONDITIONS AND DIMENSIONS WITH THOSE SHOWN ON THE PLANS. THE CONTRACTOR SHALL USE THE FIELD CONDITIONS AND DIMENSIONS, AND MAKE THE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS AS APPROVED BY THE ENGINEER. THE RESULTS OF THIS CHECK OF CONDITIONS AND DIMENSIONS SHALL BE SO NOTED ON THE DRAWINGS SUBMITTED FOR APPROVAL.
- 6. THERE SHALL BE NO CLAIM AGAINST THE CITY BY THE CONTRACTOR FOR WORK PERTAINING TO MODIFICATIONS AS MAY BE REQUIRED DUE TO ANY DIFFERENCE BETWEEN ACTUAL FIELD CONDITIONS AND THOSE SHOWN BY THE DETAILS AND DIMENSIONS ON THE CONTRACT PLANS. THE CONTRACTOR WILL BE PAID AT THE UNIT BID PRICE FOR THE ACTUAL QUANTITIES OF MATERIALS USED OR FOR THE WORK PERFORMED, AS INDICATED BY THE VARIOUS ITEMS IN THE CONTRACT AND PER SPECIFICATIONS FOR MAJOR AND MINOR ITEMS. SEF NOTE 8.
- 7. AT ALL TIMES, THE CONTRACTOR SHALL TAKE MEASURES TO PROVIDE POSITIVE DRAINAGE OF SURFACE RUNOFF FROM THE TRAVEL LANES AND CONTROL OF THE RUNOFF TO PREVENT EROSION, POLLUTION, SEDIMENTATION OR OTHER DISCHARGES WHICH WOULD AFFECT PROPERTIES ADJACENT TO THE WORK SITE. ALL MEASURES TAKEN TO PROVIDE POSITIVE DRAINAGE SHALL BE APPROVED BY THE ENGINEER PRIOR TO IMPLEMENTATION. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.
- 8. THE CONTRACTOR SHOULD NOTE THAT ADDITIONAL WORK MAY BE REQUIRED AS THE CONTRACT PROGRESSES WHICH IS NOT SHOWN OR NOTED ON THE PLANS. THIS WORK SHALL BE PERFORMED BY THE CONTRACTOR AS ORDERED BY THE ENGINEER AND PAYMENT SHALL BE MADE IN ACCORDANCE WITH SECTION 100 OF THE NYSDOT STANDARD SPECIFICATIONS BOOK.
- THE CLEARING AND GRUBBING ITEM SHALL CONSIST OF SPECIFIC REMOVAL OF TREES AND TREE STUMPS WITHIN THE PROJECT LIMITS WHERE INDICATED ON THE PLANS AND A.O.B.E. THE COST OF THIS WORK SHALL BE INCLUDED UNDER ITEMS 614.060304 AND 614.0701. SEE ADDITIONAL NOTES IN THIS DWG.
- 10. NO PAYMENT SHALL BE MADE FOR WORK CALLED FOR BY NOTES ON THE PLANS, IN THE SPECIFICATIONS, OR UNDER THE HEADING GENERAL NOTES UNLESS PAYMENT IS SPECIFICALLY INDICATED BY ITEM NUMBER. THE COST OF WORK FOR WHICH NO SEPARATE PAYMENT IS INDICATED SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS ITEMS IN THE CONTRACT.
- 11. THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY SUPPORTS, BRACING OR OTHER DEVICES THAT MAY BE REQUIRED OR THAT MAY BE DIRECTED BY THE ENGINEER TO PROTECT THE SAFETY OF ADJACENT STRUCTURES, ROADWAYS OR THE VARIOUS ITEMS IN THE CONTRACT. NO SEPARATE PAYMENT SHALL BE MADE.
- 12. PAVED AREAS DISTURBED BY THE CONTRACTOR AS PART OF WORK TO BE PERFORMED UNDER THIS CONTRACT, SHALL BE RESTORED TO AN ACCEPTABLE CONDITION AS SPECIFIED BY AND SATISFACTORY TO THE ENGINEER.
- 13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR GUARDING AND PROTECTING ALL OPEN EXCAVATIONS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 107.05 "SAFETY & HEALTH REQUIREMENTS" OF THE NYSDOT STANDARD SPECIFICATIONS AND ENSURING ACCESS TO ADJACENT STORE FRONTS. IN ADDITION, A MINIMUM OF 1-INCH THICK PLATE WILL BE REQUIRED OVER ALL EXCAVATIONS THAT ARE TO BE COVERED (A.O.B.E.). THE PLATES SHALL BE SECURELY FASTENED DOWN TO THE SATISFACTION OF THE ENGINEER AND SHALL BE STRUCTURALLY CAPABLE OF CARRYING ALL IMPOSED LOADS. THE COST SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT. SEE ADDITIONAL PROPOSAL NOTES.
- 14. INFILTRATION TESTS ARE LOCATED IN THE CONTRACT PROPOSAL DOCUMENTS.

ALTERED BY:

AFFIX SEAL: GREENMAN - PEDERSEN, INC

- 15. PROVISIONS TO DE-WATER EXCAVATIONS, DUE TO CONSTRUCTION OPERATIONS ALONG THE PROJECT MAY BE REQUIRED. THERE SHALL BE NO SEPARATE PAYMENT FOR ANY DE-WATERING SYSTEMS. COST SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.
- 16. THE CONTRACTOR SHALL KEEP ALL DRAINAGE FACILITIES, WITHIN THE CONTRACT LIMITS, CLEAN AND FULLY OPERATIONAL AT ALL TIMES (A.O.B.E.). THIS WORK SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT
- 17. THE CONTRACTOR SHALL PROVIDE SURVEY AND STAKEOUT AS REQUIRED AND IN ACCORDANCE WITH SECTION 625 OF THE STANDARD SPECIFICATIONS. COST FOR THIS WORK SHALL BE INCLUDED UNDER ITEM 625.01-SURVEY OPERATIONS.

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

- 18. THE CONTRACTOR IS TO VISIT THE SITE BEFORE BIDDING TO BECOME FAMILIAR WITH THE PRESENT CONDITIONS AND TO JUDGE THE EXTENT AND NATURE OF THE WORK TO BE DONE UNDER THIS CONTRACT. NO EXTRA COMPENSATION WILL BE ALLOWED BECAUSE OF FAILURE TO INCLUDE IN THE BID ALL ITEMS AND MATERIALS WHICH ARE REQUIRED TO BE FURNISHED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
- 19. THE CONTRACTOR IS REQUIRED TO PROTECT WORKERS AT ALL TIMES IN CONFORMANCE WITH APPLICABLE OSHA REGULATIONS.
- O. DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT WORK IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM, ANY DAMAGE TO THE VARIOUS WATER BODIES FROM POLLUTION BY DEBRIS, SEDIMENT OR OTHER FOREIGN MATERIALS, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL OR OTHER IMPURITIES.
- 21. THE FIRST WATERING NEEDED FOR VEGETATION AND OTHER LANDSCAPING ITEMS SHALL BE INCLUDED UNDER EACH RESPECTIVE ITEMS IN THE CONTRACT ALL SUBSEQUENT WATERINGS SHALL BE PAID FOR UNDER ITEM 610.19.
- 22. DETAILS ON THE DRAWINGS LABELED AS 'NOT TO SCALE' ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONALLY AND ARE FULLY DIMENSIONED.

UTILITY NOTES

- LOCATION OF UTILITIES, PUBLIC AND/OR PRIVATE, INDICATED ON THE PLANS AS EXISTING AND/OR TO BE CONSTRUCTED ARE APPROXIMATE ONLY. THEIR EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD. ADDITIONAL UTILITY LINES, WHETHER ABANDONED OR IN SERVICE, MAY EXIST AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONDUCT ALL OPERATIONS AND TAKE NECESSARY PRECAUTIONS SUCH THAT INTERFERENCE WITH OR DAMAGE TO THESE OR OTHER FACILITIES DURING THE COURSE OF CONSTRUCTION IS PREVENTED. PRIOR TO ANY EXCAVATION, THE CONTRACTOR IS TO CALL DIG SAFELY N.Y. TO HAVE UNDERGROUND UTILITIES LOCATED.
- IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE, CAUSING THE INTERRUPTION IN SAID SERVICE, THE CONTRACTOR SHALL IMMEDIATELY COMMENCE WORK TO RESTORE SERVICE AND MAY NOT CEASE WORK UNTIL SERVICE IS RESTORED. ALL COSTS TO REPAIR OR REPLACE DAMAGED UTILITIES SHALL BE AT THE EXPENSE OF THE CONTRACTOR. IF THE CONTRACTOR DOES NOT MAKE IMMEDIATE NECESSARY REPAIRS, THE RESPECTIVE OWNING COMPANIES OR MUNICIPAL FORCES MAY DO THE WORK, AND THE COST THEREOF CHARGED AGAINST THE CONTRACTOR.
- THE CONTRACTOR SHALL MAKE EXPLORATIONS AS MAY BE NECESSARY TO DETERMINE THE DIMENSIONS AND LOCATIONS OF LINES THAT MAY BE SUBJECT TO DAMAGE. COST TO BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT. ITEM 204.03 WILL BE USED A.O.B.E. AT BIO-SWALES.
- PRIVATE UTILITY COMPANIES ARE ALSO REQUIRED TO ALTER/RELOCATE THEIR FACILITIES WITHIN THE PROJECT LIMITS. IT IS ANTICIPATED THAT THIS WORK SHALL BE COMPLETED PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR MAY BE REQUIRED TO LAYOUT THE PROJECT CENTERLINE AND OTHER FEATURES TO PROVIDE THE UTILITY COMPANIES WITH SUFFICIENT INFORMATION TO ALTER/RELOCATE THEIR FACILITIES. THE COST OF THIS WORK SHALL BE INCLUDED UNDER ITEM 625.01.
- . THE CONTRACTOR WILL BE RESPONSIBLE FOR CONTACTING THE UTILITY COMPANY FOR PROVIDING RESTRAINTS TO THE EXISTING UTILITY POLES (IF REQUIRED) WITHIN THE WORK LIMITS, WHILE CONSTRUCTION IS TAKING PLACE. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER TO PERFORM THE WORK. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS.
- THE UNDERGROUND UTILITY INFORMATION SHOWN ON THE PLANS IS BASED UPON THE FOLLOWING:

QUALITY LEVEL A IS THE HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED BY THE ACTUAL EXPOSURE (OR VERIFICATION OF PREVIOUSLY EXPOSED AND SURVEYED UTILITY FACILITIES) OF THE SUBSURFACE UTILITIES, USING (TYPICALLY) MINIMALLY INTRUSIVE EXCAVATION EQUIPMENT TO DETERMINE THEIR PRECISE HORIZONTAL AND VERTICAL POSITIONS, AS WELL AS THEIR OTHER OWN FACILITY ATTRIBUTES. (SHOWN AS QLA)

QUALITY LEVEL B IS THE SECOND HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED THROUGH THE APPLICATION OF APPROPRIATE SURFACE GEOPHYSICAL METHODS (I.E., UNDERGROUND CAMERAS, RADAR, SONAR, TONE OUTS, ETC.) TO IDENTIFY THE EXISTENCE AND APPROPRIATE HORIZONTAL POSITION OF SUBSURFACE UTILITY FACILITIES, QUALITY LEVEL B DATA ARE REPRODUCIBLE BY SURFACE GEOPHYSICS AT ANY POINT OF THEIR DEPICTION, THE INFORMATION WAS SURVEYED TO APPLICABLE TOLERANCES AND REDUCED ONTO THE PLANS. NO EXCAVATIONS WERE PERFORMED. (SHOWN AS QLB)

QUALITY LEVEL C IS THE THIRD HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES AND BY USING PROFESSIONAL JUDGEMENT IN CORRELATING THIS INFORMATION TO QUALITY LEVEL D INFORMATION. (SHOWN AS QLC)

QUALITY LEVEL D IS THE LOWEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS WAS DERIVED SOLELY FROM EXISTING NYSDOT AND/OR UTILITY COMPANY RECORDS OR RECOLLECTIONS. (SHOWN AS QLD)

- *ALL UTILITIES THIS CONTRACT QL C AND D*
- THE CONTRACTOR SHALL PROTECT ALL UNDERGROUND UTILITIES TO REMAIN IN PLACE FROM DAMAGE DURING THE CONSTRUCTION. METHODS OF PROTECTION MAY INCLUDE STEEL PLATES OVER THE UTILITY SO THAT WHEEL LOADINGS FROM CONSTRUCTION VEHICLES DO NOT DAMAGE THE UTILITY. THE COST OF PROVIDING PROTECTION OF UNDERGROUND UTILITIES SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT AND APPROVED BY THE FINGINFER.
- CONTRACTOR SHALL PROVIDE ALTERNATIVE BENCHMARK AND/OR BASELINE TIES IF IMPACTED DURING CONSTRUCTION. COST INCLUDED UNDER ITEM 625.01.

KINGSTON CONNECTIVITY

CITY OF KINGSTON

BROADWAY STREETSCAPE PROJECT

FIRE HYDRANT REPLACEMENT THROUGHOUT THE PROJECT LIMITS SHALL USE HYDRANTS APPROVED BY THE CITY OF KINGSTON WATER DEPARTMENT. ALL REPLACEMENT WORK SHALL BE COORDINATED WITH THE CITY OF KINGSTON WATER AND FIRE DEPARTMENTS.

DAMAGE TO EXISTING STRUCTURES: VEGETATION/SHRUBS: OR OTHER AMENITIES

NUMEROUS STRUCTURES (INCLUDING BUILDINGS, UTILITIES, ETC.) AND VEGETATION/SHRUBS ARE PRESENT WITHIN THE WORK LIMITS AND ARE TO REMAIN IN PLACE. THE CONTRACTOR SHALL TAKE EXTRA PRECAUTIONS NOT TO DAMAGE THESE ITEMS. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ALL DAMAGED CAUSED BY OPERATIONS TO THE EXISTING STRUCTURES OF MATERIALS WHICH ARE NOT INCLUDED AS PART OF THE INTENDED WORK. ALL DAMAGE TO THE EXISTING STRUCTURES OR MATERIALS WHICH ARE NOT PART OF THE INTENDED WORK SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR WITHOUT COST TO THE CITY AND TO THE SATISFACTION OF THE ENGINEER EXCEPT AS NOTED WITHIN.

EXCAVATION NOTES

- 1. ASSUME ALL EXCAVATED MATERIAL IS NOT SUITABLE FOR EMBANKMENT CONSTRUCTION.
- 2. ASSUME SUBGRADE IMPROVEMENTS WILL NOT BE REQUIRED.

ENVIRONMENTAL PERMITS

 THE CONTRACTOR SHALL COMPLY WITH ALL ENVIRONMENTAL PERMIT REQUIREMENTS PROVIDED IN THE CONSTRUCTION DOCUMENTS.

TREE CUTTING RESTRICTIONS

- I. IN ORDER TO PREVENT ANY DIRECT IMPACT OF INDIANA BAT (MYOTIS SODALIS), AND NORTHERN LONG-EARED BAT (MYOTIS SEPTENTRIONALIS), BOTH FEDERAL AND STATE LISTED ENDANGERED SPECIES, THE CONTRACTOR'S ATTENTION IS HEREBY DIRECTED TO THE FACT THAT TREE CUTTING RESTRICTIONS APPLY TO TREES OR BRANCHES THAT ARE 3 INCHES OR GREATER IN DIAMETER AT BREAST HEIGHT (DBH).
- 2. DUE TO THE POSSIBLE PRESENCE OF PROTECTED BAT SPECIES, THE CONTRACTOR SHALL BE AWARE THAT TREES OR BRANCHES 3" DIAMETER OR LARGER (TREES MEASURED AT BREAST HEIGHT), CAN ONLY BE CUT FROM NOVEMBER 1 TO MARCH 31. NO TREE CUTTING WILL OCCUR FROM APRIL 1 TO OCTOBER 31.

RIGHT OF WAY NOTES

- 1. ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY (ROW) IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR IS TO ASSURE HIMSELF THAT ALL WORK IS BEING PERFORMED WITHIN THE ROW, INCLUDING BUT NOT LIMITED TO VEHICLE ACCESS; STORAGE OF EQUIPMENT, MATERIALS, DEBRIS AND WASTE; LANDSCAPING; VEGETATION REMOVAL AND MANAGEMENT; GRADING, SEEDING AND THE INSTALLATION OF TURF; AND THE INSTALLATION OF ANY FENCES OR PROTECTIVE BARRIER.
- . IF CONTRACTOR IS UNABLE TO IDENTIFY THE LIMITS OF THE RIGHT-OF-WAY WHEN THE CONTRACT CALLS FOR WORK IN THOSE VICINITIES, THE CONTRACTOR MUST CONTACT THE PROJECT ENGINEER FOR DEFINITIVE BOUNDARY DETERMINATIONS BEFORE ANY WORK MAY BE INITIATED AT THOSE LOCATIONS (STANDARD SPECIFICATIONS SECTIONS 105-10 AND 625).
- 3. IN ACCORDANCE WITH SECTION 107-13 OF THE STANDARD SPECIFICATIONS, RELEASES FOR ANY NON-ESSENTIAL CONTRACT WORK OUTSIDE OF THE EXISTING RIGHT-OF-WAY, INCLUDING PLANTINGS, LANDSCAPING OR DRIVEWAY ENHANCEMENT, WILL BE PROVIDED BY THE CITY OF KINGSTON AND IN NO INSTANCE ARE TO BE SECURED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT INVADE UPON PRIVATE PROPERTIES, LANDS OR BUILDINGS OUTSIDE OF THE RIGHT-OF-WAY FOR ANY REASON WITHOUT FIRST SECURING WRITTEN PERMISSION FROM THE PROPERTY OWNER (STANDARD SPECIFICATIONS SECTION 105-15.
- 4. THE CONTRACTOR WILL BE HELD LIABLE FOR ANY DAMAGES DONE. ANY SUCH INJURIES OR DAMAGES SHALL BE SATISFACTORILY REPAIRED OR ITEMS REPLACED AT THE CONTRACTOR'S EXPENSE (STANDARD SPECIFICATIONS SECTION 107-08).

CURB RAMPS AND SIDEWALK NOTES

- 1. ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS, AT SOME LOCATIONS THE PUBLIC RIGHT-OF-WAY IS AT THE BACK OF THE EXISTING SIDEWALK OR VERY CLOSE TO THE BACK OF THE EXISTING SIDEWALK, AT THESE LOCATIONS THE EXISTING SIDEWALK AND CURB RAMP REMOVAL, AND CURB RAMP INSTALLATIONS WILL REQUIRE NON-TRADITIONAL FORM WORK AND/OR PECAST INSTALLATION TO ENSURE THAT ALL WORK IS PERFORMED WITHIN THE PUBLIC RIGHT-OF-WAY, WHEN THE APPARENT HIGHWAY BOUNDARY IS AT THE BACK OF THE SIDEWALK OR CURB RAMP, ALL WORK MUST BE PERFORMED FROM THE ROAD SIDE.
- 2. THE DESIGN OF THE CURB RAMPS WAS DONE USING OBTAINED SURVEY INFORMATION. HOWEVER, HORIZONTAL DIMENSIONS SHOWN ON THE DETAILS ARE APPROXIMATE AND ARE FOR ESTIMATING PURPOSES ONLY. THE PERCENT SLOPES SHOWN ON THE DETAILS SHALL TAKE PRECEDENT OVER HORIZONTAL DIMENSIONS WHEN LAYING OUT CURB PANDS.

STREET AMENITIES

- THE CONTRACTOR SHALL INSTALL ANTI-THEFT/ANCHORAGE FOUNDATIONS, IN ACCORDANCE WITH THE SELECTED
 MANUFACTURERS' RECOMMENDED INSTALLATION PROCEDURES FOR ALL STREET AMENITIES (I.E. BIKE RACK,
 TRASH/RECYCLE BINS, BENCH, PAY STATION, ETC.). THE COST SHALL BE INCLUDED UNDER THE VARIOUS
 STREET AMENITY ITEMS IN THE CONTRACT AND METHODS A.O.B.E.
- 2. THE PARKING METER AT THE CORNER OF BROADWAY AND FIELD COURT SHALL BE REINSTALLED UNDER ITEM 634,06010007. THE CONTRACTOR IS TO REFER TO THE CITY OF KINGSTON'S PARKING METER CODE AND DETAILS FOR INSTALLATION.

RRFB NOTES:

BRIDGES

CULVERTS

PIN 8780.49

RTE 32

- CONTRACTOR SHALL CONTACT CENTRAL HUDSON GAS & ELECTRIC (CHG&E) PRIOR TO STARTING ANY ELECTRICAL WORK.
- . INSTALLATION TO BE PERFORMED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER
- THE HEIGHT OF THE ORNAMENTAL RRFB POLES SHALL BE 15 FEET TO ACCOMMODATE SOLAR PANEL AND ALL REQUIRED SIGNS, PAYMENT FOR THIS ITEM SHALL BE MADE UNDER ITEM 680.82254310.
- . RRFB POLES ARE TO BE PAINTED BLACK AT THE DISCRETION OF THE CITY. PAINTING SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 680.82254310.

ALL DIMENSIONS IN ff UNLESS OTHERWISE NOTED

GENERAL NOTES

CONTRACT NUMBER
DL34880

DRAWING NO. GNN-1
SHEET NO. 7

GREENMAN-PEDERSEN, INC.

CONSULTING ENGINEERS

CITY OF KINGSTON

COUNTY: ULSTER

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

WORK ZONE TRAFFIC CONTROL NOTES:

GENERAL:

WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), THE CURRENT VERSION OF SECTION 619 OF THE STANDARD SPECIFICATIONS, THE CURRENT WORK ZONE TRAFFIC CONTROL (619 SERIES) STANDARD SHEETS, ANY PROVISIONS CONTAINED IN THE PLANS AND/OR PROPOSAL OF THIS CONTRACT, AND AS DIRECTED BY THE ENGINEER.

WORK ZONE TRAFFIC CONTROL FOR MOBILE OPERATIONS SHALL BE PROVIDED IN ACCORDANCE WITH FIGURES 6H-4, 6H-17 AND 6H-35, AS APPLICABLE, OF THE MUTCD.

AS DEFINED IN SECTION 101-02 OF THE STANDARD SPECIFICATIONS, THE MUTCD CONSISTS OF THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THE NEW YORK STATE SUPPLEMENT TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS.

NIGHTTIME CONSTRUCTION IS NOT ALLOWED UNDER THIS CONTRACT.

CHANGES TO THE WORK ZONE TRAFFIC CONTROL PLAN:

THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE WORK ZONE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE A MINIMUM OF SEVEN (7) CALENDAR DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE BASIC CONCEPT OR SCOPE OF THE WORK ZONE TRAFFIC CONTROL PLAN. SUCH CHANGES TO THE BASIC CONCEPT OR SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE A MINIMUM OF FORTY-FIVE (45) CALENDAR DAYS PRIOR TO IMPLEMENTATION OF SIGH REVISIONS.

CONSTRUCTION EQUIPMENT AND VEHICLES

ALL VEHICLES AND EQUIPMENT WITHIN THE CONTRACT LIMITS AND ON TRAVEL LANES AND/OR SHOULDERS SHALL BE EQUIPPED WITH AND SHALL OPERATE A MINIMUM OF ONE AMBER ROTATING OR FLASHING LIGHT EMITTING DIODE (LED) BEACON VISIBLE FROM ALL DIRECTIONS FOR A MINIMUM OF 1000 FEET DURING DAYLIGHT. IF VISIBILITY OF A SINGLE BEACON IS BLOCKED BY A PORTION OF THE VEHICLE OR EQUIPMENT, ADDITIONAL BEACONS SHALL BE PROVIDED. BEACONS SHALL BE MOUNTED IN A MANNER WHICH DOES NOT CAUSE GLARE FOR DRIVERS USING THE ROADWAY OR THE OPERATOR OF THE VEHICLE OR EQUIPMENT.

ALL BARRIER/SHADOW VEHICLES SHALL WEIGH A MINIMUM OF 18,000 LB AND SHALL BE EQUIPPED WITH TRUCK-MOUNTED IMPACT ATTENUATORS AND TRUCK-MOUNTED ARROW PANELS, IMPACT ATTENUATORS SHALL MEET THE REQUIREMENTS OF NCHRP 350 TEST LEVEL 3 AND SECTION 729-12 OF THE STANDARD SPECIFICATIONS, ARROW PANELS SHALL BE TRUCK-MOUNTED SERIES B OR C PANELS MEETING THE REQUIREMENTS OF SECTION 729-15 OF THE STANDARD SPECIFICATIONS AND SECTION 6F.61 OF THE MUTCD. THE PRICE OF TRUCK-MOUNTED IMPACT ATTENUATORS AND TRUCK-MOUNTED ARROW PANELS SHALL BE INCLUDED IN THE PRICE BID FOR THE BASIC WORK ZONE TRAFFIC CONTROL ITEM.

PARKING OF CONSTRUCTION EQUIPMENT & VEHICLES AND STORAGE OF MATERIALS:

CONSTRUCTION EQUIPMENT (INCLUDING LIGHT TOWERS, IF USED), VEHICLES AND MATERIALS SHALL BE PLACED OR STORED DURING NON-WORKING HOURS A MINIMUM OF 30 FEET FROM THE EDGE OF PAYMENT OR BEHIND TEMPORARY CONCRETE BARRIER OR GUIDE RAIL. EQUIPMENT, VEHICLES AND MATERIALS STORED BEHIND TEMPORARY CONCRETE BARRIER OR GUIDE RAIL SHALL NOT BE PLACED OR STORED WITHIN THE APPROPRIATE DEFLECTION DISTANCE SHOWN IN TABLE 619-6, GUIDE RAIL AND TEMPORARY CONCRETE BARRIER STANDARD DEFLECTION DISTANCES, IN THE STANDARD DEFLECTIONS.

PRIVATE VEHICLES OWNED BY THE CONTRACTOR OR THE CONTRACTOR'S WORKERS SHALL BE PARKED, DURING WORKING AND NON-WORKING HOURS, IN ACCORDANCE WITH THE REQUIREMENTS IN THE PRECEDING PARAGRAPH.

CONDITION OF TRAFFIC CONTROL DEVICES:

AT THE START OF CONTRACT WORK, ALL CONES, TEMPORARY TUBULAR MARKERS, DRUMS, CONSTRUCTION BARRICADES, VERTICAL PANELS, WARNING LIGHTS, ARROW PANELS, PORTABLE VARIABLE MESSAGE SIGNS, PAVEMENT MARKING TAPE AND RAISED PAVEMENT MARKERS SHALL APPEAR IN "ACCEPTABLE" CONDITION AS DESCRIBED AND PICTURED IN THE CURRENT EDITION OF THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) MANUAL QUALITY GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES. THESE DEVICES SHALL NOT BE ALLOWED TO FALL BELOW THE "MARGINAL" CONDITION AT ANY TIME DURING THE LIFE OF THE CONTRACT.

ALL SIGNS SHALL BE IN "ACCEPTABLE" CONDITION AS DESCRIBED IN THE ATSSA MANUAL QUALITY GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES THROUGHOUT THE LIFE OF THE CONTRACT. NON-STANDARD SIGN LEGENDS ARE NOT ACCEPTABLE.

RIGID SIGN PANELS SHALL HAVE A MINIMUM MOUNTING HEIGHT OF 7 FEET, MEASURED FROM THE ROADWAY SURFACE TO THE BOTTOM OF THE LOWEST SIGN PANEL. FOR SIGNS INCORPORATING AN AUXILIARY PANEL BELOW THE PRIMARY PANEL, THE MINIMUM MOUNTING HEIGHT SHALL BE 6 FEET. FLEXIBLE PANEL AND LIGHTWEIGHT RIGID PANELS SHALL BE MOUNTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: REQUIREMENTS FOR PORTABLE VARIABLE MESSAGE SIGNS:

COORDINATE WITH NYSDOT CONTRACT D263784 I-587 ROUNDABOUT FOR PLACEMENT OF PVMS SIGNS.

PORTABLE VARIABLE MESSAGE SIGNS SHALL BE PLACED AT THE LOCATIONS SHOWN IN THE PLANS, AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT SHALL BE MADE UNDER ITEM 619.110511.

PORTABLE VARIABLE MESSAGE SIGNS WILL BE USED TO NOTIFY MOTORISTS AT LEAST TWO WEEKS IN ADVANCE OF THE ANTICIPATED START OF WORK DATE AT EACH LOCATION AND, WHEN APPLICABLE, THE ANTICIPATED START OF EACH SUBSEQUENT STAGE THAT REQUIRES A NEW WORK ZONE TRAFFIC CONTROL PATTERN. THE PYMS SHALL REMAIN IN PLACE UNTIL ALL WORK IS COMPLETED AT A LOCATION OR IN A STAGE, OR AS DIRECTED BY THE ENGINEER. ASSUME DURATION OF CONSTRUCTION AS NOTED.

WHERE TRAFFIC WILL BE RIDING ON MILLED OR GROOVED PAVEMENT AND THE POSTED SPEED LIMIT IS 45 MPH OR GREATER, THE CONTRACTOR SHALL PLACE A PORTABLE VARIABLE MESSAGE SIGN IN ADVANCE OF THE MILLED OR GROOVED PAVEMENT WARNING MOTORCYCLE RIDERS TO USE CAUTION.

THE FOLLOWING IS A SUMMARY OF THE ANTICIPATED NEED FOR PORTABLE VARIABLE MESSAGE SIGNS:

PYMS 1*
LOCATION 1: I-587, 400 LF NORTH OF ALBANY AVE, ON SHOULDER
DURATION: ENTIRE DURATION OF CONSTRUCTION
PYMS 2*
LOCATION 2: ALBANY AVE, 100 LF EAST OF I-587, ON SHOULDER

DURATION: ENTIRE DURATION OF CONSTRUCTION
PVMS 3*
LOCATION 3: ALBANY AVE, 100 LF WEST OF I-587, ON SHOULDER
DURATION: ENTIRE DURATION OF CONSTRUCTION

PVMS 4 LOCATION 4: HENRY ST / CLINTON AVE INTERSECTION, SOUTH OF WORK ZONE, FIRST PARKING SPOT ON HENRY ST.

FIRST PARKING SPOT ON HENRY ST.

DURATION: ENTIRE DURATION OF CONSTRUCTION

PYMS 3 LOCATION 5: BROADWAY, 200 LF EAST OF E/W O'REILLY ST, PARKING SPOT ON BROADWAY DURATION: ENTIRE DURATION OF CONSTRUCTION

• COORDINATE WITH D263784 PVMS, JOINT SIGNAGE MAY BE POSSIBLE BASED UPON SCHEDULE.

THE CONTRACTOR IS REMINDED THAT, IN ACCORDANCE WITH SECTION 619-3.10 OF THE STANDARD SPECIFICATIONS, PORTABLE VARIABLE MESSAGE SIGNS WITH A PAY UNIT OF EACH SHALL BE RELOCATED OR REORIENTED, IF NECESSARY, UP TO FOUR (4) TIMES PER YEAR AS CONDITIONS DICTATE AT NO ADDITIONAL COST TO THE CITY.

STANDARD (TYPICAL) MESSAGES FOR PORTABLE VARIABLE MESSAGE SIGNS SHALL BE SUPPLIED TO THE CONTRACTOR BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL SUBMIT ANY UNIQUE MESSAGES TO THE ENGINEER FOR APPROVAL BY THE SURFACE TRANSPORTATION CONTROLLER. REQUESTS FOR MESSAGE APPROVAL SHOULD ACCOMPANY THE ROAD WORK FORM DESCRIBED IN THE TEMPORARY LANE AND SHOULDER CLOSURES NOTE BELOW.

ACTIVITY AREAS

ACTIVITY AREAS SHOULD BE LIMITED TO ONE SIDE OF A ROADWAY AT A TIME UNLESS APPROVED BY THE ENGINEER. UNLESS OTHERWISE APPROVED BY THE ENGINEER, ACTIVITY AREAS ON ALTERNATE SIDES OF A ROADWAY SHALL BE SEPARATED BY A MINIMUM LONGITUDINAL DISTANCE OF 4L, WHERE L IS DEFINED IN TABLE 6H-4 OF THE MUTCD.

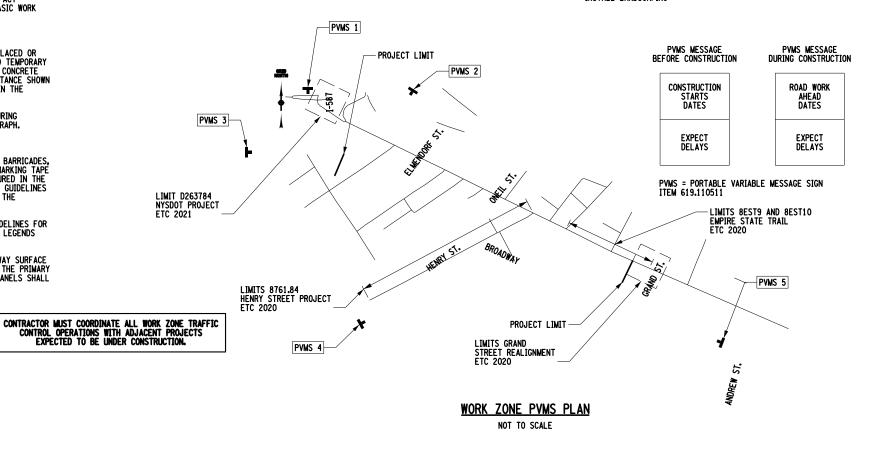
IT MAY BE NECESSARY TO LIMIT THE LENGTH OF A GIVEN ACTIVITY AREA. THE ENGINEER WILL DETERMINE THE MAXIMUM LENGTH OF ACTIVITY AREAS.

WHEN TWO ACTIVITY AREAS ON A ROADWAY ARE SEPARATED BY A SUFFICIENT LONGITUDINAL DISTANCE, ADVANCE WARNING SIGNS SHALL BE PLACED FOR BOTH ACTIVITY AREAS IN ACCORDANCE WITH THE MUTCD OR APPLICABLE WORK ZONE TRAFFIC CONTROL STANDARD SHEET'S). WHEN THE LONGITUDINAL DISTANCE BETWEEN SUCCESSIVE ACTIVITY AREAS IS NOT SUFFICIENT, THE CONTRACTOR SHALL PROVIDE ADEQUATE SIGNAGE TO INFORM ROAD USERS AND REDUCE CONFUSION. THE ENGINEER SHALL DETERMINE THE ADEQUACY OF SIGNAGE IN SUCH CASES.

SUGGESTED CONSTRUCTION SEQUENCE:

INSTALL WORK ZONE TRAFFIC CONTROL AND EROSION AND SEDIMENT CONTROL MEASURERS PERFORM DRAINAGE MODIFICATIONS
INSTALL BIO-SWALES AND NEW CURB
INSTALL SIDEWALKS AND CONCRETE PAVEMENTS
INSTALL PERMEABLE PAVERS
INSTALL SIONS
PERFORM LIGHTING MODIFICATIONS
PERFORM SIGNAL MODIFICATIONS
MILL BROADWAY
PAVE BROADWAY

PAYE BROADWAY
CLEAN EXISTING DRAINAGE SYSTEMS
GENERAL CLEANUP
INSTALL PAYEMENT MARKINGS
INSTALL I LANDSCAPING



AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:

ALTERED BY:
ON:

KINGSTON CONNECTIVITY
BROADWAY STREETSCAPE PROJECT
CITY OF KINGSTON

COUNTY: ULSTER REGION: 8

PIN 8780.49 BRIDGES RTE 32

CULVERTS

ALL DIMENSIONS IN ## UNLESS OTHERWISE NOTED

WORK ZONE TRAFFIC CONTROL

GREENMAN-PEDERSEN, INC

DL34880
DRAWING NO. MPN-1

CONTRACT NUMBER

SHEET NO. 8

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



TEMPORARY LANE AND SHOULDER CLOSURES:

IT WILL BE NECESSARY TO TEMPORARILY CLOSE TRAVEL LANES AND/OR SHOULDERS TO PERFORM THE CONTRACT WORK. THE FOLLOWING RESTRICTIONS SHALL APPLY TO LANE AND SHOULDER CLOSURES:

NO KEY CORRIDOR LANE, RAMP AND/OR SHOULDER CLOSURES ARE ANTICIPATED IN THIS CONTRACT.

- THE SAME NUMBER OF TRAVEL LANES (INCLUDING TURNING LANES) AS EXIST PRIOR TO THIS CONTRACT SHALL BE MAINTAINED IN EACH DIRECTION DURING THE HOURS OF 7:00 A.M. TO 9:00 A.M. AND 4:00 P.M. TO 6:00 P.M. MONDAY TO FRIDAY, INCLUSIVE. RAMP AND/OR SHOULDERS SHALL NOT BE CLOSED DURING THESE
- AS DESCRIBED BELOW, THE TIME RESTRICTIONS CAN BE ADJUSTED BY THE ENGINEER AS TRAFFIC CONDITIONS WARRANT.

THE ENGINEER IS AUTHORIZED TO ORDER ADDITIONAL TIME RESTRICTIONS ON ALL ROADS OF UP TO ONE HOUR PER WORK DAY IF TRAFFIC CONDITIONS WARRANT. THERE SHALL BE NO COST TO THE CITY FOR SUCH ADDED RESTRICTIONS. THE CONTRACTOR SHALL NOT HAVE ANY DELAY CLAIMS AGAINST THE CITY IF THE ENGINEER ORDERS UP TO ONE HOUR OF ADDITIONAL TIME RESTRICTIONS PER DAY.

THE ENGINEER IS AUTHORIZED TO REDUCE TIME RESTRICTIONS AS TRAFFIC CONDITIONS WARRANT.

THE FOLLOWING HOLIDAY RESTRICTIONS SUPPLEMENT THE "SPECIAL NOTE - TEMPORARY LANE CLOSURE RESTRICTIONS FOR MAJOR HOLIDAYS (2019 & 2020)". THE CONTRACTOR WILL NOT BE ALLOWED TO IMPLEMENT ANY TEMPORARY LANE, RAMP OR SHOULDER CLOSURES OR OTHERWISE DISRUPT TRAFFIC IN ANY IMPLEMENT ANY TEMPORARY LANE, RAMP OR SHOULDER CLUSURES OR OTHERWISE DISTOPT TRAFFIC IN ANY WAY DURING THE FOLLOWING HOLIDAYS IN ADDITION TO THOSE STATED IN "SPECIAL NOTE - TEMPORARY LANE CLOSURE RESTRICTIONS FOR MAJOR HOLIDAYS (2019 & 2020)". TEMPORARY LANE CLOSURES SHALL NOT BE PERMITTED UNTIL 10:00 AM, THE DAY FOLLOWING THE HOLIDAY, THIS TIME RESTRICTION SUPERSEDES THOSE TIMES SHOWN IN THE "SPECIAL NOTE - TEMPORARY LANE CLOSURE RESTRICTIONS FOR MAJOR UNITED TO THE PROPERTY CASE OF THE STRICTIONS FOR MAJOR

MOTHERS DAY - SUNDAY, MAY 10. BEGINNING 6:00 AM FRIDAY MAY 8 AND ENDING 10:00 AM MONDAY MAY 11. COLUMBUS DAY - MONDAY, OCTOBER 12. BEGINNING 6:00 AM FRIDAY OCTOBER 9 AND ENDING 10:00 AM

THE CONTRACTOR SHALL BE AWARE OF THE FOLLOWING CITY OF KINGSTON EVENTS WHICH WILL TRAVEL

SAINT PATRICK'S DAY PARADE & SHAMROCK RUN: SUNDAY, MARCH 15. MEMORIAL DAY PARADE: MONDAY, MAY 25. CHILDREN'S DAY PARADE: FRIDAY, JULY 10.

THE CONTRACTOR SHALL SECURE THE WORK ZONE FOR THESE EVENTS AND SHALL HAVE STAFF ON HAND TO RELOCATE EQUIPMENT AS NECESSARY TO ACCOMMODATE THESE EVENTS. PAYMENT SHALL BE INCLUDED IN THE PRICE BID FOR THE BASIC WORK ZONE TRAFFIC CONTROL ITEM.

THE CITY MAY GRANT A WAIVER OF THESE RESTRICTIONS UPON A TIMELY RECEIPT OF A REQUEST FOR SAID WAIVER FROM THE CONTRACTOR. A MINIMUM OF SEVEN (7) CALENDAR DAYS FOR THE REVIEW OF THE CONTRACTOR'S REQUEST

RECOMMENDED PRACTICES FOR CHANNELIZING DEVICES ARE PROVIDED IN THE MUTCD AND SECTION 619-3.02 J OF THE STANDARD SPECIFICATIONS.

TALL CONES, MEETING THE REQUIREMENTS OF SECTION 729-02 OF THE STANDARD SPECIFICATIONS, ARE NORMALLY ADEQUATE FOR WORK ZONES SET UP AND REMOVED ON A DAILY BASIS DURING DAYLIGHT HOURS. DRUMS OR OVERSIZED VERTICAL PANELS ARE PREFERRED FOR ALL OTHER WORK ZONES AND AT ANY LOCATIONS WHERE THE RISK OF INTRUSION IS HIGH, AS DETERMINED BY THE RESIDENT ENGINEER.

THE CONTRACTOR SHALL MAKE FREQUENT CHECKS COMMENSURATE WITH TRAFFIC CONDITIONS TO IDENTIFY AND RESET CHANNELIZING DEVICES DISLODGED BY TRAFFIC.

FLAGGER EQUIPMENT AND STATIONS:

TO INSURE A PROPER LEVEL OF TRAFFIC SAFETY, EACH FLAGGER SHALL BE EQUIPPED WITH A TWO-WAY RADIO DEVICE, THE COST SHALL BE INCLUDED IN THE BASIC WORK ZONE TRAFFIC CONTROL ITEM 619.01.

ALL FLAGGER STATIONS SHALL BE ENHANCED WITH ADDITIONAL CONES AND A FLAG TREE AS SHOWN ON THE WORK ZONE TRAFFIC CONTROL (619 SERIES) STANDARD SHEETS AND AS DIRECTED BY THE ENGINEER. THE FLAG TREE SHALL MEET THE REQUIREMENTS OF SECTION 6F.62 OF THE MUTCD EXCEPT THAT A MINIMUM OF THREE (3) FLAGS ARE REQUIRED. THIS SETUP SHALL BE USED FOR ALL FLAGGER STATIONS EXCEPT THOSE THAT ARE CONSTANTLY ADDITIONS ASSOCIATED WITH THE PROPERTY OF THE PROPERTY MOVING. ALL COSTS ASSOCIATED WITH THESE REQUIREMENTS SHALL BE INCLUDED UNDER THE BASIC WORK ZONE

FLAGGER SIGNS ARE TO BE USED ONLY WHEN A FLAGGER IS ACTUALLY PRESENT AND VISIBLE TO THE MOTORIST. FLAGGER SIGNS SHALL BE COVERED OR REMOVED AT ALL OTHER TIMES.

ENFORCEABLE REDUCED REGULATORY SPEED LIMIT AND ADVISORY SPEED SIGNS (DAILY OPERATION INSTALLATIONS):

ALL REDUCTIONS IN REGULATORY SPEED LIMITS AND ADVISORY SPEEDS ASSOCIATED WITH WORK ZONES MUST BE APPROVED, IN WRITING AND IN ADVANCE, BY THE REGIONAL TRAFFIC ENGINEER OR HIS/HER DESIGNEE.

THE CONTRACTOR MAY REQUEST APPROVAL OF REDUCED REGULATORY SPEED LIMITS AND ADVISORY SPEEDS FOR SHORT-TERM STATIONARY WORK ZONES WHERE SUCH PROVISIONS ARE NOT OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS. REQUESTS MUST BE SUBMITTED THROUGH THE ENGINEER TO THE REGIONAL TRAFFIC ENGINEER A MINIMUM OF FOURTEEN (14) CALENDAR DAYS IN ADVANCE OF THE SCHEDULED IMPLEMENTATION OF ANY WORK ZONE CONTAINING THE REQUESTED REDUCED REGULATORY SPEED LIMITS AND/OR ADVISORY SPEEDS.

GENERALLY, TO QUALIFY FOR A REDUCTION IN REGULATORY SPEED LIMIT AT A SHORT-TERM STATIONARY WORK ZONE, THE WORK ZONE MUST CONTAIN AN ACTIVITY AREA THAT IS GREATER THAN 1/2 MILE ON A HIGHWAY WITH A PRECONSTRUCTION POSTED SPEED LIMIT OF 55 MPH OR GREATER AND BE OF A DURATION EXCEEDING FOUR (4) HOURS WHERE WORKERS ARE ON FOOT IN THE ACTIVITY AREA AND ARE NOT PREDOMINANTLY SEPARATED FROM TRAFFIC BY A POSITIVE BARRIER. IF APPROVED, THE REDUCED REGULATORY SPEED SHALL BE NO MORE THAN 10 MEDIA PER CONCENTRATION POSTED 1 FOR THE PROPERTY OF THE P MPH BELOW THE PRECONSTRUCTION POSTED SPEED LIMIT.

TO QUALIFY FOR AN ADVISORY SPEED AT A SHORT-TERM STATIONARY WORK ZONE, HAZARDOUS WORK ZONE CONDITIONS MUST EXIST THAT WARRANT A LOCALIZED REDUCTION IN SPEED, SUCH CONDITIONS INCLUDE, BUT ARE NOT LIMITED TO, NARROW LANES, BUMPS, GROOVED PAVEMENT, LOW OR NO SHOULDERS, ROADWAY DROP-OFFS, POOR ROADWAY SURFACE, POOR SIGHT DISTANCE, GEOMETRIC CONSTRAINTS AND EXPOSED WORKERS ADJACENT TO ACTIVE

IF A REDUCTION IN REGULATORY SPEED LIMIT AND/OR ADVISORY SPEED IS APPROVED, THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN REGULATORY SPEED LIMIT AND/OR ADVISORY SPEED SIGNS IN ACCORDANCE WITH SECTIONS 619-3.02 H 4 AND 5 OF THE STANDARD SPECIFICATIONS AND THE MUTCD, AND AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE USED IN CONJUNCTION WITH THE LANE CLOSURE OR OTHER WORK ZONE TRAFFIC CONTROL SIGNS AND SHALL BE COVERED OR REMOVED WHEN THE WORK ZONE TRAFFIC CONTROL PATTERN IS REMOVED EACH DAY, ANY EXISTING REGULATORY SPEED LIMIT SIGNS WITHIN THE WORK ZONE SHALL BE COVERED ON THE THAT THE WORK ZONE TRAFFIC CONTROL SIGNS AND SHALL BE COVERED. DURING THE TIME THE WORK ZONE TRAFFIC CONTROL IS IN PLACE AND UNCOVERED WHEN THE WORK ZONE TRAFFIC

REDUCTIONS IN REGULATORY SPEED LIMITS AND ADVISORY SPEEDS SHALL NOT BE PERMITTED FOR MOBILE OR SHORT DURATION WORK ZONES.

THE COST OF UTILIZING APPROVED REGULATORY OR ADVISORY SPEED ZONE SIGNS, AND COVERING EXISTING SIGNS, SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL 619.01.

REPLACEMENT OF GUIDE RAIL, MEDIAN RAIL AND/OR BRIDGE RAIL:

GUIDE RAIL: UNLESS OTHERWISE SHOWN IN THE PLANS OR APPROVED BY THE ENGINEER, THE CONTRACTOR SHALL REMOVE ONLY THE GUIDE RAIL THAT CAN BE REPLACED WITHIN ITHE SAME WORK SHIFT OR X (WHERE X<14) CALENDAR DAYS). IF GUIDE RAIL IS NOT REPLACED WITHIN THE SPECIFIED TIME THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES IN THE AMOUNT OF \$5,000 PER CALENDAR DAY PER LOCATION UNTIL THE GUIDE RAIL IS REPLACED. IN ADDITION, THE ENGINEER MAY DIRECT THE CONTRACTOR TO STOP ALL OTHER WORK UNTIL THE CUIDE PART IS DEPLACED.

IN ACCORDANCE WITH SECTION 619-3.02 J 6 OF THE STANDARD SPECIFICATIONS, THE CONTRACTOR SHALL USE DRUMS, TALL CONES, EXTRA TALL CONES, TEMPORARY TUBULAR MARKERS, VERTICAL PANELS, OVERSIZED VERTICAL PANELS, TYPE II CONSTRUCTION BARRICADES AND/OR TYPE III CONSTRUCTION BARRICADES TO DELINEATE THE EDGE OF SHOULDER IN LOCATIONS WHERE GUIDE RAIL IS REMOVED. THE CENTER-TO-CENTER SPACING BETWEEN CHANNELIZING DEVICES SHALL NOT EXCEED 80 FEET WHERE THE SHOULDER WIDTH IS 4 FEET OR GREATER, AND SHALL NOT EXCEED 40 FEET WHERE THE SHOULDER WIDTH IS LESS THAN 4 FEET. A MINIMUM OF THREE (3) DEVICES SHALL BE PROVIDED FOR EACH INDIVIDUAL RUN OF GUIDE RAIL THAT IS REMOVED.

TRAFFIC SIGNAL MODIFICATIONS:

REFER TO TRAFFIC SIGNAL NOTES AND TRAFFIC SIGNAL PLANS FOR WZTC WORK INVOLVING THE MODIFICATIONS OF EXISTING TRAFFIC SIGNALS.

PUBLIC ACCOMMODATIONS WITHIN CONTRACT LIMITS NOTES:

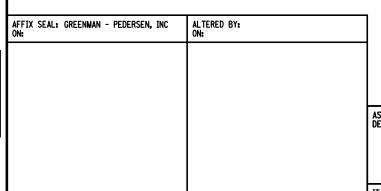
AT THE END OF EACH WORK DAY, ALL EXCAVATED MATERIAL SHALL BE TAKEN FROM THE SITE TO A LOCATION AT THE END OF EACH MORE DAT, ALL EXCAVALED MATERIAL SHALL BE ALEAN FROM THE SITE TO A LUCATION APPROVED BY THE ENGINEER AND ALL CONSTRUCTION EQUIPMENT, MATERIALS, AND OTHER OBSTRUCTIONS SHALL BE REMOVED FROM DRIVEWAYS, SIDEWALKS OR PEDESTRIAN WAYS THAT ARE OPEN TO TRAFFIC, CONSTRUCTION MATERIALS, VEHICLES, EQUIPMENT, DEBRIS, TEMPORARY SIGN SUPPORTS OR OTHER MATERIALS SHALL NOT BE PLACED OR STORED ON OPEN DRIVEWAYS, SIDEWALKS OR PEDESTRIAN WAYS UNLESS EXPRESSLY SHOWN IN THE CONTRACT DOCUMENTS OR APPROVED BY THE ENGINEER.

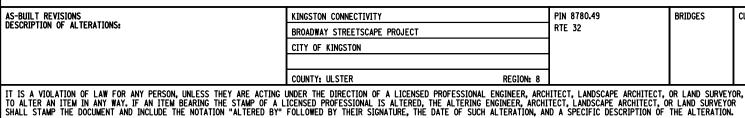
UPON COMPLETION OF THE WORK AT EACH LOCATION, THE CONTRACTOR SHALL REMOVE ALL REMAINING MATERIAL AND EQUIPMENT AND SHALL LEAVE THE AFFECTED AREA(S) IN A NEAT CONDITION.

WHERE EXCAVATIONS OR OTHER WORK OCCUR ON OR NEAR SIDEWALKS OR OTHER PEDESTRIAN WAYS, THE CONTRACTOR SHALL PROVIDE A SAFE AND ORDERLY PEDESTRIAN PASSAGE THAT COMPLIES WITH ADA STANDARDS AROUND OR THROUGH THE WORK AREA. THE PEDESTRIAN PASSAGE SHALL NOT SUBJECT PEDESTRIANS TO HAZARDS FROM TRAFFIC OR CONSTRUCTION OPERATIONS NOR CAUSE THE PEDESTRIANS TO WALK UPON UNSUITABLE OR

DRIVEWAY/PUBLIC INGRESS AND EGRESS:

THE CONTRACTOR SHALL PROVIDE BUSINESS OWNERS AND PROPERTY OWNERS WITH PROPER ACCESS TO AND MINIMUM WIDTHS FOR THEIR DRIVEWAYS IN ACCORDANCE WITH THE "POLICY AND STANDARDS FOR ENTRANCES TO STATE HIGHWAYS" AND A.C.L.U. AT ALL TIMES FOR THE DURATION OF THE CONTRACT. DRIVEWAYS SHALL BE MAINTAINED AND DELINEATED BY MEANS OF SIGNS, CONES AND/OR DRUMS SHALL BE INCLUDED IN THE BID PRICE FOR ITEM 619.01 BASIC MAINTENANCE AND PROTECTION OF TRAFFIC







CULVERTS

BRIDGES



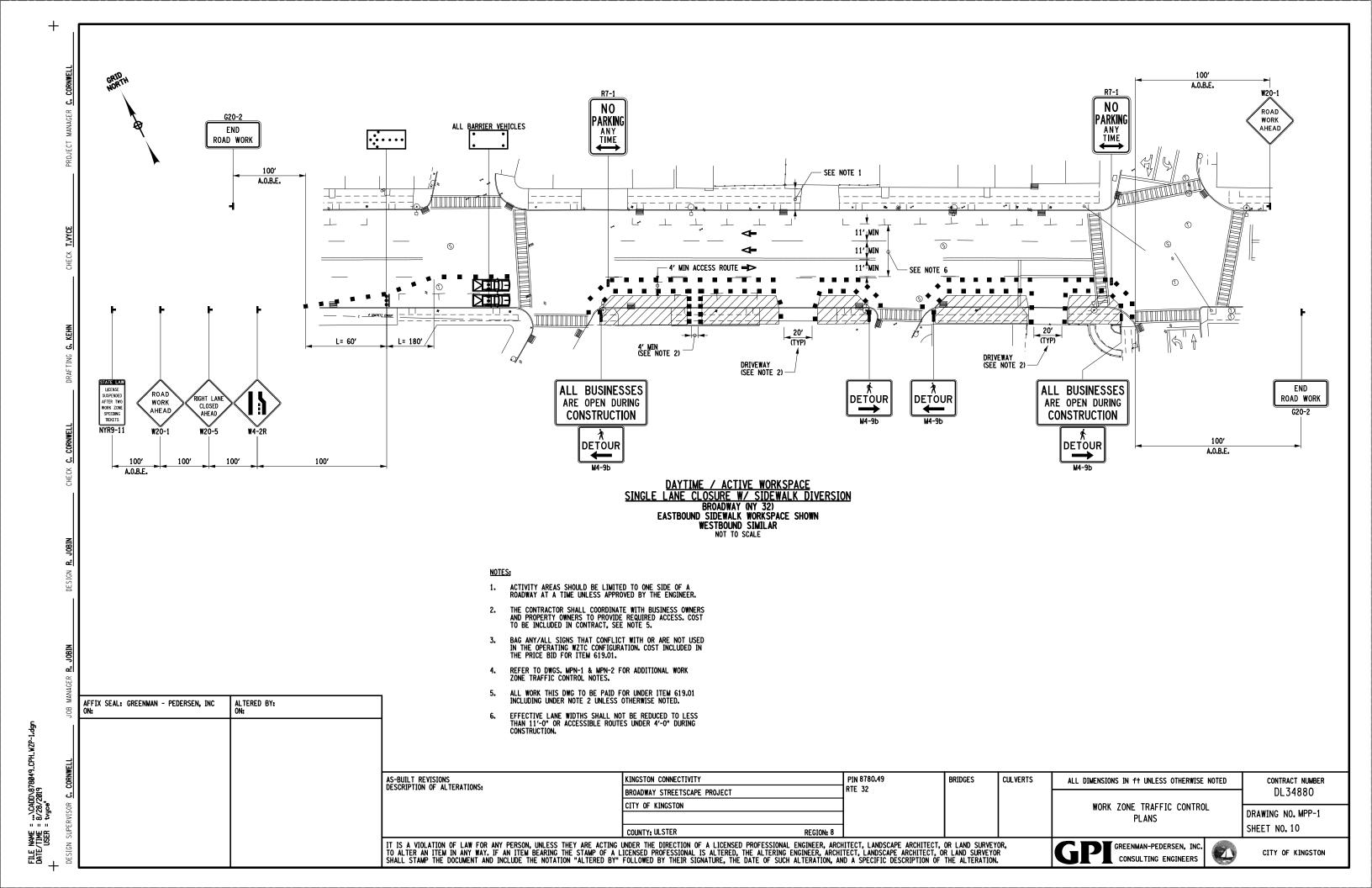
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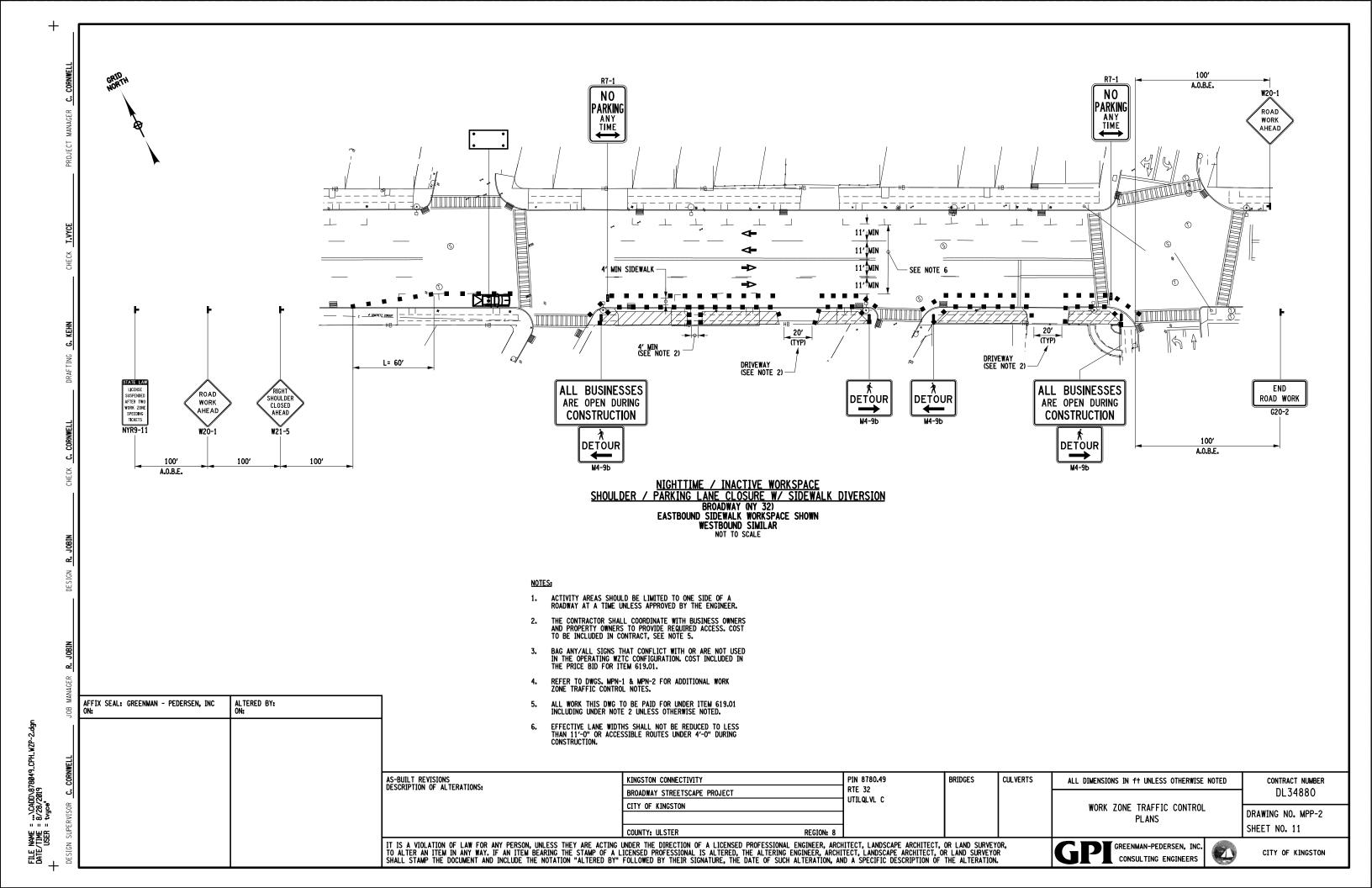
WORK ZONE TRAFFIC CONTROL

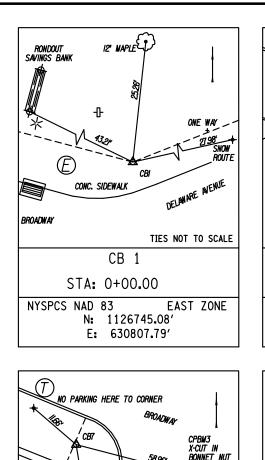
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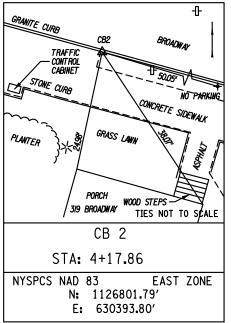
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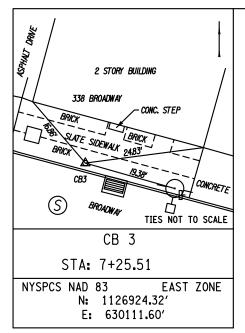
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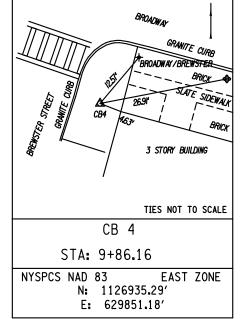


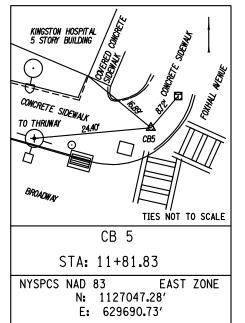


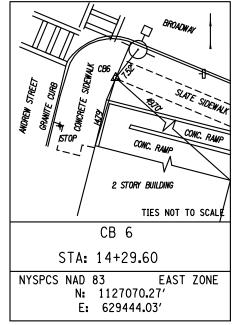


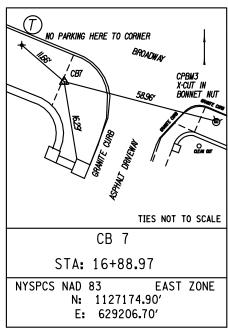


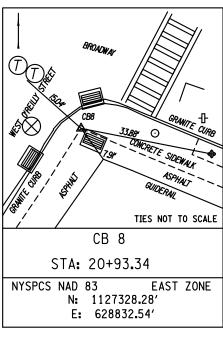


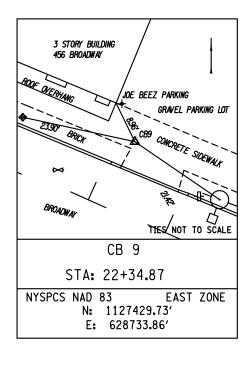


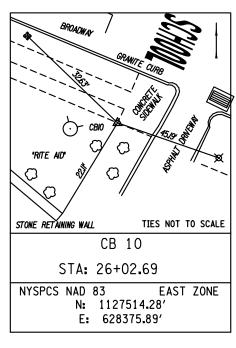


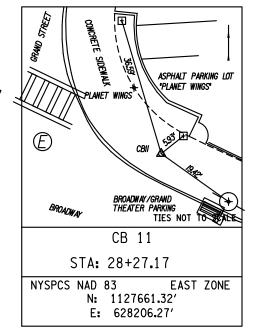






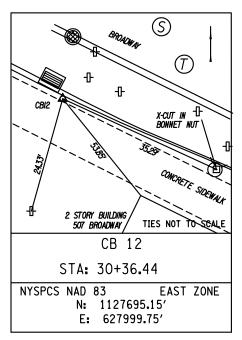


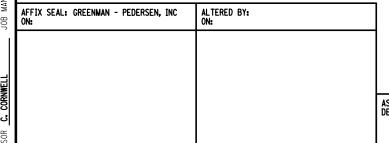




CULVERTS

BRIDGES





NOTE:

1. SEE DWG CLP-1 THROUGH CLP-6 FOR BASELINE ALIGNMENT.

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 KINGSTON CONNECTIVITY RTE 32 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

GREENMAN-PEDERSEN, INC CONSULTING ENGINEERS

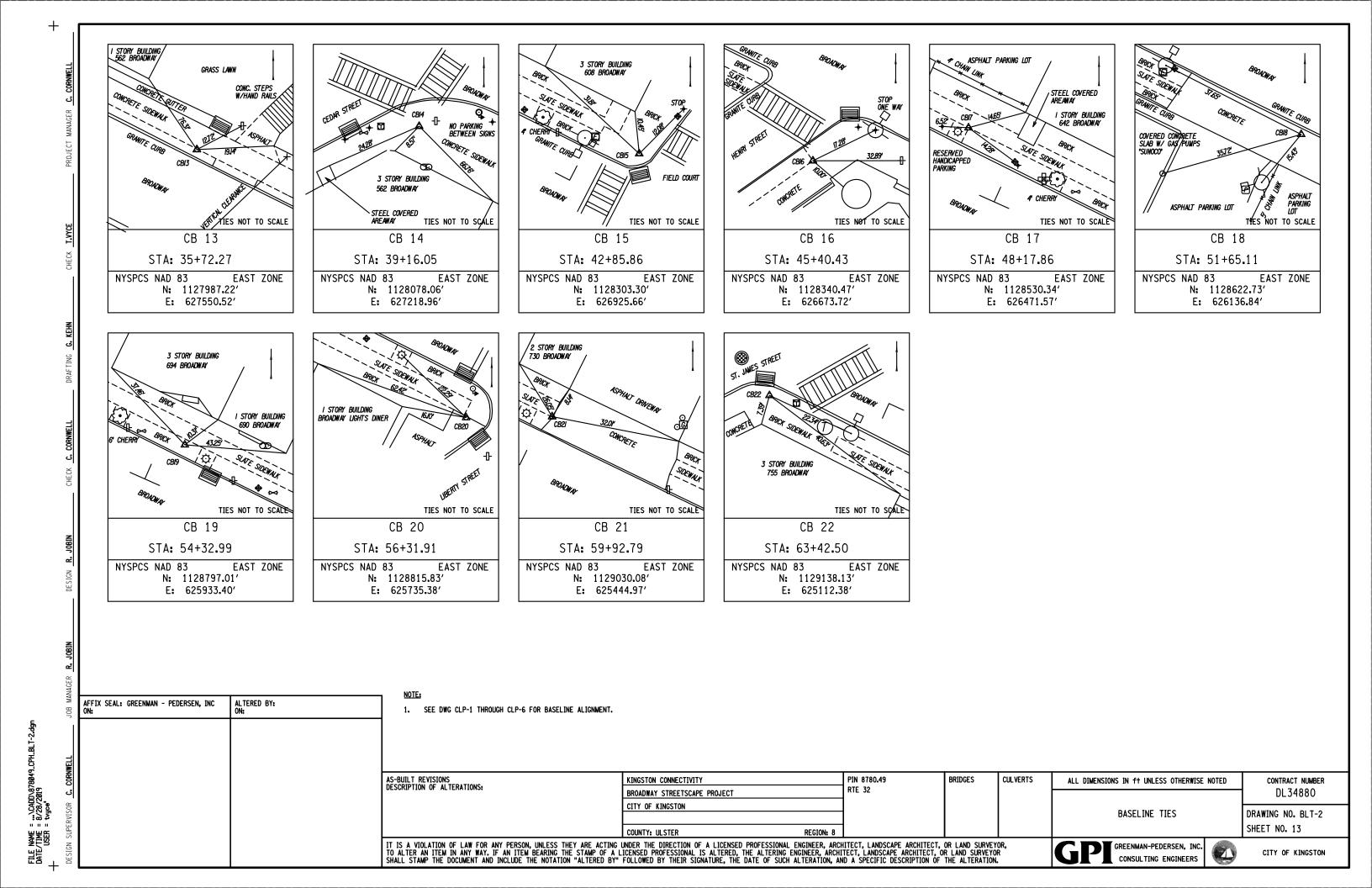
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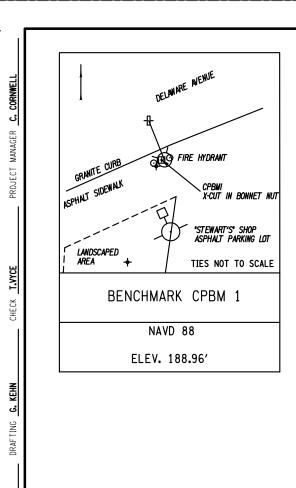
BASELINE TIES

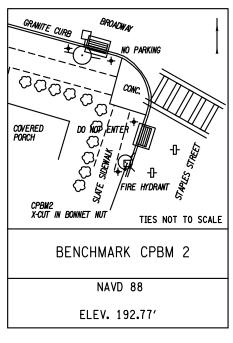
DL34880 DRAWING NO. BLT-1 SHEET NO. 12

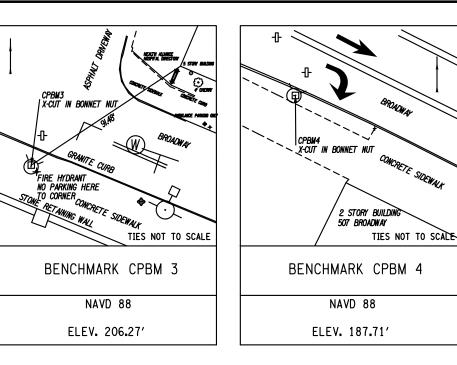
CONTRACT NUMBER

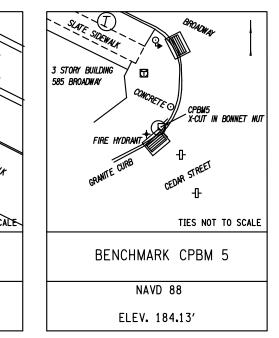
CITY OF KINGSTON

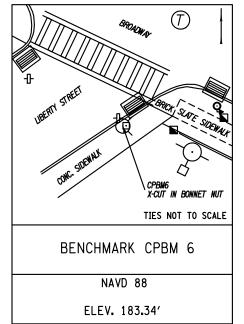


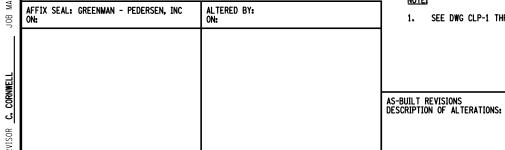












FILE NAME = ...\CADD\878849_CPH_BLT-3.dgn DATE/TIME = 8/28/2019 USER = tvyce'

NOTE:

1. SEE DWG CLP-1 THROUGH CLP-6 FOR BASELINE ALIGNMENT.

CITY OF KINGSTON COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

KINGSTON CONNECTIVITY

BROADWAY STREETSCAPE PROJECT

PIN 8780.49

RTE 32

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

BASELINE TIES

CONTRACT NUMBER DL34880

DRAWING NO. BLT-3 SHEET NO. 14

GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

— В 40+0Ŏ BROADWAY B 60+00 BROADWAY B 61+00 -B 54400 憲 MATCHL TABLE OF MAINTENANCE JURISDICTION - KINGSTON CONNECTIVITY PROJECT **AUTHORITY FOR MAINTENANCE** STATION LIMITS PART NO. DESCRIPTION **GENERAL FEATURES TO BE MAINTAINED** JURISTICTIONAL AGENCY JURISDICTION I-587 @ ALBANY AVE & BROADWAY PAVEMENT, DRAINAGE, CURBING, SIGNAGE, STRIPING, SIDEWALKS, LANDSCAPING, LIGHTING. AND NEW YORK STATE DEPARTMENT OF 1 STA B 1+00 TO STA B 5+30 HIGHWAY LAW, SECTION 340-B TRANSPORTATION - REGION 8 PAVEMENT, DRAINAGE, CURBING, SIGNAGE, STRIPING, SIDEWALKS, LANDSCAPING, LIGHTING. AND 2 STA B 5+30 TO STA B 37+76 BROADWAY CITY OF KINGSTON HIGHWAY LAW, SECTION 349-C SNOW & ICE CONTROL PAVEMENT, DRAINAGE, CURBING, SIGNAGE, STRIPING, SIDEWALKS, LANDSCAPING, LIGHTING. AND HENRY STREET STA B 21+01 TO STA B 21+41 (OFFSET RT) CITY OF KINGSTON HIGHWAY LAW, SECTION 349-C SNOW & ICE CONTROL AFFIX SEAL: GREENMAN - PEDERSEN, INC ALTERED BY: ON: 4 E/W O'REILLY STREET INTERSECTION STA B 45+16 TO STA B 46+80± TRAFFIC SIGNALS CITY OF KINGSTON HIGHWAY LAW, SECTION 349-C FILE NAME = ...\CADD\878849.CPH_MJP-2.dgr DATE/TIME = 8/28/2019 USER = tvyce' 5 FOXHALL AVE INTERSECTION STA B 54+70 TO STA B 55+22± TRAFFIC SIGNALS CITY OF KINGSTON HIGHWAY LAW, SECTION 349-C 6 E/W CHESTER STREET INTERSECTION STA B 61+29 TO STA B 61+78± TRAFFIC SIGNALS CITY OF KINGSTON HIGHWAY LAW, SECTION 349-C AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 BRIDGES CULVERTS KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT UTILQLVL C CITY OF KINGSTON MAINTENANCE JURISDICTION PLANS DRAWING NO. MJP-2 SHEET NO. 16 COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC. CITY OF KINGSTON CONSULTING ENGINEERS

E = ...\CADD\878049_CPH_MST-1.dgn E = 8/28/2019 ? = tvuce

AFFIX SEAL: GREENMAN - PEDERSEN, INC ALTERED BY:

B 25+80 RT UD 4-24 B 25+24 UD 5-5 B 25+82 - B 27+12 RT 128 UD 5-6 B 25+82 B 27+03 LT 118 UD 5-8 B 27+99 B 29+27 LT 147 B 29+50 RT UD 5-11 B 28+00 150 UD 5-16 B 29+66 - B 30+66 RT 99 UD 5-17 B 30+02 - B 30+67 LT 64 17 349 UD 6-4 | B 30+68 B 34+17 RT 15 B 30+70 B 34+19 LT 349 - B 36+49 RT 228 UD 6-10 B 34+21 UD 6-11 B 34+22 - B 36+33 LT 10 211 UD 7-5 | B 36+52 | - | B 37+70 | RT 117 TOTALS: **24**3 183 4703 AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

UNDERDRAIN

B 6+55 LT

B 6+80 RT

B 7+73 LT

B 8+22 RT

B 8+56 LT

B 9+62 RT

B 9+98 LT

B 12+73 LT

B 13+56 LT

B 13+89 RT

B 14+96 LT

- B 15+84 RT

- B 16+41 LT

- B 16+96 RT

B 17+41 LT

B 18+77 LT

B 19+14 RT

B 20+25 LT

B 20+23 RT

B 22+60 RT

B 24+15 LT

B 25+82 LT

- B 20+56 LT

- B 22+46 LT

- B 23+01 LT

- B 23+13 RT

- B 24+96 RT

- B 11+57 LT

- B 12+41 RT

203.02

I.D. NO. STATION TO STATION SIDE

UD 1-2 B 5+80

B 6+10

B 6+85

B 7+12

B 8+08

B 8+50

B 8+86

UD 2-4 B 10+27

UD 2-9 B 11+74

UD 2-11 B 12+25

UD 2-16 B 12+75

UD 2-19 B 12+66

UD 2-22 B 13+73

UD 2-26 B 14+51

UD 3-4 | B 15+17

UD 3-7 B 15+87

UD 3-10 B 17+00

UD 3-17 B 17+74

UD 3-19 B 18+00 UD 3-22 B 19+05

UD 3-27 B 19+65

UD 4-2 B 20+28

UD 4-6 | B 21+31

UD 4-13 B 22+93

UD 4-18 B 23+14

UD 4-15 B 23+30

B 21+55

B 22+53

UD 4-8

UD 4-9

UD 1-6

UD 1-10

UD 1-12

UD 1-18

UD 2-1

ITEM

605.1001

ITEM

605.1601

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					DRAI	NAGE MOD	FICATIONS	<u> </u>		
			ITEM	ITEM	ITEM	ITEM	ITEM	ITEM	ITEM	
I.D. NO.	STATION	SIDE		604.070802	655.0704	655.0705	655.1103	655.1106	655.25010005	DESCRIPTION OF WORK
			(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	
DM 1-1	B 5+87	LT	1		1					TOP REBUILD
DM 1-3	B 6+24	RT	1						1	TOP REBUILD
DM 1-8	B 7+18	LT	1				1			TOP REBUILD
DM 1-13	B 7+95	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 1-16	B 8+46	LT	1				1			TOP REBUILD
DM 2-2	B 9+57	LT	1				1			TOP REBUILD
DM 2-32	B 9+97	RT	1						1	TOP REBUILD
DM 2-5	B 10+66	LT	1				1			TOP REBUILD
DM 2-31	B 11+25	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 2-30	B 11+35	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 2-29	B 11+60	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 2-7	B 11+75	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 2-28	B 11+80	LT		1			1			TOP REBUILD + WALL REPAIRS
DM 2-27	B 11+85	LT		1					1	TOP REBUILD + WALL REPAIRS
DM 2-10	B 12+25	LT		1			1			TOP REBUILD + WALL REPAIRS
DM 2-17	B 13+25	LT		1			1			TOP REBUILD + WALL REPAIRS
DM 2-23	B 14+40	LT		1			1			TOP REBUILD + WALL REPAIRS
DM 3-29	B 15+04	LT	1						1	TOP REBUILD
DM 3-2	B 15+40	LT		1			1			TOP REBUILD + WALL REPAIRS
DM 3-5	B 15+90	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 3-32	B 16+45	LT		1			1			TOP REBUILD + WALL REPAIRS
DM 3-9	B 16+95	LT	1				1			TOP REBUILD
DM 3-31	B 17+10	RT	1						1	TOP REBUILD
DM 3-30	B 17+40	RT	1						1	TOP REBUILD
DM 3-13	B 17+75	LT	1				1			TOP REBUILD
DM 3-29	B 19+25	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 3-23	B 19+65	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 3-24	B 19+70	LT	1				1			TOP REBUILD
DM 4-28	B 20+20	LT	1						1	TOP REBUILD
DM 4-1	B 20+25	LT	1				1			TOP REBUILD
DM 4-29	B 20+75	LT		1					1	TOP REBUILD + WALL REPAIRS
DM 4-30	B 20+90	RT	1						1	TOP REBUILD
DM 4-31	B 21+24	RT	1						1	TOP REBUILD
DM 4-3	B 21+30	LT	1						1	TOP REBUILD
DM 4-10	B 22+70	LT	1				1			TOP REBUILD
DM 4-12	B 23+00	RT	1						1	TOP REBUILD
DM 4-16	B 23+75	LT	1					1		TOP REBUILD
DM 4-27	B 24+25	LT	1					1		TOP REBUILD
DM 4-25	B 24+70	LT	1					1		TOP REBUILD
DM 5-1	B 25+80	RT		1					1	TOP REBUILD + WALL REPAIRS
DM 5-2	B 25+80	LT		1			1			TOP REBUILD + WALL REPAIRS
DM 5-23	B 27+25	LT	1						1	TOP REBUILD
DM 5-22	B 27+60	LT		1					1	TOP REBUILD + WALL REPAIRS
DM 5-14	B 29+20	RT	1						1	TOP REBUILD
DM 6-16	B 31+07	RT	1						1	TOP REBUILD
DM 6-15	B 31+27	LT	1						1	TOP REBUILD
DM 6-14	B 32+91	LT	1						1	TOP REBUILD
DM 6-7	B 34+20	LT	1			1			-	TOP REBUILD
DM 7-1	B 36+35	LT	1			1				TOP REBUILD
DM 7-3	B 36+60	LT		1				1		TOP REBUILD + WALL REPAIRS
		TALS:	30	20	1	2	16	4	27	

DRAINAGE MODIFICATIONS

DRAINAGE MODIFICATION NOTES:

- ITEM 655.0704 FURNISH AND INSTALL NEW CAST FRAME F1, UNMOUNTABLE CURB BOX, RETICULINE GRATE G1.
- ITEM 655.0705 FURNISH AND INSTALL NEW CAST FRAME F2, UNMOUNTABLE CURB BOX, RETICULINE GRATE G2.
- ITEM 655.1103 FURNISH AND INSTALL NEW WELDED FRAME AND RETICULINE GRATE 3.
- ITEM 655.1106 FURNISH AND INSTALL NEW WELDED FRAME AND RETICULINE GRATE 6.
- ITEM 655.25010005 REFURNISH AND REINSTALL EXISTING FRAME AND GRATE ASSEMBLY.

			KINGSTON	CONNECTIV	ΙΤΥ						PIN 8780.49
			BROADWA	Y STREETSC	APE	PR	OJECT				RTE 32
			CITY OF	KINGSTON							
			COUNTY:	ULSTER					REGION	8 :	
THEY A	\RE	ACTING	UNDER THE	DIRECTION	0F	ΑL	ICENSED	PROFESSIONAL	ENGINEER.	ARCH	ITECT. LANDSCAI

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

CONTRACT NUMBER DL34880

DRAWING NO. MST-1 SHEET NO. 17

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS



CITY	0F	KINGSTON

				DRAI	NAGE CLEANIN	IG
STATION	то	STATION	SIDE	ITEM 621.03 (LF)	ITEM 621.04 (EA)	DESCRIPTION OF WORK
B 5+31	-	B 12+00	LT	184	8	ST. JAMES ST. TO ELMENDORF ST.
B 6+13	-	B 11+50	RT	687	9	ST. JAMES ST. TO LIBERTY ST.
B 12+00	-	B 16+61	LT	179	5	ELMENDORF ST. TO DOWNS ST.
B 11+50	-	B 14+22	RT	373	5	LIBERTY ST. TO FRANKLIN ST.
B 14+22	-	B 17+50	RT	533	6	FRANKLIN ST. TO VAN BUREN ST.
B 16+61	-	B 21+00	LT	241	6	DOWNS ST. TO O'NEIL ST.
B 17+50	-	B 19+50	RT	281	5	VAN BUREN ST. TO VAN DEUSEN ST.
B 19+50	-	B 21+00	RT	260	5	VAN DEUSEN ST. TO HENRY ST.
B 21+00	-	B 24+50	LT	154	4	O'NEIL ST. TO FIELD CT.
B 21+00	-	B 27+50	RT	909	10	HENRY ST. TO CEDAR ST.
B 24+50	-	B 27+50	LT	111	5	FIELD CT. TO CORNELL ST.
B 27+50	-	B 32+00	RT	420	8	CEDAR ST. TO GREENKILL AVE.
B 27+50	-	B 32+00	LT	164	4	CORNELL ST. TO RAILROAD AVE.
B 32+00	-	B 37+72	RT	704	10	GREENKILL AVE. TO PINE GROVE AVE.
B 32+00	-	B 37+72	LT	197	6	RAILROAD AVE. TO DEDERICK ST.
		TOT	ALS:	5397	96	

DRAINAGE STRUCTURES

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I.D. NO. STATION SIDE

R 1-19A | B 8+96 | RT

R 2-21A | B 13+78 | RT

R 2-24A | B 14+51 | RT

R 3-12A | B 17+72 | RT

R 3-18A | B 18+68 | LT

R 3-20A B 19+13 RT

R 4-5A | B 21+84 | RT

R 4-20A B 24+41 RT

R 5-7A | B 27+13 | RT

R 5-15A B 29+21 LT

R 5-20A B 30+65 RT

R 5-21A B 30+67 LT

R 6-2A B 32+21 RT

R 6-3A | B 32+25 | LT

R 6-14A B 33+08 RT

R6-14B B 33+18 RT

R 6-6A B 34+21 RT

R 7-2A B 36+52 RT

DRAINAGE REMOVALS

I.D. NO. STATION

R 1-19 | B 08+96 | RT | 15 R 2-21 | B 13+78 | RT | 15 R 2-24 B 14+51 RT

R 3-12 B 17+72 RT R 3-18 B 18+68 LT 15 R 3-20 B 19+13 RT 15 R 4-5 B 21+84 RT

R 4-20 B 24+41 RT

R 5-7 B 27+13 RT

R 5-21 B 30+67 LT R 6-2 B 32+21 RT 19

R 6-14B B 33+18 RT

R 6-6 B 34+21 RT R 7-2 B 36+52 RT 19

R 5-15 B 29+21 LT 19 R 5-20 B 30+65 RT 19

R 6-3 B 32+25 LT 19

R 6-14A B 33+08 RT 19

ITEM

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19

19

TOTALS: 306

SIDE 206.0201 655.16000011

TOTALS:

ITEM 203.07 | ITEM 604.300611 | ITEM 604.301411 | ITEM 655.1103

(EA)

18

54

ш	'DDANT DEDI	CAEMENT T	ARIE
STATION	OFFSET	SIDE	ITEM 663.1301 (E/
B 11+61	45.9'	RT	1
B 11+67	31.9'	LT	1
B 19+49	46.9'	RT	1
B 20+85	53.9'	RT	1
B 24+65	43.4'	LT	1
B 26+07	44.6'	LT	1
B 27+18	48.1'	RT	1
B 36+91	30.2'	RT	1
		TOTAL:	8
*CITY OF KIN	NGSTON SHA	LL COORDIN	ATE HYDRAN
REPL	ACEMENTS \	NITH CONTR	ACTOR

AFFIX SEAL: GREENMAN - PEDERSEN, INC

B 20+44 RT B 20+18 B 20+44 B 20+87 RT 43 405 ALTERED BY:

SIDEWALK TABLE

ARFA

(SQFT)

1551

376

434

141

188

378

388

335

890

307

236

233

876

141

501

286

681

266

716

123

330

139

635

559

233

334

235

426

1002

165

437

216

470

263

1097

115

267

405

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412

717

501

THICKNESS

(FT)

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LOCATION

B 6+55

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B 7+49

B 7+75

B 8+05

B 8+57

B 8+83

B 9+99

B 10+23

B 10+55

B 10+73

B 11+79

B 5+59

B 5+85

B 6+85

B 7+11

B 8+25

B 8+49

B 9+66

B 9+76

B 10+24

B 10+42

B 11+35

B 12+44

B 12+64

B 13+17

B 13+37

B 13+97

B 13+58

B 13+70

B 14+23

B 14+39

B 14+95

B 15+15

B 16+50

B 14+55

B 14+78

B 15+42

B 15+66

B 15+81

B 16+17

B 17+12

B 17+42

B 17+71

B 18+79

B 19+01

B 20+76

B 17+66

B 17+98

B 18+52

B 18+84

B 19+25

B 20+18

STATION TO STATION

B 6+55

B 7+39

B 7+75

B 8+57

B 8+83

B 9+99

B 10+23 B 10+55

B 10+73

B 5+59

B 6+85

B 7+11

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AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

				TOTALS	: 669				
CURB TABLE									
BEGIN STATION	OFFSET	END STATION	OFFSET	SIDE	LENGTH	RADIUS			
B 05+41	20.5'	B 05+57	12.6'	LT	18'				
B 05+57	12.6'	B 05+71	9.4'	LT	14'	32°			
B 05+71	9.4'	B 05+83	9.4'	LT	12'				
B 05+83	9.4'	B 05+83	10.5'	LT	1'				
B 05+73	19.4'	B 11+06	27.0'	LT	533'				
B 11+06	27.0'	B 11+09	30.0'	LT	5'	3°			
B 11+09	30.0'	B 11+08	32.5'	LT	3'				
B 11+08	32.5'	B 11+21	32.8'	LT	13'				
B 11+07	27.0'	B 11+69	28.6'	LT	62'				
B 11+69	28.6'	B 11+79	36.4'	LT	13'	10°			
B 11+79	36.4'	B 11+80	45.8'	LT	10'				
B 12+15	42.4'	B 12+27	29.5'	LT	20'	12°			
B 12+27	29.5'	B 16+40	30.0'	LT	413'				
B 16+40	30.0'	B 16+49	37.6'	LT	14'	10°			
B 16+49	37.6'	B 16+51	48.6'	LT	11'				
B 16+89	52.7'	B 17+08	29.5'	LT	34'	20°			
B 17+08	30.0'	B 20+64	30.0'	LT	356'				
B 20+64	30.0'	B 20+73	37.7'	LT	14'	10°			
B 20+73	37.7'	B 20+76	52.6'	LT	15'				
B 21+24	59.3'	B 21+51	29.5'	LT	46'	28°			
B 21+51	30.0'	B 24+21	30.0'	LT	270'				
B 24+21	30.0'	B 24+27	34.3'	LT	8'	6°			
B 24+27	34.3'	B 24+30	47.5'	LT	14'				
B 24+64	44.1'	B 24+62	36.6'	LT	8'				
B 24+62	36.6'	B 24+68	30.0'	LT	10'	6°			
B 24+68	30.0'	B 27+02	30.0'	LT	234'				
B 26+49	29.5'	B 26+52	32.9'	LT	5'	3°			
B 27+02	30.0'	B 27+21	44.5'	LT	27'	20°			
B 27+21	44.5'	B 27+23	53.5'	LT	9'				
B 27+61	63.2'	B 27+60	55.7'	LT	8'				
B 27+60	55.7'	B 27+84	26.4'	LT	43'	25°			
B 27+84	26.4'	B 29+23	26.4'	LT	139'	200			
B 29+23	26.4'	B 29+42	41.4'	LT	26'	20°			

0 00 .	00	0 0	3.03		0.5	334	0.55	,		0
B 35+	-85	В3	6+07	LT	22	228	0.50	5		0
B 36+	+07	В 3	6+59	LT	52	344	0.33	5		0
							TOTALS	: 669	T :	917
					CU	RB TABLE				
	l	GIN TION	OFFSE	т	END STATION	OFFSET	SIDE	LENGTH	RADIUS	
	B 05	5+41	20.5	'	B 05+57	12.6'	LT	18'		
	B 05	5+57	12.6	'	B 05+71	9.4'	LT	14'	32°]
	B 05	5+71	9.4'		B 05+83	9.4'	LT	12']
	B 05	5+83	9.4'		B 05+83	10.5'	LT	1']
	B 05	5+73	19.4	'	B 11+06	27.0'	LT	533']
	B 11	L+06	27.0	!	B 11+09	30.0'	LT	5'	3°]
	B 11	L+09	30.0	<u> </u>	B 11+08	32.5'	LT	3'		
	B 11	L+08	32.5	'	B 11+21	32.8'	LT	13'		
	B 11	L+07	27.0	'	B 11+69	28.6'	LT	62'		
	B 11	L+69	28.6	'	B 11+79	36.4'	LT	13'	10°	
	B 11	L+79	36.4	'	B 11+80	45.8'	LT	10'		
	B 12	2+15	42.4	'	B 12+27	29.5'	LT	20'	12°]
	B 12	2+27	29.5	<u> </u>	B 16+40	30.0'	LT	413'		
	B 16	5+40	30.0	'	B 16+49	37.6'	LT	14'	10°	
	B 16	5+49	37.6	<u> </u>	B 16+51	48.6'	LT	11'		
	B 16	5+89	52.7	<u> </u>	B 17+08	29.5'	LT	34'	20°	
	B 17	7+08	30.0	<u> </u>	B 20+64	30.0'	LT	356'		
	B 20)+64	30.0	'	B 20+73	37.7'	LT	14'	10°	
	B 20)+73	37.7	<u> </u>	B 20+76	52.6'	LT	15'		
	B 21	L+24	59.3	<u> </u>	B 21+51	29.5'	LT	46'	28°	
	B 21	L+51	30.0		B 24+21	30.0'	LT	270'		
	B 24	1+21	30.0		B 24+27	34.3'	LT	8'	6°	
	B 24	1+27	34.3		B 24+30	47.5'	LT	14'		
	B 24	1+64	44.1	'	B 24+62	36.6'	LT	8'		
	B 24	1+62	36.6	'	B 24+68	30.0'	LT	10'	6°	
	B 24	1+68	30.0	'	B 27+02	30.0'	LT	234'		
	B 26	5+49	29.5	'	B 26+52	32.9'	LT	5'	3°	
	B 27	7+02	30.0	'	B 27+21	44.5'	LT	27'	20°	
		7+21	44.5	_	B 27+23	53.5'	LT	9'		1
		7+61	63.2		B 27+60	55.7'	LT	8'	ar°	-
		7+60 7+84	55.7 26.4		B 27+84 B 29+23	26.4' 26.4'	LT LT	43' 139'	25°	1
	D Z	104	20.4	. +	D Z3TZ3	20.4	LI	133		1

SIDEWALK TABLE (CONT.)

ARFA

(SQFT)

359

1622

274

125

214

992

917

977

1626

313

882

2235

2241

993

320

533

141

4351

452

164

2343

348

554

THICKNESS

(FT)

0.50

0.33

0.50

0.33

0.50

0.33

0.00

0.33

0.33

0.50

0.33

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608.0101

20

13

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12

20

11

28

28

13

54

29

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

BROADWAY STREETSCAPE PROJECT

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IFNGTH

(FT)

81

36

25

28

163

108

157

178

24

104

261

184

79

23

78

11

807

68

12

405

34

85

SIDE

RT

RT

RT

RT

RT

RT

RT

RT

LT

LT

LT

LT

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RT

RT

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1 T

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LT

LOCATION

B 21+22

B 22+03

B 22+39

B 22+64

B 22+92

B 25+63

B 27+20

B 23+02

B 23+26

B 24+30

B 27+23

B 29+43

B 28+53

B 28+76

B 29+54

B 37+72

B 30+61

B 34+66

B 35+00

B 35+85

STATION TO STATION

B 21+18

B 22+03

B 22+39

B 22+64

B 22+92

B 24+55

B 25+63

B 21+24

B 23+02

B 23+26

B 24+62

B 27+74

B 28+76

B 29+65

B 30+49

B 30+61

B 34+66

B 35+00

608.4	0010008		STATION	OFFSET	STATION
	SF)		B 29+42	41.4'	B 29+44
•	0		B 29+82	50.5'	B 30+01
	0		B 30+01	26.4'	B 36+42
	0		B 36+42	26.4'	B 36+59
	0		B 36+59	43.6'	B 36+59
	0		B 05+43	26.2'	B 09+76
	0		B 09+76	29.0'	B 09+79
	0		B 09+79	32.4'	B 11+11
9	917		B 11+11	33.2'	B 11+17
	0		B 11+17	29.8'	B 11+28
	0		B 11+28	29.5'	B 11+34
	0		B 11+34	40.3'	B 11+30
	0		B 11+59	46.1'	B 11+66
	0		B 11+66	34.6'	B 11+74
	0		B 11+74	30.0'	B 13+85
	0		B 13+85	30.0'	B 13+96
	0		B 13+96	43.7'	B 13+95
	0		B 14+39	42.8'	B 14+43
	0		B 14+43	35.8'	B 14+53
	0		B 14+53	30.0'	B 17+00
	0		B 17+00	30.0'	B 17+10
	0		B 17+10	47.2'	B 17+08
	0		B 17+40	51.5'	B 17+49
	0		B 17+49	34.8'	B 17+58
	0		B 17+58	30.0'	B 19+18
	0		B 19+18	30.0'	B 19+25
	0		B 19+25	39.8'	B 19+24
Ģ	917		B 19+51	41.4'	B 19+54
		•	B 19+54	36.2'	B 19+64
			B 19+64	30.0'	B 20+67
DILLE			B 20+67	30.0'	B 20+87
DIUS			B 20+87	48.3'	B 20+87
			B 21+01	184.3'	B 21+21
32°			B 21+21	146.7'	B 21+30

BEGIN

STATION

B 08+51

B 08+84

B 09+40

B 10+24

B 11+72

B 14+78

B 17+98

B 20+57

OFFSET

39.1'

35.1'

35.9'

38.0'

41.0'

41.7'

41.3'

41.8'

41.0'

BEGIN

ITEM

STATION		STATION	052.	0.02		10.5.00
B 29+42	41.4'	B 29+44	48.4'	LT	7'	
B 29+82	50.5'	B 30+01	26.4'	LT	35'	20°
B 30+01	26.4'	B 36+42	26.4'	LT	641'	
B 36+42	26.4'	B 36+59	43.6'	LT	27'	18°
B 36+59	43.6'	B 36+59	54.3'	LT	11'	
B 05+43	26.2'	B 09+76	29.0'	RT	433'	
B 09+76	29.0'	B 09+79	32.4'	RT	5'	
B 09+79	32.4'	B 11+11	33.2'	RT	132'	
B 11+11	33.2'	B 11+17	29.8'	RT	7'	
B 11+17	29.8'	B 11+28	29.5'	RT	11'	
B 11+28	29.5'	B 11+34	40.3'	RT	15'	8°
B 11+34	40.3'	B 11+30	47.9'	RT	9'	
B 11+59	46.1'	B 11+66	34.6'	RT	13'	
B 11+66	34.6'	B 11+74	30.0'	RT	10'	10°
B 11+74	30.0'	B 13+85	30.0'	RT	211'	
B 13+85	30.0'	B 13+96	43.7'	RT	20'	12°
B 13+96	43.7'	B 13+95	52.0'	RT	8'	
B 14+39	42.8'	B 14+43	35.8'	RT	8'	
B 14+43	35.8'	B 14+53	30.0'	RT	12'	12°
B 14+53	30.0'	B 17+00	30.0'	RT	247'	
B 17+00	30.0'	B 17+10	47.2'	RT	24'	12°
B 17+10	47.2'	B 17+08	51.4'	RT	5'	
B 17+40	51.5'	B 17+49	34.8'	RT	19'	
B 17+49	34.8'	B 17+58	30.0'	RT	10'	10°
B 17+58	30.0'	B 19+18	30.0'	RT	160'	
B 19+18	30.0'	B 19+25	39.8'	RT	14'	8°
B 19+25	39.8'	B 19+24	43.1'	RT	3'	
B 19+51	41.4'	B 19+54	36.2'	RT	6'	
B 19+54	36.2'	B 19+64	30.0'	RT	13'	12°
B 19+64	30.0'	B 20+67	30.0'	RT	103'	
B 20+67	30.0'	B 20+87	48.3'	RT	30'	20°
B 20+87	48.3'	B 20+87	56.0'	RT	8'	
B 21+01	184.3'	B 21+21	146.7'	RT	43'	
B 21+21	146.7'	B 21+30	105.4'	RT	43'	70°
B 21+30	105.4'	B 21+26	50.8'	RT	55'	
B 21+26	50.8'	B 21+46	30.0'	RT	32'	20°
B 21+46	30.0'	B 27+06	30.0'	RT	560'	
B 27+06	30.0'	B 27+18	50.3'	RT	29'	14°
B 27+74	43.6'	B 27+79	35.7'	RT	10'	
B 27+79	35.7'	B 27+99	25.4'	RT	23'	25°
B 27+99	25.4'	B 37+72	25.0'	RT	973'	

RESETTING CURB TABLE

B 08+85

B 09+12

B 09+99

B 10+55

B 12+44

B 13+03

B 15+38

B 18+52

B 20+60

PIN 8780.49

RTE 32

OFFSET

39.4'

35.7'

37.2'

38.7'

41.1'

41.7'

41.2'

41.5'

41.0'

ITEM

609.15 (LF

34'

28'

59'

31'

72'

22'

60'

54'

3'

363'

SIDE

LT

LT

LT

RT

RT

RT

RT

TOTAL:

BRIDGES

RT

CURB TABLE (CONT.)

OFFSET

SIDE LENGTH RADIUS

END

OFFSET

		& DETECT/	ABLE WARNIN	IG TABLE	
LOCATIO	ON	CURB	DWS	RAMP	ITEM
STATION	CIDE	RAMP	PLACEMENT	SIDE/BACK	608.21000003
(+/-)	SIDE	TYPE	PLACEIVIENT	CONFIG.	(SY)
B 11+12 ±	LT	9	6	В	1.1
B 11+34 ±	RT	8	1	Α	1.1
B 11+61 ±	LT	11	1	Α	1.1
B 11+68 ±	RT	1	2	Α	1.1
B 11+77 ±	LT	1	2	Α	1.1
B 11+87 ±	RT	11	1	Α	1.1
B 12+18 ±	LT	8	1	Α	1.1
B 13+95 ±	RT	3	3	Α	2.3
B 13+96 ±	LT	10	1	Α	1.1
B 14+43 ±	RT	3	6	Α	1.3
B 16+48 ±	LT	1	2	Α	1.1
B 16+92 ±	LT	7	1	Α	1.1
B 17+03 ±	RT	7	1	Α	1.1
B 17+05 ±	LT	7	1	Α	1.1
B 17+11 ±	RT	7	1	Α	1.1
B 17+51 ±	RT	1	2	Α	1.1
B 19+24 ±	RT	1	2	Α	1.1
B 19+55 ±	RT	1	2	Α	1.1
B 20+28 ±	LT	11	1	Α	1.1
B 20+61 ±	LT	7	1	Α	1.1
B 20+65 ±	RT	1	1	Α	1.1
B 20+74 ±	LT	7	2	Α	1.1
B 20+87 ±	RT	7	1	Α	1.1
B 21+26 ±	LT	1	1	Α	1.1
B 21+27 ±	RT	7	1	Α	1.1
B 21+49 ±	LT	1	1	А	1.1
B 21+51 ±	RT	7	1	Α	1.1
B 24+19 ±	RT	11	1	Α	1.1
B 24+19 ±	LT	7	1	Α	1.1
B 24+24 ±	LT	7	2	А	1.1
B 24+63 ±	LT	1	2	Α	1.1
B 26+55 ±	LT	9	6	В	1.1
B 27+03 ±	RT	7	1	А	1.1
B 27+17 ±	LT	4	6	А	3.7
B 27+17 ±	RT	1	2	Α	1.1
B 27+66 ±	LT	1	2	Α	1.1
B 27+82 ±	RT	7	2	А	1.1
B 27+91 ±	RT	7	2	А	1.1
B 27+93 ±	LT	11	1	Α	1.1
B 29+35 ±	LT	1	2	А	1.1
B 29+84 ±	LT	8	6	A	1.1
B 36+53 ±	LT	1	2	A	1.1
				TOTAL:	50.6

CURB RAMP

*SEE NYSDOT STANDARD SHEET 609 FOR CURB TYPES

(DWS) - DETECTABLE WARNING SURFACE

CULVERTS ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER MISCELLANEOUS TABLES DRAWING NO. MST-2 SHEET NO. 18

GREENMAN-PEDERSEN, INC CONSULTING ENGINEERS

CITY OF KINGSTON

DL34880

FILE NAME = ...\CADD\878849.CPH_MST-3.dgn DATE/TIME = 8/28/2819 USER = tvyce*

AFFIX SEAL: GREENMAN - PEDERSEN, INC

FROM STATION

B 5+41

B 5+43

B 6+13

B 11+32

B 11+58

B 11+81

B 12+15

B 13+96 B 14+39

B 16+54

TO STATION

B 11+31

B 6+13

B 11+51

B 13+95

B 12+15

B 16+52

B 14+30

B 17+13

B 16+88

B 16+89 B 20+76 LT 467 B 17+32 RT 26 2" DEPTH B 17+08 B 19+28 B 19+44 18 2" DEPTH RT B 19+51 B 20+88 188 B 20+79 44 2" DEPTH B 21+23 B 20+89 B 21+25 RT 37 2" DEPTH RT 778 B 21+00 B 27+21 B 24+30 381 B 24+31 B 24+63 LT 34 2" DEPTH B 24+62 B 27+23 317 RT 44 B 27+20 B 27+63 2" DEPTH LT 241 B 27+56 B 29+45 B 27+73 B 37+72 RT 1,044 2" DEPTH B 29+47 B 29+81 LT 35 B 29+81 B 36+60 LT 754 B 36+64 B 37+72 LT/RT 179 7,256 TOTAL:

ALTERED BY: ON:

TABLE OF PAVEMENT CUTTING

RT

LT

RT

LT

ITEM 627.50140008

LENGTH (FT)

657

44

22

290 35

499

35

322

35

NOTES

2" DEPTH

2" DEPTH

2" DEPTH

2" DEPTH

	!	SNOW S	STORAGE	ABLE		
	OCATION		LENGTH (FT)	ITEM 608.13000039	ITEM 608.12010011	
STATION T	O STATION	SIDE	(F1)	(SY)	(SF)	
B 5+30	B 5+83	LT	53	22	0	
B 5+43	B 5+50	RT	7	4	0	
B 5+55	B 5+59	RT	4	2	0	
B 5+64	B 6+24	LT	60	25	0	
B 5+99	B 6+33	RT	34	20	0	
B 6+50	B 6+55	LT	5	3	0	
B 6+59	B 6+85	RT	26	14	0	
B 6+83	B 6+99	LT	16	9	0	
B 7+11 B 7+25	B 7+14 B 7+39	RT LT	3 14	7	0	
B 7+25	B 7+67	RT	27	16	0	
B 7+40	B 7+61	LT	12	6	0	
B 7+66	B 7+75	LT	9	5	0	
B 7+73	B 7+96	RT	23	13	0	
B 8+05	B 8+17	LT	12	6	0	
B 8+22	B 8+25	RT	3	2	0	
B 8+43	B 8+48	LT	5	3	0	
B 8+49	B 8+54	RT	5	3	0	
B 8+53	B 8+57	LT	4	2	0	
B 8+80	B 9+02	RT	22	13	0	
B 8+83	B 8+85	LT	2	1	0	
B 9+11	B 9+19	LT	8	5	0	
B 9+24	B 9+49	LT	25	14	0	
B 9+28	B 9+50	RT	22	12	0	
B 9+54	B 9+63	LT	9	6	0	
B 9+55	B 9+66	RT	11	6	0	
B 9+76	B 10+24	RT	48	9	0	
B 9+89	B 10+00	LT	11	6	0	
B 10+24	B 10+38	LT	14	8	0	
B 10+42	B 11+37	RT	95	27	0	
B 10+43	B 10+55	LT	12	7	0	
B 10+73	B 10+93	LT	20	11	0	
B 10+98	B 11+08	LT	10	6	0	
B 11+26	B 11+59	LT	33	15	0	
B 11+26	B 11+35	RT	9	6	0	
B 11+64	B 11+76	LT	12	5	0	
B 11+69	B 11+85	RT	16	6	0	
B 11+90	B 11+95	RT	5	3	0	
B 12+20	B 12+32	LT	12	7	0	
B 12+21	B 12+27	RT	6	3	0	
B 12+32 B 12+59	B 12+44	RT	12	5 9	0	
	B 12+73	LT	14		0	
B 12+64 B 12+93	B 12+88 B 13+17	RT RT	24 24	13 13	0	
B 12+99	B 13+17	LT	31	13	0	
B 13+37	B 13+58	RT	21	12	0	
B 13+56	B 13+58	LT	2	1	0	
B 13+70	B 13+94	LT	24	12	0	
B 13+76	B 13+89	RT	13	7	0	
B 13+99	B 14+09	LT	10	6	0	
B 14+14	B 14+23	LT	9	5	0	
B 14+39	B 14+41	LT	2	1	0	
B 14+45	B 14+55	RT	10	3	0	
B 14+67	B 14+95	LT	28	16	0	
B 14+78	B 15+10	RT	32	18	0	
B 15+15	B 15+40	LT	25	14	0	
B 15+15	B 15+42	RT	27	15	0	
	_	. –	_			

B 15+84

B 16+26 B 16+47 LT 21

B 16+63 RT

B 15+81

B 16+21

B 15+66

B 16+10

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

LT

RT

LT

18

15

11

46

10

0

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

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	SNOW STORAGE TABLE (CONT.)							
	OCATION O STATION	SIDE	LENGTH (FT)	ITEM 608.13000039 (SY)	608.12010011			
B 16+89	B 16+96	LT	7	(31)	(SF)			
B 16+89	B 16+99	RT	10	6	0			
B 16+95	B 17+03	LT	8	4	0			
B 17+04	B 17+11	RT	7	3	0			
B 17+08	B 17+13	LT	5	3	0			
B 17+39	B 17+42	LT	3	2	0			
B 17+54	B 17+69	RT	15	4	0			
B 17+71	B 17+82	LT	11	7	0			
B 18+01	B 18+06	RT	5	3	0			
B 18+08	B 18+20	LT	12	7	0			
B 18+11	B 18+42	RT	31	17	0			
B 18+46	B 18+79	LT	33	18	0			
B 18+47	B 18+52	RT	5	3	0			
B 18+84	B 19+04	RT	20	11	0			
B 19+01	B 19+08	LT	7	3	0			
B 19+10	B 19+23	RT	13	6	0			
B 19+34 B 19+56	B 19+71 B 19+70	LT	37 14	21 6	0			
B 19+56 B 19+75	B 19+70 B 20+10	RT RT	35	20	0			
B 19+73	B 20+10	LT	28	17	0			
B 20+15	B 20+23	RT	3	2	0			
B 20+30	B 20+49	LT	19	12	0			
B 20+44	B 20+61	RT	17	9	0			
B 20+52	B 20+59	LT	7	3	0			
B 20+64	B 20+74	LT	10	8	0			
B 20+69	B 20+86	RT	17	5	0			
B 21+27	B 21+28	RT	1	11	0			
B 21+27	B 21+28	RT	1	18	0			
B 21+27	B 21+28	RT	1	7	0			
B 21+30	B 21+48	RT	18	13	0			
B 21+58	B 21+89	RT	31	12	0			
B 21+87	B 22+12	LT	25	14	0			
B 21+94	B 22+03	RT	9	3	0			
B 22+38	B 22+46	LT	8	5	0			
B 22+39	B 22+64	RT	25	7	0			
B 22+51	B 22+70	LT	19	11	0			
B 22+92	B 24+16	RT	124	34	0			
B 22+96 B 23+26	B 23+01 B 23+79	LT LT	5 53	29	0			
B 24+05	B 24+16	LT	11	6	0			
B 24+03	B 24+27	LT	6	2	0			
B 24+21	B 24+64	RT	43	21	0			
B 24+63	B 24+89	LT	26	14	0			
B 24+94	B 25+14	LT	20	12	0			
B 25+40	B 25+73	LT	33	18	0			
B 25+66	B 26+43	RT	77	43	0			
B 25+78	B 25+94	LT	16	9	0			
B 25+99	B 26+17	LT	18	11	0			
B 26+46	B 26+52	LT	6	5	0			
B 26+62	B 26+74	RT	12	6	0			
B 26+69	B 27+04	LT	35	10	0			
B 26+79	B 27+00	RT	21	10	0			
B 27+05	B 27+16	RT	11	5	0			
B 27+67	B 27+91	LT	24	13	0			
B 27+83	B 27+90	RT	7	3	0			
B 27+95 B 27+96	B 28+19	RT	24	15 25	0			
B 27+96 B 28+45	B 28+40 B 28+53	LT RT	44 8	25 4	0			
B 28+45 B 28+66	B 28+53 B 28+90	RT LT	8 24	13	0			
B 28+66 B 28+76	B 28+90 B 28+92	RT	16	9	0			
B 29+16	B 29+34	LT	18	8	0			
B 29+18	B 29+54	RT	36	20	0			
					•			

B 16+89	B 16+96	LT	7	4	0
B 16+89	B 16+99	RT	10	6	0
B 16+95	B 17+03	LT	8	4	0
B 17+04	B 17+11	RT	7	3	0
B 17+08	B 17+13	LT	5	3	0
B 17+39	B 17+42	LT	3	2	0
				4	
B 17+54	B 17+69	RT	15		0
B 17+71	B 17+82	LT	11	7	0
B 18+01	B 18+06	RT	5	3	0
B 18+08	B 18+20	LT	12	7	0
B 18+11	B 18+42	RT	31	17	0
B 18+46	B 18+79	LT	33	18	0
B 18+47	B 18+52	RT	5	3	0
B 18+84	B 19+04	RT	20	11	0
B 19+01	B 19+08	LT	7	3	0
B 19+10	B 19+23	RT	13	6	0
B 19+34	B 19+71	LT	37	21	0
B 19+56	B 19+70	RT	14	6	0
B 19+75	B 20+10	RT	35	20	0
B 19+97	B 20+25	LT	28	17	0
B 20+15	B 20+18	RT	3	2	0
B 20+30	B 20+49	LT	19	12	0
B 20+44	B 20+61	RT	17	9	0
B 20+52	B 20+59	LT	7	3	0
B 20+64	B 20+74	LT	10	8	0
B 20+69	B 20+86	RT	17	5	0
B 21+27	B 21+28	RT	1	11	0
B 21+27	B 21+28		1	18	0
		RT			
B 21+27	B 21+28	RT	1	7	0
B 21+30	B 21+48	RT	18	13	0
B 21+58	B 21+89	RT	31	12	0
B 21+87	B 22+12	LT	25	14	0
B 21+94	B 22+03	RT	9	3	0
B 22+38	B 22+46	LT	8	5	0
B 22+39	B 22+64	RT	25	7	0
B 22+51	B 22+70	LT	19	11	0
B 22+92	B 24+16	RT	124	34	0
B 22+96	B 23+01	LT	5	3	0
B 23+26	B 23+79	LT	53	29	0
B 24+05	B 24+16	LT	11	6	0
					0
B 24+21	B 24+27	LT	6	2	
B 24+21	B 24+64	RT	43	21	0
B 24+63	B 24+89	LT	26	14	0
B 24+94	B 25+14	LT	20	12	0
B 25+40	B 25+73	LT	33	18	0
B 25+66	B 26+43	RT	77	43	0
B 25+78	B 25+94	LT	16	9	0
B 25+99	B 26+17	LT	18	11	0
B 26+46	B 26+52	LT	6	5	0
B 26+62	B 26+74	RT	12	6	0
B 26+69	B 27+04	LT	35	10	0
B 26+79	B 27+04	RT	21	10	0
					0
B 27+05	B 27+16	RT	11	5	
B 27+67	B 27+91	LT	24	13	0
B 27+83	B 27+90	RT	7	3	0
B 27+95	B 28+19	RT	24	15	0
B 27+96	B 28+40	LT	44	25	0
B 28+45	B 28+53	RT	8	4	0
B 28+66	B 28+90	LT	24	13	0
B 28+76	B 28+92	RT	16	9	0
B 29+16	B 29+34	LT	18	8	0
	,				
B 29+18	B 29+54	RT	36	20	0

PIN 8780.49

RTE 32



ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

CULVERTS

BRIDGES

CITY OF KINGSTON

DRAWING NO. MST-3 SHEET NO. 19

CONTRACT NUMBER

DL34880

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:

ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

	DRIVEWAY TABLE									
DRIVEWAY	L	OCATION		EXISTING	CLASS	w	CORNER	PL	TL	ENTRANCE
	BEGIN STA	END STA	SIDE	MATERIAL		(FT)	ANGLE	(FT)	(FT)	TYPE
DWY-1	B 6+55	B 6+83	LT	AC	С	28'	90°	13.1'	0.0'	3
DWY-2	B 6+85	B 7+11	RT	AC	С	26'	90°	10.7'	0.0'	3
DWY-3	B 7+39	B 7+50	LT	AC	С	11'	90°	12.6'	0.0'	3
DWY-4	B 7+75	B 8+05	LT	AC	С	30'	90°	12.3'	0.0'	3
DWY-5	B 8+25	B 8+49	RT	AC	С	24'	90°	10.7'	0.0'	3
DWY-6	B 8+57	B 8+83	LT	AC	С	26'	90°	12.6'	0.0'	3
DWY-7	B 9+66	B 9+76	RT	AC	С	10'	90°	11.5'	0.0'	3
DWY-8	B 10+00	B 10+24	LT	AC	С	24'	90°	12.4'	0.0'	3
DWY-9	B 10+24	B 10+42	RT	AC	С	18'	90°	7.5'	0.0'	3
DWY-10	B 10+55	B 10+73	LT	AC	С	18'	90°	12.4'	0.0'	3
DWY-11	B 12+44	B 12+65	RT	AC	С	21'	90°	11.2'	0.0'	3
DWY-12	B 13+17	B 13+37	RT	AC	С	20'	90°	11.3'	0.0'	3
DWY-13	B 13+58	B 13+70	LT	AC	С	12'	90°	12.6'	0.0'	3
DWY-14	B 14+23	B 14+39	LT	AC	С	16'	90°	13.0'	0.0'	3
DWY-15	B 14+55	B 14+78	RT	AC	С	23'	90°	11.3'	0.0'	3
DWY-16	B 14+95	B 15+15	LT	AC	С	20'	90°	13.0'	0.0'	3
DWY-17	B 15+42	B 15+66	RT	AC	С	24'	90°	11.3'	0.0'	3
DWY-18	B 15+81	B 16+17	RT	AC	С	36'	90°	11.2'	0.0'	3
DWY-19	B 17+42	B 17+70	LT	AC	С	28'	90°	13.5'	0.0'	3
DWY-20	B 17+66	B 17+98	RT	AC	С	32'	90°	11.2'	0.0'	3
DWY-21	B 18+52	B 18+84	RT	AC	С	32'	90°	10.7'	0.0'	3
DWY-22	B 18+79	B 19+01	LT	AC	С	22'	90°	13.6'	0.0'	3
DWY-23	B 20+18	B 20+44	RT	AC	С	26'	90°	10.4'	0.0'	3
DWY-24	B 22+03	B 22+39	RT	AC	С	36'	90°	10.4'	0.0'	3
DWY-25	B 22+64	B 22+92	RT	AC	С	28'	90°	10.6'	0.0'	3
DWY-26	B 23+02	B 23+26	LT	AC	С	24'	90°	12.7'	0.0'	3
DWY-27	B 28+53	B 28+77	RT	AC	С	24'	90°	13.3'	0.0'	3
DWY-28	B 29+54	B 29+65	RT	AC	С	11'	90°	12.1'	0.0'	3
DWY-29	B 30+49	B 30+61	LT	AC	С	12'	90°	15.6'	0.0'	3
DWY-30	B 34+66	B 35+00	LT	AC	С	34'	90°	13.8'	0.0'	3
DWY-31	B 35+85	B 36+07	LT	AC	С	22'	90°	13.4'	0.0'	3
DWY-32	B 21+13	B 21+24	RT	AC	С	24'	90°	16.9'	0.0'	3
DWY-33	B 5+59	B 5+85	RT	AC	С	26'	90°	10.4'	0.0'	3

JIAIION	OITSET	SIDE	615.01010110 (EA)	615.01010210 (EA)	304.11000008 (CY)	608.0101 (CY)
B 06+22	20.6'	LT	1	1	0.23	0.31
B 07+43	28.5'	RT	1	1	0.23	0.31
B 09+14	23.5'	LT	1	1	0.23	0.31
B 09+57	30.0'	RT	1	1	0.23	0.31
B 11+41	29.4'	LT	1	1	0.23	0.31
B 12+24	31.2'	RT	1	1	0.23	0.31
B 13+01	31.1'	LT	1	1	0.23	0.31
B 13+56	31.1'	RT	1	1	0.23	0.31
B 15+18	31.2'	RT	1	1	0.23	0.31
B 16+38	32.3'	LT	1	1	0.23	0.31
B 18+11	31.2'	LT	1	1	0.23	0.31
B 19+02	31.2'	RT	1	1	0.23	0.31
B 20+35	34.4'	LT	1	1	0.23	0.31
B 21+87	37.3'	RT	1	1	0.23	0.31
B 22+68	31.2'	LT	1	1	0.23	0.31
B 24+62	31.2'	RT	1	1	0.23	0.31
B 26+15	31.2'	LT	1	1	0.23	0.31
B 26+41	31.2'	RT	1	1	0.23	0.31
B 28+38	27.7'	LT	1	1	0.23	0.31
B 28+90	26.7'	RT	1	1	0.23	0.31
B 31+01	27.7'	LT	1	1	0.23	0.31
B 34+52	26.7'	RT	1	1	0.23	0.31
B 35+25	27.7'	LT	1	1	0.23	0.31
		TOTALS:	23	23	5.3	7.1
			BENCH TABLE			
					1751.4	
STATION	OFFSET	SIDE	ITEM	ITEM	ITEM	

LITTER AND RECYCLING RECEPTICAL TABLE

STATION OFFSET SIDE

ITEM

ITEM

	BENCH TABLE									
STATION	OFFSET	SIDE	ITEM 615.08010005 (EA)	ITEM 304.11000008 (CY)	ITEM 608.0101 (CY)					
B 05+99	20.4'	LT	1	0.4	0.5					
B 07+93	29.7'	RT	1	0.4	0.5					
B 09+32	24.7'	LT	1	0.4	0.5					
B 11+18	32.7'	RT	1	0.4	0.5					
B 11+31	28.6'	LT	1	0.4	0.5					
B 12+36	31.9'	RT	1	0.4	0.5					
B 13+10	31.9'	LT	1	0.4	0.5					
B 13+45	31.9'	RT	1	0.4	0.5					
B 14+90	31.9'	RT	1	0.4	0.5					
B 16+33	32.2'	LT	1	0.4	0.5					
B 18+20	31.9'	RT	1	0.4	0.5					
B 20+50	29.9'	LT	1	0.4	0.5					
B 20+53	29.9'	LT	1	0.4	0.5					
B 21+79	37.4'	RT	1	0.4	0.5					
B 23+76	31.9'	LT	1	0.4	0.5					
B 24+45	31.9'	RT	1	0.4	0.5					
B 26+83	32.7'	RT	1	0.4	0.5					
B 26+89	38.8'	LT	1	0.4	0.5					
B 29+21	27.4'	RT	1	0.4	0.5					
B 30+17	28.4'	LT	1	0.4	0.5					
B 35+44	27.4'	RT	1	0.4	0.5					
B 35+57	28.4'	LT	1	0.4	0.5					
		TOTALS:	22	8.8	11.0					

			RRFB TABLE	
STATION	OFFSET	SIDE	NOTES	ITEM 680.82254310 (EA)
B 13+87	32.5'	RT	SOLAR POWERED	1
B 13+89	32.0'	LT	SOLAR POWERED	1
B 16+95	31.8'	RT	SOLAR POWERED	1
B 17+10	32.5'	LT	SOLAR POWERED	1
B 24+12	31.2'	RT	SOLAR POWERED	1
B 24+14	32.3'	LT	SOLAR POWERED	1
			·	•

	METER KIOSK PAD TABLE									
STATION	OFFSET	SIDE	ITEM 304.11000008 (CY)	ITEM 608.0101 (CY)						
B 08+08	23.9'	LT	0.07	0.1						
B 11+16	34.7'	RT	0.07	0.1						
B 13+14	33.9'	LT	0.07	0.1						
B 15+69	33.9'	LT	0.07	0.1						
B 15+70	34.0'	RT	0.07	0.1						
B 18+98	34.0'	RT	0.07	0.1						
B 19+06	33.9'	LT	0.07	0.1						
B 22+99	33.9'	LT	0.07	0.1						
B 24+91	34.0'	RT	0.07	0.1						
B 25+56	33.9'	LT	0.07	0.1						
B 28+69	30.5'	LT	0.07	0.1						
B 31+45	30.4'	LT	0.07	0.1						
B 34+07	30.3'	LT	0.07	0.1						
		TOTALS:	0.96	1.3						

*CITY SHALL INSTALL METER KIOSK ON PAD.

BRIDGES

CULVERTS

PIN 8780.49

RTE 32

METER KIOSK PAD TABLE						
STATION	OFFSET	SIDE	ITEM 304.11000008 (CY)	ITEM 608.0101 (CY)		
B 08+08	23.9'	LT	0.07	0.1		
B 11+16	34.7'	RT	0.07	0.1		
B 13+14	33.9'	LT	0.07	0.1		
B 15+69	33.9'	LT	0.07	0.1		
B 15+70	34.0'	RT	0.07	0.1		
B 18+98	34.0'	RT	0.07	0.1		
B 19+06	33.9'	LT	0.07	0.1		
B 22+99	33.9'	LT	0.07	0.1		
B 24+91	34.0'	RT	0.07	0.1		
B 25+56	33.9'	LT	0.07	0.1		
B 28+69	30.5'	LT	0.07	0.1		
B 31+45	30.4'	LT	0.07	0.1		
B 34+07	30.3'	LT	0.07	0.1		
		TOTALS:	0.96	1.3		

TOTALS:

BIKE RACK TABLE

STATION OFFSET SIDE 615.27020010 (EA)

B 06+14 21.7' LT B 06+73 29.4' RT B 08+88 30.9' RT B 09+41 25.4' LT B 11+49 30.4' LT B 12+80 32.4' RT B 13+21 32.4' LT B 13+80 32.4' LT B 14+84 32.4' LT B 15+31 32.4' RT B 16+16 32.4' LT B 18+32 32.4' RT B 18+56 | 32.4' | LT B 19+92 32.5' RT B 20+04 32.4' LT B 21+68 42.7' RT B 21+99 32.4' LT B 23+66 32.4' LT B 26+05 32.4' LT B 26+14 32.4' RT B 26+22 32.4' RT B 26+30 32.4' RT B 28+01 28.9' LT B 28+04 27.9' RT B 29+36 27.9' RT B 30+05 28.9' LT B 36+24 28.9' LT

FROM STATION	SIDE	ITEM 634.0603 (EA)
B 5+16	RT	1
B 5+52	RT	1
B 6+19	RT	1
B 6+57	RT	1
B 7+16	LT	1
B 7+50	RT	1
B 7+53	LT	1
B 7+91	RT	1
B 8+35	LT	1
B 8+87	LT	1
B 8+87	RT	1
B 9+32	LT	1
B 9+35	RT	1
B 9+73	LT	1
B 10+29	LT	1
B 10+71	RT	1
B 10+95	RT	1
B 11+03	LT	1
B 12+13	RT	1
B 12+87	RT	1
B 12+92	LT	1
B 13+36	LT	1
B 13+63	RT	1
B 14+00	LT	1
B 14+72	LT	1
B 15+00	RT	1
B 15+20	LT	1
B 15+40	RT	1
B 15+68	LT	1
B 16+09	LT	1
B 16+53	RT	1
B 16+98	RT	1
B 17+73	LT	1
B 18+13	LT	1
B 18+25	RT	1
B 18+52	LT	1
B 19+30	LT	1
B 19+71	LT	1
B 19+87	RT	1
B 20+12	LT	1
B 20+33	LT	1
B 21+73	LT	1
B 22+14	LT	1
B 22+54	LT	1
B 22+74	LT	1
B 23+23	RT	1
B 23+48	LT	1
B 23+59	RT	1
B 23+92	LT	1
B 24+00	RT	1
B 24+45	RT	1
B 25+05	LT	1
B 25+45	RT	1
B 25+46	LT	1
B 25+87	RT	1
B 25+90	LT	1
B 26+27	RT	1
B 26+33	LT	1
B 26+69	RT	1
B 26+93	RT	1
B 28+02	LT	1
B 28+44	LT	1
B 28+90	LT	1
	TOTAL:	

CITY OWNED FACILITY AT THE DIRECTION OF THE DPW DIRECTOR ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER

MISCELLANEOUS TABLES

GP GREENMAN-PEDERSEN, INC.
CONSULTING ENGINFERS

DL34880 DRAWING NO. MST-4 SHEET NO. 20

CITY OF KINGSTON

				i I
	COUNTY: ULSTER	REGION: 8		
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A I SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	LICENSED PROFESSIONAL IS ALTERED), THE ALTERING ENGINEER, ARCHIT	TECT, LANDSCAPE ARCHITECT, O	R LAND SURVEYOR

KINGSTON CONNECTIVITY

CITY OF KINGSTON

BROADWAY STREETSCAPE PROJECT

B 35+64 8 B 16+67 B 16+67 22 30 PARKING STRIP - RT B 35+64 16 PARKING STRIP - LT 12 1 B 12+69 B 12+69 20 PARKING STRIP - LT 8 22 B 12+91 B 12+91 12 20 PARKING STRIP - LT 8 1 B 5+64 B 6+55 196 B 18+24 B 18+24 30 PARKING STRIP - RT 1 HATCH W/ SURROUNDING FDGE LINES 196 B 18+44 B 18+44 22 30 B 13+13 B 13+13 12 20 PARKING STRIP - LT 1 B 6+83 B 7+39 HATCH W/ SURROUNDING EDGE LINES 178 178 B 19+72 B 19+72 22 30 PARKING STRIP - RT B 13+35 B 13+35 12 20 PARKING STRIP - LT B 7+50 B 7+75 HATCH W/ SURROUNDING FDGE LINES 94 94 1 B 19+92 22 PARKING STRIP - RT B 13+92 B 13+92 12 22 PARKING STRIP - LT 10 10 B 8+05 B 8+58 HATCH W/ SURROUNDING EDGE LINES 187 187 B 19+92 30 B 20+12 22 30 PARKING STRIP - RT 8 1 B 14+01 B 14+01 12 22 PARKING STRIP - LT 10 1 10 B 8+84 B 9+99 12 HATCH W/ SURROUNDING EDGE LINES 346 1 346 B 20+12 B 14+57 B 14+57 12 20 PARKING STRIP - LT B 10+24 B 10+56 13 13 HATCH W/ SURROUNDING EDGE LINES 133 133 B 23+09 B 23+09 22 30 PARKING STRIP - RT 12 22 30 PARKING STRIP - RT 8 R 14+77 R 14+77 20 PARKING STRIP - LT 1 8 B 10+74 B 11+49 14 19 HATCH W/ SURROUNDING EDGE LINES 230 230 B 23+29 B 23+29 1 383 B 15+35 B 15+35 12 20 B 13+58 20 HATCH W/ SURROUNDING EDGE LINES 383 B 23+51 B 23+51 22 30 PARKING STRIP - RT B 12+27 20 22 B 15+57 B 15+57 12 20 PARKING STRIP - LT 1 8 178 B 23+73 30 B 13+70 B 14+23 20 20 178 22 B 15+79 12 PARKING STRIP - LT 20 1 189 B 23+93 B 23+93 30 PARKING STRIP - RT 1 B 15+79 20 1 B 14+39 B 14+95 20 HATCH W/ SURROUNDING EDGE LINES 189 PARKING STRIP - LT B 25+29 22 30 PARKING STRIP - RT B 16+01 B 16+01 12 20 8 1 B 15+15 B 16+39 20 HATCH W/ SURROUNDING EDGE LINES 344 PARKING STRIP - LT B 25+49 B 25+49 22 30 PARKING STRIP - RT B 17+92 B 17+92 12 20 8 B 17+10 B 17+42 20 20 HATCH W/ SURROUNDING EDGE LINES 132 132 B 25+63 B 25+63 22 B 18+14 B 18+14 12 PARKING STRIP - L B 17+71 B 18+79 20 20 HATCH W/ SURROUNDING EDGE LINES 330 330 B 18+36 B 18+36 12 PARKING STRIP - LT 8 1 B 25+83 B 25+83 22 30 PARKING STRIP - RT 1 20 8 B 19+01 B 20+57 20 21 HATCH W/ SURROUNDING EDGE LINES 423 1 423 B 26+05 B 26+05 22 30 PARKING STRIP - RT B 18+58 B 18+58 12 20 PARKING STRIP - LT 1 B 21+68 B 23+02 20 20 HATCH W/ SURROUNDING EDGE LINES 421 421 B 26+27 B 26+27 22 30 PARKING STRIP - RT 8 1 B 19+25 B 19+25 12 20 PARKING STRIP - LT 1 8 B 23+26 B 24+14 20 19 HATCH W/ SURROUNDING EDGE LINES 278 1 278 B 26+49 B 26+49 22 PARKING STRIP - RT B 19+47 B 19+47 12 20 B 24+68 B 27+07 20 HATCH W/ SURROUNDING EDGE LINES 642 642 21 B 6+01 B 6+05 1 1 WB LANE - LT EDGE LINE 4 1 R 19+69 R 19+69 12 20 PARKING STRIP - 17 1 8 B 28+01 B 29+23 16 16 HATCH W/ SURROUNDING EDGE LINES 355 1 355 B 6+45 WB LANE - LT EDGE LINE 40 40 B 19+91 B 19+91 12 PARKING STRIP - LT B 6+05 1 B 30+01 B 30+49 185 16 16 HATCH W/ SURROUNDING EDGE LINES 185 B 6+45 B 6+49 WB LANE - LT EDGE LINE 4 B 20+13 B 20+13 12 20 PARKING STRIP - IT 8 1 8 B 34+66 1071 1071 1 1 1 B 30+61 16 16 HATCH W/ SURROUNDING EDGE LINES 20 PARKING STRIP - LT B 35+85 HATCH W/ SURROUNDING EDGE LINES 268 268 B 6+89 B 6+91 WB LANE - LT EDGE LINE B 21+90 B 21+90 12 8 B 34+99 16 16 B 7+31 40 B 22+10 12 PARKING STRIP - LT 1 B 36+07 B 36+42 16 16 HATCH W/ SURROUNDING EDGE LINES 138 138 1 B 7+33 1 WB LANE - LT EDGE LINE B 22+32 B 22+32 12 PARKING STRIP - LT 8 1 B 7+31 1 20 14 EDGE LINES - GREEN CONFLICT ZONE B 7+69 WB LANE - LT EDGE LINE 1 14 B 22+54 B 22+54 12 20 PARKING STRIP - LT B 6+55 B 6+55 41 27 9 B 8+11 B 8+51 WRIANE-ITEDGELINE 1 41 B 22+76 B 22+76 12 20 PARKING STRIP - LT 1 8 B 6+56 B 6+82 11 11 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 0.33 104 B 9+95 WB LANE - LT EDGE LINE B 23+52 B 23+52 B 6+83 B 6+83 EDGE LINES - GREEN CONFLICT ZONE 12 20 11 19 B 23+74 B 23+74 12 20 PARKING STRIP - LT 8 1 8 B 7+39 B 7+39 11 19 EDGE LINES - GREEN CONFLICT ZONE 8 1 8 AFFIX SEAL: GREENMAN - PEDERSEN, INC ALTERED BY: B 23+94 B 23+94 12 20 PARKING STRIP - LT B 7+40 B 7+48 11 11 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 9 0.33 8 B 24+88 B 24+88 12 PARKING STRIP - LT B 7+50 B 7+50 19 EDGE LINES - GREEN CONFLICT ZONE AS-BUILT REVISIONS PIN 8780.49 BRIDGES **CULVERTS** KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER DESCRIPTION OF ALTERATIONS: RTE 32 BROADWAY STREETSCAPE PROJECT DL34880 CITY OF KINGSTON MISCELLANEOUS TABLES DRAWING NO. MST-5 SHEET NO. 21 COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC CITY OF KINGSTON CONSULTING ENGINEERS

TABLE OF PAVEMENT MARKINGS - WHITE (CONT.) STATION TO STATION | BEGIN | END LENGTH PAY ITEM DESCRIPTION B 10+30 B 10+50 WB LANE - LT EDGE LINI 20 1 20 B 10+80 R 10+99 8 WRIANE-ITEDGELINE 20 1 20 B 13+50 WB LANE - LT EDGE LINE 114 114 B 12+35 12 12 B 13+78 B 13+92 12 12 WB LANE - LT EDGE LINE 14 1 14 B 14+01 B 14+15 12 12 14 WB LANE - LT EDGE LINE 14 R 14+47 R 14+87 12 12 WB LANE - LT EDGE LINE 40 1 40 78 B 15+23 B 16+01 12 12 WB LANE - LT EDGE LINE 78 1 B 17+34 12 12 WB LANE - LT EDGE LINE 16 16 1 92 B 17+79 B 18+71 12 12 WB LANE - LT FDGE LINE 92 B 19+09 B 20+13 12 12 WB LANE - LT EDGE LINE 104 1 104 12 B 21+66 B 22+94 12 WB LANE - LT EDGE LINE 128 1 128 B 24+06 12 WB LANE - LT EDGE LINE 72 72 B 24+76 B 26+45 12 12 WB LANE - LT EDGE LINE 169 1 169 B 28+09 B 29+15 WB LANE - LT EDGE LINE 107 107 B 30+09 B 30+41 8 8 WB LANE - LT EDGE LINE 32 1 32 B 30+69 B 34+58 389 1 389 WB LANE - LT EDGE LINE B 35+08 8 8 39 1 39 B 35+77 WB LANE - LT EDGE LINE 19 B 36+15 B 36+34 8 1 19 WB LANE - LT EDGE LINE B 6+05 B 6+05 1 PARKING STRIP - LT 8 1 B 6+25 B 6+25 PARKING STRIP - LT 1 B 6+45 B 6+45 B 6+91 B 6+91 1 9 PARKING STRIP - 17 8 1 8 B 7+11 B 7+11 PARKING STRIP - LT B 7+31 B 7+31 1 9 PARKING STRIP - LT 1 8 B 7+52 B 7+52 B 7+72 B 7+72 2 9 PARKING STRIP - 17 1 7 B 8+21 B 8+21 PARKING STRIP - LT 2 10 8 1 B 8+41 B 8+41 10 PARKING STRIP - LT 8 1 2 8 B 9+04 B 9+04 PARKING STRIP - LT 4 12 1 B 9+24 B 9+24 4 12 1 B 9+46 B 9+46 PARKING STRIP - LT 8 1 13 8 B 9+68 B 9+68 PARKING STRIP - LT B 9+88 B 9+88 6 14 PARKING STRIP - LT 8 1 8 PARKING STRIP - LT B 12+49 B 12+49 20 12 8

TABLE OF PAVEMENT MARKINGS - WHITE (CONT.)

DESCRIPTION

PARKING STRIP - LT

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STATION TO STATION | BEGIN

B 25+08

B 25+52

B 25+74

B 25+96

B 26+18

B 26+40

B 28+23

B 28+43

B 28+65

B 28+87

B 29+07

B 30+82

B 31+04

B 31+24

B 31+44

B 31+84

B 32+04

B 32+24

B 32+44

B 32+64

B 32+84

B 33+04

B 33+24

B 33+44

B 33+64

B 33+84

B 34+04

B 34+24

B 34+46

B 35+20

B 35+42

B 25+08

B 25+30

B 25+52

B 25+74

B 25+96

B 26+18

B 26+40

B 28+23

B 28+43

B 28+65

B 28+87

B 29+07

B 30+82

B 31+04

B 31+24

B 31+44

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TABLE OF PAVEMENT MARKINGS - WHITE

DESCRIPTION

FB LANF - RT FDGF LINE

FB LANF - RT FDGF LINE

EB LANE - RT EDGE LINE

EB LANE - RT EDGE LINE

EB LANE - RT EDGE LINE

FRIANE - RT FDGF LINE

EB LANE - RT EDGE LINE

PARKING STRIP - RT

ITEM

685.11

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LENGTH PAY

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STATION TO STATION

B 9+03

B 10+19

B 11+10

B 13+11

B 13+63

B 15+40

B 16+67

B 18+44

B 20+12

B 23+93

B 25+49

B 26+49

B 9+79

B 10+19

B 10+48

B 10+68

B 10+90

B 11+10

B 12+71

B 12+91

B 13+11

B 13+43

B 13+63

B 14+80

B 15+01

B 15+20

B 15+40

B 16+27

B 16+47

B 6+13

B 9+79

B 12+71

B 13+43

B 14+80

B 16+27

B 18+04

B 19+72

B 23+09

B 25+29

B 9+79

B 10+19

B 10+48

B 10+68

B 10+90

B 11+10

B 12+71

B 12+91

B 13+11

B 13+43

B 13+63

B 14+80

B 15+01

B 15+20

B 15+40

B 16+27

B 16+47

TABLE OF PAVEMENT MARKINGS - WHITE (CONT.) TABLE OF PAVEMENT MARKINGS - WHITE (CONT.) BEGIN END ITEM I FNGTH PAY LENGTH PAY ITEM BEGIN END STATION TO STATION OFFSET OFFSET STATION TO STATION DESCRIPTION DESCRIPTION 685.11 **FACTOR** B 26+92 B 26+92 30 18" STOP BAR 30 EDGE LINES - GREEN CONFLICT ZONE B 7+75 B 7+75 | 11 | 19 B 27+07 B 27+07 22 12" STOP BAR 26 B 8+04 11 11 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 27 0.33 B 27+22 B 27+41 49 45 18" STOP BAR 20 B 8+05 B 8+05 11 19 EDGE LINES - GREEN CONFLICT ZONE 1 B 27+47 B 27+64 45 57 18" STOP BAR 20 B 8+58 B 8+58 12 20 EDGE LINES - GREEN CONFLICT ZONE B 28+01 B 28+01 22 12" STOP BAF 13 21 0.33 B 8+62 B 8+83 13 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE B 28+12 B 28+12 14 8 18" STOP BAR 22 B 8+84 B 8+84 13 21 **EDGE LINES - GREEN CONFLICT ZONE** 1 B 36+59 B 36+75 36 18" STOP BAR 16 4.5 B 10+01 B 10+01 16 24 EDGE LINES - GREEN CONFLICT ZONE 8 1 8 B 37+70 B 37+70 3 18" STOP BAR 22 B 10+23 17 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 21 0.33 B 10+02 16 B 10+24 17 25 EDGE LINES - GREEN CONFLICT ZONE B 10+24 B 11+45 10 MISC EDGE LINES 45 17 **EDGE LINES - GREEN CONFLICT ZONE** B 10+56 B 10+56 25 B 11+96 B 12+41 MISC EDGE LINES 61 26 26 17 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 15 0.33 B 10+59 B 10+72 B 20+13 B 20+57 12 16 MISC EDGE LINES 52 B 10+74 EDGE LINES - GREEN CONFLICT ZONE B 10+74 18 26 1 B 22+01 B 21+56 26 26 MISC EDGE LINES 61 EDGE LINES - GREEN BIKE BOX 44 B 26+47 B 26+90 22 26 MISC EDGE LINES 51 21 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 38 B 11+69 B 12+07 0.33 13 B 11+69 B 12+07 28 29 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 38 0.33 B 9+86 B 10+94 14 DOTTED EXTENSION LINE - RT 108 0.33 B 13+58 B 13+58 22 30 EDGE LINES - GREEN CONFLICT ZONE 8 1 B 10+94 B 11+30 14 14 LANE EDGELINE - RT 36 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 10 0.33 B 13+70 22 B 13+60 B 12+27 B 12+77 LANE EDGE LINE - LT 50 B 13+70 22 30 EDGE LINES - GREEN CONFLICT ZONE 1 B 13+70 B 13+02 DOTTED EXTENSION LINE - LT 25 EDGE LINES - GREEN CONFLICT ZONE B 14+23 22 30 B 14+23 1 B 14+53 B 15+03 LANE EDGE LINE - LT 50 1 B 14+24 B 14+38 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 14 0.33 25 0.33 B 15+03 B 15+28 1 DOTTED EXTENSION LINE - LT B 14+39 22 30 **EDGE LINES - GREEN CONFLICT ZONE** 1 B 14+39 B 16+03 25 B 15+78 1 DOTTED EXTENSION LINE - RT B 14+95 B 14+95 30 **EDGE LINES - GREEN CONFLICT ZONE** B 16+53 LANE EDGELINE - RT 50 B 15+12 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 14 0.33 B 14+98 B 17+58 B 18+08 1 1 LANE EDGE LINE - LT 50 B 15+15 B 15+15 22 30 **EDGE LINES - GREEN CONFLICT ZONE** B 18+08 B 18+33 DOTTED EXTENSION LINE - LT 25 0.33 22 58 B 16+43 B 16+99 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 0.33 20 R 19+64 B 20+54 11 11 LANE EDGE LINE - RT 90 B 16+43 B 16+99 30 30 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 58 0.33 20 B 22+16 46 B 21+70 LANE EDGE LINE - LT B 17+43 B 17+69 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 27 0.33 B 22+16 B 22+45 1 1 DOTTED EXTENSION LINE - L 29 B 18+80 B 19+01 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 21 0.33 B 23+36 B 23+61 11 11 26 0.33 DOTTED EXTENSION LINE - RT B 20+69 B 21+43 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 74 0.33 25 B 23+61 B 24+11 11 11 LANE EDGE LINE - RT 50 1 B 20+69 B 21+43 30 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 74 0.33 30 25 DOTTED EXTENSION LINE - RT 64 B 25+66 B 26+42 11 11 B 21+54 B 21+68 **EDGE LINES - GREEN BIKE BOX** 44 B 26+42 B 26+92 11 11 LANE EDGE LINE - RT 50 26 12 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 38 0.33 B 21+54 B 21+54 13 B 27+05 B 27+87 11 14 DOTTED EXTENSION LINE - RT 82 0.33 B 21+62 26 12 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 38 0.33 13 B 21+62 B 28+11 B 28+58 3 LANE EDGE LINE - RT B 23+04 B 23+25 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 21 0.33 B 28+58 B 29+94 3 3 DOTTED EXTENSION LINE - RT 136 0.33 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 44 B 24+66 0.33 B 24+22 22 B 34+03 B 35+53 14 14 DOTTED EXTENSION LINE - RT 150 0.33 B 24+22 B 24+66 30 30 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 44 0.33 15 B 36+51 14 14 98 B 35+53 LANE EDGE LINE - RT 21 19 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 81 0.33 B 27+05 B 27+85 B 37+70 79 B 36+91 14 14 LANE EDGE LINE - RT B 27+05 B 27+86 29 26 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 81 0.33 27 B 27+17 B 27+31 17 EDGE LINES - GREEN BIKE BOX 44 44 16 1 B 11+11 B 11+12 19 27 CROSSWALK - TYPE LS B 29+25 B 29+99 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 74 0.33 25 B 11+33 B 11+59 30 CROSSWALK - TYPE LS 26 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 74 0.33 B 29+25 B 29+99 25 B 11+37 B 11+62 36 36 CROSSWALK - TYPE LS 25 B 30+49 B 30+59 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 13 0.33 B 11+80 B 12+14 35 38 CROSSWALK - TYPE LS 34 18 B 34+70 B 34+96 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 27 0.33 R 11+87 B 12+17 28 33 CROSSWALK - TYPE LS 67 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE 21 0.33 B 35+86 B 13+96 B 13+97 29 33 CROSSWALK - TYPE LS 62 B 13+98 B 14+38 40 39 CROSSWALK - TYPE IS B 11+27 B 11+27 25 13 18" STOP BAR 4.5 54 B 16+50 B 16+90 36 37 CROSSWALK - TYPE LS B 11+30 B 11+31 13 2 18" STOP BAR 11 4.5 50 B 17+02 B 17+05 29 29 CROSSWALK - TYPE LS 58 B 11+49 B 11+49 20 12" STOP BAR 3 12 24 B 17+13 B 17+44 40 39 CROSSWALK - TYPE LS B 11+98 49 46 18" STOP BAR 17 4.5 77 B 19+26 B 19+52 35 35 CROSSWALK - TYPE LS 25 B 12+27 B 12+27 11 12 18" STOP BAR 22 4.5 99 B 20+28 22 B 20+28 30 **CROSSWALK - TYPE LS** B 12+27 26 30 12" STOP BAR 4 3.00 12 B 12+27 B 20+62 B 20+65 29 29 CROSSWALK - TYPE LS 58 17 4.5 B 14+17 B 14+31 48 56 18" STOP BAR 77 B 20+76 43 47 48 B 21+24 CROSSWALK - TYPE LS B 16+50 B 16+72 43 44 18" STOP BAR 22 4.5 99 B 20+88 B 21+25 50 48 CROSSWALK - TYPE LS 10 B 19+39 B 19+47 44 48 18" STOP BAR 4.5 45 B 21+49 B 21+50 29 **CROSSWALK - TYPE LS** 58 29 B 20+54 B 20+54 1 30 18" STOP BAR 30 4.5 135 B 24+19 B 24+20 29 30 CROSSWALK - TYPE LS B 20+56 B 20+56 22 26 12" STOP BAR 4 3.0 R 24+29 B 24+62 40 40 CROSSWALK - TYPE LS 33 32 4.5 B 20+78 B 21+09 61 55 18" STOP BAR 144 B 26+55 B 26+55 22 30 CROSSWALK - TYPE LS 58 18" STOP BAR 22 4.5 B 21+26 99 B 27+03 B 27+15 29 31 CROSSWALK - TYPE LS 62 12" STOP BAR 4 3.0 B 21+59 12 B 27+20 B 27+61 39 37 CROSSWALK - TYPE LS AFFIX SEAL+76REENMAN-1+74EDERSEN! INC 12 ALTERED BY: 18" STOP BAR 22 4.5 99 B 27+20 B 27+76 37 36 CROSSWALK - TYPE LS 56 18" STOP BAR 15 4.5 B 27+92 B 27+93 25 **CROSSWALK - TYPE LS** 51 26 B 29+82 33 CROSSWALK - TYPE LS 41 B 29+41 AS-BUILT REVISIONS KINGSTON CONNECTIVITY DESCRIPTION OF ALTERATIONS: BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

	TABLE OF PAVEME	ENT LETTERS - WHITE	
STATION (TO CENTER)	OFFSET (TO CENTER) (FT)	DESCRIPTION	ITEM 685.13 (EA
B 11+13	12'	STOP	4
B 11+32	13'	BUS	3
B 12+09	25'	BUS	3
B 12+28	25'	STOP	4
B 20+26	15'	STOP	4
B 20+44	15'	BUS	3
B 21+72	25'	BUS	3
B 21+92	25'	STOP	4
B 26+58	25'	BUS	3
B 26+58	16'	STOP	4
B 26+78	25'	STOP	4
B 26+78	15'	BUS	3
B 35+49	9'	ONLY	4
		TOTAL:	46

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

GREENMAN-PEDERSEN, INC

CONSULTING ENGINEERS

TOTAL:

263

CONTRACT NUMBER

DL34880

DRAWING NO. MST-6 SHEET NO. 22

CITY OF KINGSTON

STATION TO STATION		BEGIN	END	DECEDIDATION	ITEM
STATION	USTATION	OFFSET	OFFSET	DESCRIPTION	601.01000018
		(FT)	(FT)		(SY)
B 6+56	B 6+82	11	11	GREEN CONFLICT ZONE	8
B 7+40	B 7+48	11	11	GREEN CONFLICT ZONE	4
B 7+78	B 8+04	11	11	GREEN CONFLICT ZONE	8
B 8+62	B 8+82	13	13	GREEN CONFLICT ZONE	7
B 10+02	B 10+22	16	17	GREEN CONFLICT ZONE	7
B 10+59	B 10+72	17	18	GREEN CONFLICT ZONE	5
B 11+04	B 11+06	19	19	GREEN CONFLICT ZONE	2
B 11+17	B 11+19	19	19	GREEN CONFLICT ZONE	2
B 11+64	B 11+71	12	12	BIKE BOX (7 FT x 8 FT)	7
B 11+71	B 11+78	12	12	BIKE BOX (7 FT x 8 FT)	7
B 11+69	B 12+07	20	21	GREEN CONFLICT ZONE	16
B 13+60	B 13+68	22	22	GREEN CONFLICT ZONE	4
B 13+89	B 13+91	22	22	GREEN CONFLICT ZONE	1
B 14+01	B 14+03	22	22	GREEN CONFLICT ZONE	1
B 14+24	B 14+38	22	22	GREEN CONFLICT ZONE	5
B 14+98	B 15+12	22	22	GREEN CONFLICT ZONE	5
B 16+43	B 16+99	22	22	GREEN CONFLICT ZONE	16
B 17+43	B 17+69	22	22	GREEN CONFLICT ZONE	8
B 18+80	B 19+00	22	22	GREEN CONFLICT ZONE	7
B 20+20	B 20+22	21	21	GREEN CONFLICT ZONE	2
B 20+33	B 20+35	21	21	GREEN CONFLICT ZONE	2
B 20+69	B 21+43	22	22	GREEN CONFLICT ZONE	21
B 21+54	B 21+61	14	13	BIKE BOX (7 FT x 8 FT)	7
B 21+61	B 21+68	13	13	BIKE BOX (7 FT x 8 FT)	7
B 21+58	B 21+58	26	12	GREEN CONFLICT ZONE	7
B 23+04	B 23+24	22	22	GREEN CONFLICT ZONE	7
B 24+22	B 24+66	22	22	GREEN CONFLICT ZONE	13
B 26+47	B 26+49	21	21	GREEN CONFLICT ZONE	2
B 26+60	B 26+62	21	21	GREEN CONFLICT ZONE	2
B 27+17	B 27+85	21	18	GREEN CONFLICT ZONE	19
B 27+17	B 27+24	13	12	BIKE BOX (7 FT x 8 FT)	7
B 27+24	B 27+31	12	12	BIKE BOX (7 FT x 8 FT)	7
B 29+25	B 29+99	18	18	GREEN CONFLICT ZONE	21
B 30+51	B 30+59	18	18	GREEN CONFLICT ZONE	4
B 34+70	B 34+96	18	18	GREEN CONFLICT ZONE	8
B 35+86	B 36+06	18	18	GREEN CONFLICT ZONE	7

FACTOR 685.11

3.0 12

135

90

90

12

99

99

45

61

52

61

51

0

36

50

50

9

50

50

90

46

10

50

22

50

28

45

50

98

79

112

868

350

476

938

868

560

560

812

434

350

112

812

672

518

812

812

462

112

868

574

784

714

574

BRIDGES

CUL VERTS

TOTAL: 27965

PIN 8780.49

RTE 32

4.5

4.5

4.5

3.0

45

4.5

1

1

1

1

1

0.33

1

0.33

1

1

1

0.33

0.33

1

1

1

AFFIX SEAL: GREENMAN - PEDERSEN, INC

B 24+23 B 24+68

B 26+60

B 27+17

B 28+01

B 29+23

B 27+41 B 27+47 54 46 **FULL BARRIER LINE** 10 2.0 B 28+12 B 34+56 14 **FULL BARRIER LINE** 645 2.0 B 29+16 B 36+51 14 FULL BARRIER LINE 736 2.0 B 36+91 B 37+70 **FULL BARRIER LINE** 80 2.0 89 6.0 B 7+99 B 10+81 **HATCH LINES** B 24+75 B 26+33 HATCH LINES 165 6.0 HATCH LINES 629 6.0 B 29+16 B 34+56 DOTTED SHORT LINE B 5+74 B 11+07 15 23 533 0.33 B 11+16 B 11+39 23 24 DOTTED SHORT LINE 23 0.33 40 B 11+68 B 12+08 24 25 DOTTED SHORT LINE 0.33 B 12+37 B 13+92 26 26 DOTTED SHORT LINE 155 0.33 B 14+01 B 16+40 26 26 DOTTED SHORT LINE 239 0.33 B 16+40 B 16+99 26 26 DOTTED SHORT LINE 59 0.33 B 20+22 26 26 DOTTED SHORT LINE 312 0.33 B 20+33 B 20+57 26 26 FDGF LINE 24 1.00 B 21+45 26 26 DOTTED SHORT LINE 75 0.33 B 21+54 B 24+14 26 26 DOTTED SHORT LINE 260 0.33 12 B 21+58 B 21+58 26 DOTTED SHORT LINE 0.33 26 10 B 21+58 B 21+68 26 **FDGF LINE** 1.00

TABLE OF PAVEMENT MARKINGS - YELLOW

DESCRIPTION

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

SOLID EDGE LINE

SOLID EDGE LINE

FULL BARRIER LINE

PARTIAL BARRIER LINE

PARTIAL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

PARTIAL BARRIER LINE

PARTIAL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

PARTIAL BARRIER LINE

PARTIAL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

PARTIAL BARRIER LINE

PARTIAL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

DOTTED SHORT LINE

LENGTH

(FT)

177

408

332

95

10

10

83

83

8

75

50

50

75

12

75

83

83

75

85

85

210

169

22

45

181

49

68

122

78

641 0.33

0.33

0.33

0.33

0.33

0.33

0.33

TOTAL:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

76

7

75

PAY

FACTOR

2.0

2.0

2.0

2.0

1.0

1.0

2.0

1.3

1.3

2.0

2.0

1.3

1.3

2.0

2.0

2.0

1.3

1.3

2.0

2.0

2.0

2.0

1.3

1.3

2.0

2.0

2.0

2.0

BEGIN

OFFSET

(FT)

13

13

24

26

11

11

48

11

11

1

56

11

11

1

43

62

11

11

66

OFFSET

(FT)

0

13

13

13

23

26

11

11

55

11

11

44

11

11

49

55

11

11

45

STATION TO STATION

B 5+78

B 9+86

B 11+30

B 10+81

B 12+37

B 13+02

B 13+85

B 13+85

B 14+17

B 15+28

B 15+78

B 15+78

B 16+53

B 16+74

B 18+33

B 19+16

B 19+16

B 19+39

B 20+54

B 21+11

B 22+45

B 23+36

B 23+36

B 24+11

B 26+92

B 26+33

B 27+46

B 24+68

B 26+49

B 27+09

B 27+85

B 29+23

B 30+01

B 30+01 B 36+42 22 22

26

26

26

25

22

22

26

26

26

22

22

22

ALTERED BY:

B 11+49

B 4+01

B 5+78

B 9+86

B 12+27

B 12+27

B 13+02

B 13+02

B 14+13

B 14+53

B 15+28

B 15+28

B 15+78

B 16+72

B 17+58

B 18+33

B 18+33

B 19+36

B 19+64

B 21+09

B 21+70

B 22+45

B 22+45

B 23+36

B 24+75

B 27+41

ITEM 685.12 STATION (TO CENTER) (FT) 354 B 5+80 B 6+15 816 664 B 6+16 B 6+27 190 10 B 6+28 10 B 9+30 150 B 9+31 104 B 9+42 104 16 B 11+03 150 B 11+23 63 B 11+23 63 B 11+35 150 24 150 104 104 14 180 14 150 106 106 152 420 338 44 20 1,290 1,472 160 532 990 3,774 178 13 52 80 20 104 24 25 87 13 10 15 60 16 23 41 26 214 13,993

B 21+63

B 21+64

B 21+72

B 21+72

16'

20'

24'

LEFT TURN ARROW

PED/BIKE

PED/BIKE STRAIGHT ARROW

BROADWAY STREETSCAPE PROJECT

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

B 11+35 25' PED/BIKE 18' PED/BIKE B 11+66 15' RIGHT TURN ARROW B 11+73 13' LEFT TURN ARROW B 11+74 18' PED/BIKE B 12+34 24' PED/BIKE B 12+34 28' STRAIGHT ARROW B 12+45 24' STRAIGHT ARROW B 12+45 28' PED/BIKE B 12+50 LEFT TURN ARROW B 13+38 LEFT TURN ARROW B 13+51 LEFT TURN ARROW B 14+62 24' PED/BIKE 28' STRAIGHT ARROW B 14+62 B 14+73 24' STRAIGHT ARROW B 14+73 28' PED/BIKE B 14+76 LEFT TURN ARROW B 15+47 LEFT TURN ARROW B 15+58 5' LEFT TURN ARROW B 16+24 24' PED/BIKE 28' STRAIGHT ARROW B 16+30 LEFT TURN ARROW 24' B 16+36 STRAIGHT ARROW B 16+36 28' PED/BIKE B 17+15 24' PED/BIKE B 17+15 28' STRAIGHT ARROW B 17+27 24' STRAIGHT ARROW B 17+27 28' PED/BIKE B 17+81 LEFT TURN ARROW B 18+69 LEFT TURN ARROW B 18+81 LEFT TURN ARROW B 19+09 24' PED/BIKE B 19+09 28' STRAIGHT ARROW 24' STRAIGHT ARROW B 19+21 28' PFD/BIKE B 19+90 LEFT TURN ARROW B 20+30 LEFT TURN ARROW B 20+39 24' PED/BIKE B 20+50 28' STRAIGHT ARROW B 20+51 24' STRAIGHT ARROW B 20+51 28' PED/BIKE B 20+96 70' RIGHT/STRAIGHT 66' LEFT TURN ARROW B 21+10 68' LFFT TURN ARROW 69' B 21+19 RIGHT/STRAIGHT B 21+57 20' PED/BIKE B 21+58 16' RIGHT TURN ARROW

TABLE OF PAVEMENT SYMBOLS - WHITE

DESCRIPTION

CHEVRON

STRAIGHT ARROW

PED/BIKE

PED/BIKE

STRAIGHT ARROW

PED/BIKE

STRAIGHT ARROW

STRAIGHT ARROW

LEFT TURN ARROW

PED/BIKE

STRAIGHT ARROW

STRAIGHT ARROW

(TO CENTER)

(FT)

15'

17'

13'

17'

13'

16'

20'

16'

20'

8'

22'

25'

22'

ITEM

685.14

(EA)

1

1

	OFFSET		IT
STATION (TO CENTER)	(TO CENTER) (FT)	DESCRIPTION	68: (E
B 21+84	24'	STRAIGHT ARROW	
B 21+84	28'	PED/BIKE	
B 21+93	5'	LEFT TURN ARROW	
B 22+80	24'	PED/BIKE	
	 	·	
B 22+80	28'	STRAIGHT ARROW	-
B 22+86	6'	LEFT TURN ARROW	
B 22+92	24'	STRAIGHT ARROW	
B 22+92	27'	PED/BIKE	
B 22+98	5'	LEFT TURN ARROW	
B 23+88	5'	LEFT TURN ARROW	
B 23+98	24'	PED/BIKE	
B 23+98	28'	STRAIGHT ARROW	
B 24+10	24'	STRAIGHT ARROW	
B 24+10	28'	PED/BIKE	
B 24+73	24'	PED/BIKE	
B 24+73	28'	STRAIGHT ARROW	
B 24+85	24'	STRAIGHT ARROW	
B 24+85	28'	PED/BIKE	
	24'	·	
B 25+76	 	PED/BIKE	-
B 25+76	28'	STRAIGHT ARROW	-
B 25+88	24'	STRAIGHT ARROW	-
B 25+88	28'	PED/BIKE	_
B 26+28	5'	LEFT TURN ARROW	_
B 26+69	5'	LEFT TURN ARROW	
B 26+74	24'	PED/BIKE	
B 26+74	28'	STRAIGHT ARROW	
B 26+86	24'	STRAIGHT ARROW	
B 26+86	28'	PED/BIKE	
B 27+21	19'	PED/BIKE	
B 27+23	15'	RIGHT TURN ARROW	
B 27+26	15'	LEFT TURN ARROW	
B 27+28	19'	PED/BIKE	
B 28+09	20'		-
		PED/BIKE	\vdash
B 28+09	24'	STRAIGHT ARROW	-
B 28+21	20'	STRAIGHT ARROW	-
B 28+21	24'	PED/BIKE	-
B 28+31	8'	LEFT TURN ARROW	_
B 28+72	8'	LEFT TURN ARROW	
B 29+06	20'	PED/BIKE	
B 29+06	24'	STRAIGHT ARROW	
B 29+18	20'	STRAIGHT ARROW	
B 29+18	24'	PED/BIKE	
B 30+08	20'	PED/BIKE	
B 30+08	24'	STRAIGHT ARROW	
B 30+19	20'	STRAIGHT ARROW	
B 30+19	24'	PED/BIKE	
B 32+40	20'	PED/BIKE	-
B 32+40	24'	STRAIGHT ARROW	
B 32+52	20'	STRAIGHT ARROW	
B 32+52	24'	PED/BIKE	
B 34+43	20'	PED/BIKE	
B 34+43	24'	STRAIGHT ARROW	
B 34+55	20'	STRAIGHT ARROW	L
B 34+55	24'	PED/BIKE	
B 34+66	9'	LEFT TURN ARROW	
B 36+21	20'	PED/BIKE	
B 36+21	24'	STRAIGHT ARROW	
B 36+27	6'	LEFT TURN ARROW	
B 36+33	20'	STRAIGHT ARROW	
	24'		
B 36+33		PED/BIKE	\vdash
B 37+05 B 37+47	8' 8'	LEFT TURN ARROW	-
		LEFT TURN ARROW	

PIN 8780.49

RTE 32

BRIDGES

CUL VERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

GREENMAN-PEDERSEN, INC

CONSULTING ENGINEERS

CONTRACT NUMBER

DL34880

CITY OF KINGSTON

DRAWING NO. MST-7 SHEET NO. 23

TABLE OF PAVEMENT SYMBOLS - WHITE (CONT.)

ITEM		STATION OFFSET		
685.14	DESCRIPTION	(TO CENTER)	(TO CENTER)	
(EA)		(FT)		
1	STRAIGHT ARROW	B 21+84 24'		
1	PED/BIKE	28'	B 21+84	
1	LEFT TURN ARROW	5'	B 21+93	
1	PED/BIKE	24'	B 22+80	
1	STRAIGHT ARROW	28'	B 22+80	
1	LEFT TURN ARROW	6'	B 22+86	
1	STRAIGHT ARROW	24'	B 22+92	
1	PED/BIKE	27'	B 22+92	
1	LEFT TURN ARROW	5'	B 22+98	
1	LEFT TURN ARROW	5'	B 23+88	
1	PED/BIKE	24'	B 23+98	
1	STRAIGHT ARROW	28'	B 23+98	
1	STRAIGHT ARROW	24'	B 24+10	
1	PED/BIKE	28'	B 24+10	
1	PED/BIKE	24'	B 24+73	
1	STRAIGHT ARROW	28'	B 24+73	
1	STRAIGHT ARROW	24'	B 24+85	
1	PED/BIKE	28'	B 24+85	
1	PED/BIKE	24'	B 25+76	
1	STRAIGHT ARROW	28'	B 25+76	
1	STRAIGHT ARROW	24'	B 25+88	
1	PED/BIKE	28'	B 25+88	
1	LEFT TURN ARROW	5'	B 26+28	
1	LEFT TURN ARROW	5'	B 26+69	
1	PED/BIKE	24'	B 26+74	
1	STRAIGHT ARROW	28'	B 26+74	
1	STRAIGHT ARROW	24'	B 26+86	
1	PED/BIKE	28'	B 26+86	
1	PED/BIKE	19'	B 27+21	
1	RIGHT TURN ARROW	15'	B 27+23	
1	LEFT TURN ARROW	15'	B 27+26	
1		19'		
	PED/BIKE		B 27+28	
1	PED/BIKE	20'	B 28+09	
1	STRAIGHT ARROW	24'	B 28+09	
1	STRAIGHT ARROW	20'	B 28+21	
1	PED/BIKE	24'	B 28+21	
1	LEFT TURN ARROW	8'	B 28+31	
1	LEFT TURN ARROW	8'	B 28+72	
1	PED/BIKE	20'	B 29+06	
1	STRAIGHT ARROW	24'	B 29+06	
1	STRAIGHT ARROW	20'	B 29+18	
1	PED/BIKE	24'	B 29+18	
1	PED/BIKE	20'	B 30+08	
1	STRAIGHT ARROW	24'	B 30+08	
1	STRAIGHT ARROW	20'	B 30+19	
1	PED/BIKE	24'	B 30+19	
1	PED/BIKE	20'	B 32+40	
1	STRAIGHT ARROW	24'	B 32+40	
1	STRAIGHT ARROW	20'	B 32+52	
1	PED/BIKE	24'	B 32+52	
1	PED/BIKE	20'	B 34+43	
1	STRAIGHT ARROW	24'	B 34+43	
1	STRAIGHT ARROW	20'	B 34+55	
1	PED/BIKE	24'	B 34+55	
1	LEFT TURN ARROW	9'	B 34+66	
1	PED/BIKE	20'	B 36+21	
1	STRAIGHT ARROW	24'	B 36+21	
1	LEFT TURN ARROW	6'	B 36+27	
1	STRAIGHT ARROW	20'	B 36+33	
1	PED/BIKE	24'	B 36+33	
1	LEFT TURN ARROW	8'	B 37+05	
	LEFT TURN ARROW	8'	B 37+47	

FILE NAME =\CADD\878849.CPH_MST-8.dgn DATE/TIME = 8/28/2019 USER = tvyce*

B 7+68 LT B 7+69 RT B 8+16 RT B 8+23 B 8+51 LT B 8+60 RT B 8+91 RT B 8+91 B 9+18 LT B 9+22 RT B 9+52 RT B 9+60 B 9+92 RT B 10+14 B 10+40 LT B 10+55 RT RT B 10+81 B 10+94 LT RT B 11+08 B 11+36 B 12+36 B 12+39 B 12+64 LT LT B 12+96 RT B 13+22 LT B 13+51 LT B 13+72 RT B 13+78 B 14+11 B 14+62 RT B 14+88 B 15+26 RT B 15+44 B 15+70 LT B 15+90 LT RT B 16+37 B 16+89 RT LT B 17+37 B 17+78 LT LT B 17+99 B 18+08 RT B 18+30 LT B 18+46 RT B 19+07 RT B 19+14 B 19+41 LT B 19+84 ALTERED BY: ON: AFFIX SEAL: GREENMAN - PEDERSEN, INC

TABLE OF TREE REMOVAL

RT RT

RT

LT RT

RT

B 5+12

B 5+47

B 6+37 B 6+45

B 6+71

B 7+30

ITEM 614.060304

ITEM 614.0701

	TABLE OF TREE REMOVAL (CONT.)					
CTATION	CIDE	ITEM 614.060304	ITEM 614.0701			
STATION	SIDE	(EA)	(EA)			
B 20+09	LT	1				
B 20+29	LT	1				
B 21+67	LT	1				
B 21+99	LT	1				
B 22+18	LT	1				
B 22+49	LT	1				
B 22+98	LT	1				
B 23+27	RT	1				
B 23+76	RT	1				
B 23+98	LT	1				
B 24+37	RT	1				
B 24+91	LT	1				
B 25+20	LT	1				
B 25+51	LT	1				
B 25+75	LT	1				
B 25+85	RT	1				
B 25+96	LT	1				
B 26+49	RT	1				
B 26+51	LT	1				
B 26+79	LT	1				
B 28+20	LT	1				
B 28+40	LT	1				
B 29+13	LT	1				
B 34+71	RT	1				
B 35+67	RT	1				
B 36+24	LT	1				
	TOTALS:	72	14			

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

	BIORETENTION AREAS													
ID #	ОПТІОМ	STATION	CIDE	LENGTH	ITEM	ITEM	ITEM	ITEM	ITEM	ITEM	ITEM	ITEM	ITEM	ITEM
ID#	OPTION	STATION	SIDE	(FT)	203.02 (CY)	207.21 (SY)	(CY)	420.98010104 (SF)	(LF)	(EA)	608.72126108 (EA)	609.05 (LF)	610.1101 (CY)	623.10 (CY)
1-8	1	B 5+53	RT	5	4.0	10	2.0	25	6	5	(124)	0	0.1	1.7
1-9	Ĺ	B 5+96	RT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
1-1	С	B 6+38	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
1-10 1-2	G A	B 6+46 B 7+13	RT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
1-11	F	B 7+26	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
1-3	L	B 7+64	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
1-12 1-13	L F	B 7+70 B 8+08	RT RT	5 25	4.0 20.0	10 50	2.0 10.0	25	6 6	5 25	4	<u>0</u> 36	0.1 0.5	1.7 8.5
1-4	F	B 8+30	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
1-5	N	B 8+51	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
1-14 1-6	G C	B 8+67 B 8+98	RT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
1-15	Α	B 9+14	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
1-7	N	B 9+21	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
2-10 2-1	N N	B 9+51 B 9+52	RT LT	<u>5</u>	4.0 4.0	10 10	2.0	25 25	6	<u>5</u>		0	0.1	1.7 1.7
2-2	G	B 9+77	LT	25	20.0	50	10.0	23	6	25	4	36	0.5	8.5
2-3	L	B 10+41	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
2-4 2-11	H	B 10+96 B 12+08	LT RT	5 25	4.0 20.0	10 50	2.0 10.0	25	6	5 25	4	<u>0</u> 36	0.1 0.5	1.7 8.5
2-11	Н	B 12+46	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
2-6	Ī	B 12+86	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
2-12 2-7	L H	B 12+90 B 13+43	RT LT	5 25	4.0 20.0	10 50	2.0 10.0	25	6 6	5 25	4	<u>0</u> 36	0.1 0.5	1.7 8.5
2-13	1	B 13+43 B 13+67	RT	17.5	14.0	35	7.0		6	17.5	3	23	0.5	6.0
2-8	N	B 14+13	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
2-9	I	B 14+54	LT	25	20.0	50	10.0	25	6	25	4	36	0.5	8.5
3-9 3-1	K D	B 15+12 B 15+53	RT LT	5 25	4.0 20.0	10 50	2.0 10.0	25	6	5 25	4	<u>0</u> 36	0.1 0.5	1.7 8.5
3-2	D	B 15+97	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
3-3	L	B 16+24	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
3-10 3-4	B E	B 16+76 B 17+26	RT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
3-5	E	B 17+95	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
3-11	M	B 18+08	RT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
3-6 3-12	E M	B 18+33 B 18+44	LT RT	25 5	20.0 4.0	50 10	10.0 2.0	25	6	25 5	4	36 0	0.5 0.1	8.5 1.7
3-12	M	B 19+07	RT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
3-7	А	B 19+21	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
3-14 3-8	K A	B 19+72 B 19+84	RT LT	5 25	4.0 20.0	10 50	2.0 10.0	25	6	5 25	4	<u>0</u> 36	0.1 0.5	1.7 8.5
3-15	K	B 19+84 B 20+12	RT	5	4.0	10	2.0	25	6	5	- +	0	0.5	1.7
4-1	N	B 20+19	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
4-9 4-10	M	B 21+30 B 21+33	RT RT	<u>5</u>	4.0 4.0	10 10	2.0	25 25	6	5 5		0	0.1 0.1	1.7 1.7
4-10 4-2	J	B 21+33 B 21+74	LT	25	20.0	50	10.0	23	6	25	4	36	0.1	8.5
4-3	J	B 22+25	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
4-4 4-5	L	B 22+48 B 22+83	LT LT	5 25	4.0 20.0	10 50	2.0 10.0	25	6	5 25	4	<u>0</u> 36	0.1 0.5	1.7 8.5
4-5 4-6	C	B 22+83 B 23+92	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5 8.5
4-11	В	B 24+73	RT	17.5	14.0	35	7.0		6	17.5	3	23	0.4	6.0
4-7 4-8	L	B 24+91 B 25+27	LT LT	5 25	4.0	10 50	2.0	25	6	5 25	4	<u>0</u> 36	0.1	1.7 8.5
<u>4-8</u> 5-7	C B	B 25+27 B 25+57	RT	17.5	20.0 14.0	35	10.0 7.0		6	17.5	3	23	0.5 0.4	6.0
5-1	N	B 25+75	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
5-2	N	B 25+97	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
5-3 5-8	C B	B 26+30 B 26+52	LT RT	25 17.5	20.0 14.0	50 35	10.0 7.0		6	25 17.5	3	36 23	0.5 0.4	8.5 6.0
5-9	Α	B 28+32	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
5-4	A	B 28+53	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
5-5 5-10	H	B 29+03 B 29+05	LT RT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
5-10	M	B 30+05	RT	5	4.0	10	2.0	25	6	5		0	0.3	1.7
5-6	D	B 30+33	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
5-12	D	B 30+50	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-1 6-2	D C	B 31+16 B 33+30	LT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
6-5	В	B 33+41	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-3	F	B 34+36	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-6 6-7	G K	B 34+36 B 34+90	RT RT	25 5	20.0 4.0	50 10	10.0 2.0	25	6	25 5	4	<u>36</u> 0	0.5 0.1	8.5 1.7
6-4	C	B 34+90 B 35+41	LT	25	20.0	50	10.0	23	6	25	4	36	0.1	8.5
6-8	F	B 35+60	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-9	K P	B 36+18	RT	5	4.0	10	2.0	25	6	5	4	0	0.1 0.5	1.7
6-10 6-11	P	B 31+72 B 31+96	RT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5	8.5 8.5
7-1	F	B 36+67	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
		TO	OTALS:	1340	1072	2680	536	725	468	1340	192	1712	29	457

PIN 8780.49

RTE 32

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

BROADWAY STREETSCAPE PROJECT

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

GPT GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

CONTRACT NUMBER

DL34880

CITY OF KINGSTON

DRAWING NO. MST-8 SHEET NO. 24

FILE NAME = ...\CADD\878849.CPH_MST-9.dgn
DATE/TIME = 8/28/2819
USER = tvygoe*
DESIGN SUPERVISOR C. CORNWFI

ELECTRIC MANHOLES STATION OFFSET QUANTITY B 24+40 B 24+47 RT 38.3' *BY OTHERS, SEE PROPOSAL FOR UTILITY COORDINATION NOTES. NO DIRECT PAYMENT

STORMWATER MANHOLES						
STATION	SIDE	OFFSET	ITEM			
STATION			660.650001 (EA)			
B 6+18	RT	13.6'	1			
B 8+15	RT	14.3'	1			
B 10+21	RT	13.3'	1			
B 12+19	RT	12.8'	1			
B 14+15	RT	13.0'	1			
B 16+21	RT	12.9'	1			
B 17+42	RT	12.7'	1			
B 18+26	RT	13.0'	1			
B 19+44	RT	12.3'	1			
B 20+23	RT	12.5'	1			
B 20+92	RT	11.6'	1			
B 20+96	RT	28.4'	1			
B 22+09	RT	6.8'	1			
B 24+41	RT	6.6'	1			
B 25+81	RT	7.2'	1			
B 27+23	RT	20.5'	1			
B 27+68	RT	18.0'	1			
B 29+16	RT	10.0'	1			
B 29+43	RT	9.7'	1			
B 30+66	RT	16.1'	1			
B 31+32	RT	16.4'	1			
B 32+22	RT	16.5'	1			
B 32+86	RT	16.9'	1			
B 34+21	RT	17.8'	1			
B 35+94	RT	18.3'	1			
B 36+52	RT	18.6'	1			
B 25+48	LT	42.8'	1			
B 27+43	LT	62.1'	1			

TOTAL:

ALTERED BY: ON:

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:

TELEPHONE MANHOLES							
STATION	SIDE	OFFSET	QUANTITY				
B 7+65	RT	16.6'	1				
B 10+40	RT	35.1'	1				
B 11+62	RT	16.8'	1				
B 14+30	RT	14.6'	1				
B 14+75	RT	37.5'	1				
B 16+54	RT	17.6'	1				
B 21+10	RT	14.7'	1				
B 21+45	RT	36.1'	1				
B 21+45	RT	32.1'	1				
B 24+29	RT	12.9'	1				
B 24+56	RT	34.5'	1				
B 24+62	RT	34.7'	1				
B 26+96	RT	13.8'	1				
B 26+96	RT	32.8'	1				
B 28+18	RT	32.1'	1				
B 36+75	RT	13.2'	1				
B 21+17	LT	50.0'	1				
B 21+18	LT	20.9'	1				
TOTAL: 18							
*BY OTHERS, SEE PROPOSAL FOR UTILITY							
COORDINATION NOTES. NO DIRECT PAYMENT							

SEWER MANHOLES							
STATION	SIDE	OFFSET	ITEM 660.650001 (EA)				
B 5+34	LT	6.9'	1				
B 8+56	LT	7.2'	1				
B 11+90	LT	7.0'	1				
B 11+97	LT	16.6'	1				
B 14+26	LT	7.7'	1				
B 16+61	LT	7.7'	1				
B 19+66	LT	7.9'	1				
B 21+05	LT	8.9'	1				
B 21+18	LT	40.3'	1				
B 29+64	LT	51.7'	1				
B 12+22	RT	19.0'	1				
B 14+32	RT	21.6'	1				
B 19+51	RT	24.9'	1				
B 22+01	RT	9.5'	1				
B 24+35	RT	9.4'	1				
B 27+28	RT	7.9'	1				
B 29+50	RT	17.9'	1				
B 33+09	RT	6.9'	1				
B 34+40	RT	27.2'	1				
B 34+41	RT	18.9'	1				
B 36+64	RT	8.2'	1				
		TOTAL:	21				

WATER MANHOLES				
STATION	SIDE	OFFSET	ITEM 660.650001 (EA)	
B 27+52	RT	25.6'	1	
		TOTAL:	1	

UNKNOWN MANHOLES				
STATION	SIDE	OFFSET	ITEM 660.650001 (EA)	
B 12+28	RT	24.4'	1	
B 23+80	RT	38.1'	1	
B 10+87	LT	29.1'	1	
		TOTAL:	3	

MANHOLE SUMMA	RY TABLE
UTILITY TYPE	ITEM 660.650001 (EA)
ELECTRIC	2*
STORMWATER	28
TELEPHONE	18*
SEWER	21
WATER	1
UNKNOWN	3
TOTAL:	53
*BY OTHERS, SEE PROPOS	AL FOR UTILITY

COORDINATION NOTES. NO DIRECT PAYMENT

STATION	SIDE	OFFSET	QUANTITY			
B 6+19	LT	13.9'	1			
B 6+25	RT	23.5'	1			
B 6+26	RT	23.6'	1			
B 6+29	LT	15.3'	1			
B 6+34	LT	8.8'	1			
B 6+35	RT	7.8'	1			
B 6+89	LT	10.0'	1			
B 7+60	LT	14.4'	1			
B 8+12	RT	29.3'	1			
B 8+53	LT	21.8'	1			
B 8+58	LT	23.3'	1			
B 11+17	LT	23.5'	1			
B 11+25	LT	20.7'	1			
B 11+47	LT	17.4'	1			
B 11+56	LT	29.1'	1			
B 11+86	LT	23.1'	1			
B 11+90	LT	29.1'	1			
B 11+94	LT	6.5'	1			
B 12+15	LT	39.9'	1			
B 12+33	LT	25.1'	1			
B 12+33	LT	23.5'	1			
B 13+04	LT	31.2'	1			
B 13+40	LT	32.1'	1			
B 14+20	LT	22.8'	1			
B 14+31	LT	27.9'	1			
B 15+22	LT	32.5'	1			
B 15+78	LT	31.8'	1			
B 16+71	LT	37.1'	1			
B 16+77	LT	38.4'	1			
B 16+80	LT	48.9'	1			
B 16+82	LT	39.7'	1			
B 16+84	LT	46.6'	1			
B 16+92	LT	40.1'	1			
B 17+61	LT	19.8'	1			
B 17+77	LT	22.5'	1			
B 18+16	LT	18.1'	1			
B 19+36	RT	40.5'	1			
B 19+47	LT	32.1'	1			
B 19+79	LT	32.0'	1			
B 19+88	RT	31.4'	1			
B 20+05	LT	32.6'	1			
B 20+84	LT	55.0'	1			
B 21+57	LT	30.5'	1			
B 21+62	LT	30.7'	1			
B 21+84	LT	16.2'	1			
B 21+86	LT	25.0'	1			
B 22+24	LT	30.4'	1			
B 22+43	LT	30.5'	1			
B 22+72	LT	30.2'	1			
B 23+54	LT	30.1'	1			
B 23+79	LT	27.4'	1			
B 23+86	RT	31.2'	1			
B 26+59	RT	31.4'	1			
B 27+41	LT	25.2'	1			
B 27±60	IT.	E2 1 ¹	1			

VALVE BOX ADJUSTMENT - GAS

B 0+19	LI	19.4	1
B 6+49	LT	19.6'	1
B 6+73	LT	20.5'	1
B 6+88	LT	20.9'	1
B 7+40	LT	19.6'	1
B 8+08	LT	20.4'	1
B 8+29	LT	12.5'	1
B 8+43	LT	21.3'	1
B 9+99	LT	26.6'	1
B 10+28	LT	18.6'	1
B 10+42	LT	26.0'	1
B 11+10	LT	28.4'	1
B 11+60	LT	29.7'	1
B 11+67	LT	27.1'	1
B 11+76	LT	23.2'	1
B 11+99	LT	2.0'	1
B 12+99	LT	30.6'	1
B 13+30	LT	30.3'	1
B 14+89	LT	30.9'	1
B 15+49	LT	30.9'	1
B 15+52	LT 	30.2'	1
B 15+78	LT	30.9'	1
B 15+85	LT	31.1'	1
B 16+53	LT	46.2'	1
B 17+44	LT	30.5'	1
B 17+71	LT	30.6'	1
B 18+18	LT	30.4'	1
B 18+47	LT	30.7'	1
B 20+19	LT	30.5'	1
B 20+21	LT	30.1'	1
B 21+08	LT	0.1'	1
B 21+19	LT	45.6'	1
B 23+51	LT	29.9'	1
B 23+81	LT	30.6'	1
B 24+03	LT	29.6'	1
B 24+61	LT	44.1'	1
B 24+84	LT	29.7	1
B 25+06	LT	29.9'	1
B 25+48	LT	30.8'	1
B 25+67	LT	30.5'	1
B 25+78	LT	31.6'	1
B 25+91	LT	30.3'	1
B 26+06	LT	30.7'	1
B 26+07	LT	30.2'	1
B 26+56	LT	29.9'	1
B 26+85	RT	31.5'	1
B 28+30	LT	29.8'	1
B 31+31	LT	40.1'	1
B 35+23	LT	36.8'	1
B 35+60	LT	29.5'	1
B 36+02	LT	28.5'	1
B 7+46	RT	29.0'	1
B 7+47	RT	29.0'	1
B 10+07	RT	29.8'	1
B 11+39	RT	44.8'	1
B 11+59	RT	43.8'	1
B 11+99	RT	2.6'	1
B 12+33	RT	31.3'	1
B 12+81	RT	30.9'	1
	RT	41.3'	1
B 14+12	n-	27 -1	-
B 14+12 B 17+19 B 21+04	RT RT	27.6' 45.2'	1

VALVE BOX ADJUSTMENT - WATER

OFFSET

663.33 (EA)

STATION

SIDE

RT R	41.4' 30.5' 30.5' 24.3' 31.7' 30.4' 31.6' 51.0'	663.33 (EA) 1 1 1 1 1 1 1 1 1 1 1 1 1	
RT RT RT RT RT RT RT RT	30.5' 24.3' 31.7' 30.4' 31.6' 51.0' 56.7'	1 1 1 1 1	
RT RT RT RT RT RT RT	24.3' 31.7' 30.4' 31.6' 51.0' 56.7'	1 1 1 1	
RT RT RT RT RT RT	31.7' 30.4' 31.6' 51.0' 56.7'	1 1 1 1	
RT RT RT RT RT	30.4' 31.6' 51.0' 56.7'	1 1 1	
RT RT RT RT	31.6' 51.0' 56.7'	1 1	
RT RT RT	51.0' 56.7'	1	
RT RT	56.7'		
RT			
	10.51	1	
DT	18.5'	1	
ΝI	33.8'	1	
RT	19.9'	1	
RT	37.4'	1	
RT	16.3'	1	
RT	37.0'	1	
RT	25.8'	1	
RT	26.1'	1	
RT	19.9'	1	
RT	26.5'	1	
RT	40.4'	1	
RT	27.4'	1	
RT	19.6'	1	
RT	19.4'	1	
RT	24.5'	1	
RT	24.4'	1	
RT	21.1'	1	
RT	24.7'	1	
RT	24.5'	1	
	TOTAL:	90	
	RT R	RT 16.3' RT 37.0' RT 25.8' RT 26.1' RT 19.9' RT 26.5' RT 40.4' RT 19.6' RT 19.4' RT 24.5' RT 24.4' RT 24.5' RT 24.7' RT 24.5'	

VAL	VE BOX A	DJUSTMEN	T - UNKNOWN
STATION	SIDE	OFFSET	ITEM 663.33 (EA)
B 5+72	LT	27.5'	1
B 19+39	LT	31.3'	1
		ΤΟΤΔΙ ·	2

ILT REVISIONS	KINGSTON CONNECTIVITY	P
RIPTION OF ALTERATIONS:	BROADWAY STREETSCAPE PROJECT] R
	CITY OF KINGSTON	
	COUNTY: ULSTER REGION: 8	

PIN 8780.49 RTE 32

60

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

CONTRACT NUMBER DL34880

DRAWING NO. MST-9 SHEET NO. 25

GPI GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

CITY OF KINGSTON

	COUNTY: ULSTER	REGION: 8			
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A L SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	ICENSED PROFESSIONAL IS ALTERED, THE ALTERIN	IG ENGINEER, ARCHIT	ECT, LANDSCAPE ARCHITECT, OF	R LAND SURVEYOR	₹ं

B 27+60 LT 53.1'

B 28+04 LT 29.9'

B 35+05 LT

RT

30.8'

27.3'

*BY OTHERS, SEE PROPOSAL FOR UTILITY COORDINATION NOTES. NO DIRECT PAYMENT

B 27+75

B 27+85 B 27+98

FILE NAME = ...\CADD\878049.CPH_MST-10.dgn DATE/TIME = 8/28/2019 + USER = tvyce*

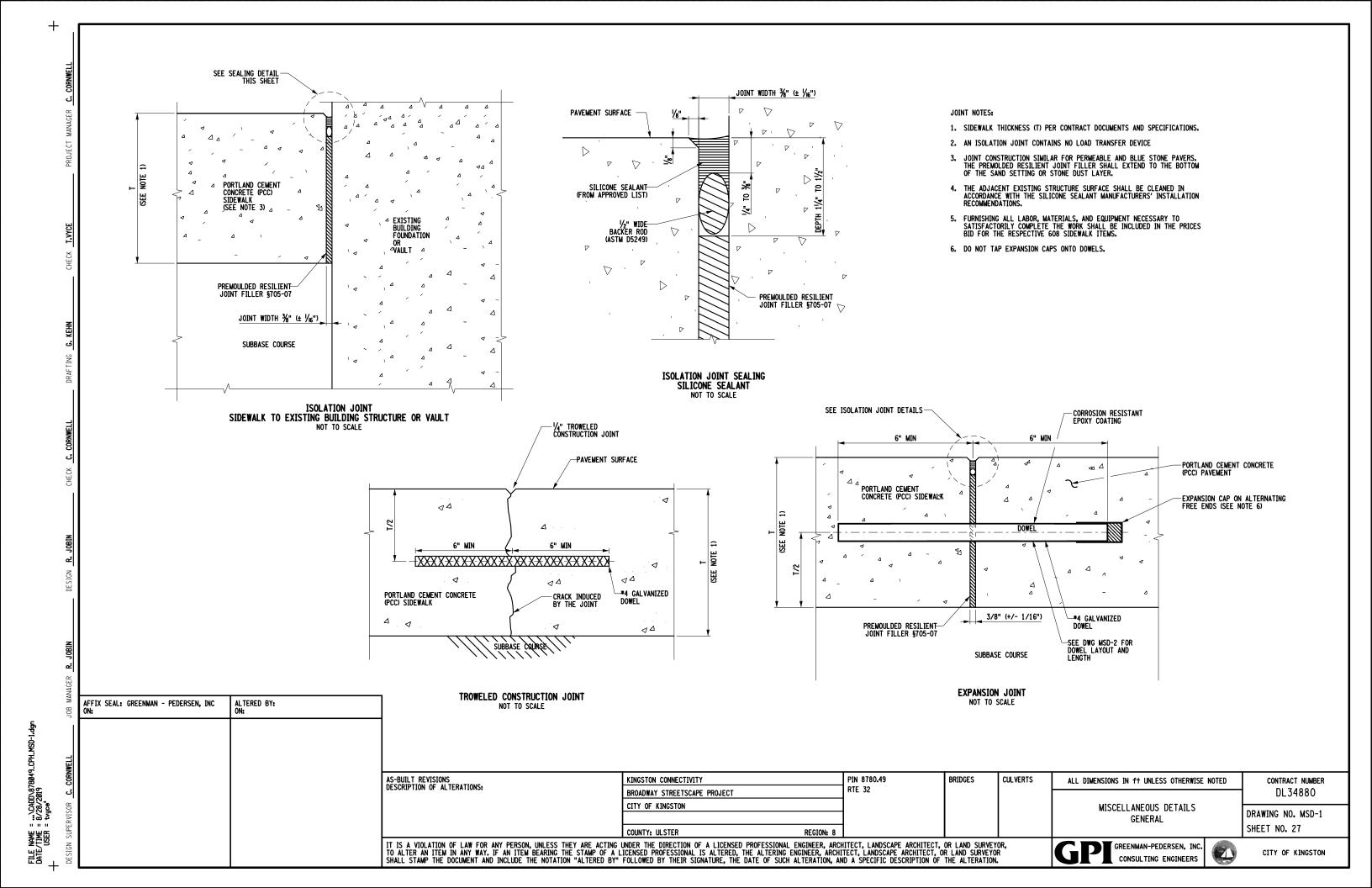
TABLE OF RIGHT-OF-WAY ACQUISITIONS MAP# PARCEL TAX MAP INFORMATION SIZE OF TOTAL SIZE OF PORTION TO TYPE OF DEED OWNER COMMENTS LIBER PAGE PARCEL BE ACQUIRED ACQUISITION # SECTION BLOCK LOT 4344 80 6,404 SF (0.147 AC.) 39 SF (0.001 AC.) DAGO ANTONIO MERAZ 101 101 56.092 21 102 102 VIRK CONVENIENT MART, INC. 56.093 3265 173 12,894 SF (0.296 AC.) 66 SF (0.002 AC.) PE 103 103 BHAI-BOON, INC. (ANTHONY G. SACCOMAN) 56.025 3502 362 3,224 SF (0.074 AC.) 13 SF (0.001 AC.) 104 104 EARL B. FEIDEN. INC. (ARTHUR FERRARO) 56.093 1646 56 14,419 SF (0.331 AC.) 6 SF (0.001 AC.) 2790 300 12,241 SF (0.281 AC.) 61 SF (0.001 AC.) A&P FERRARO CORPORATION 105 105 56.093 14 5942 12 13,286 SF (0.305 AC.) 40 SF (0.001 AC.) 106 106 CB 635, LLC 56.109 20 PE 107 107 YI YUAN INC 56.025 5892 271 24,002 SF (0.551 AC.) 55 SF (0.001 AC.) 109 109 THE PEOPLE OF THE STATE OF NEW YORK 56.109 1845 161 29,578 SF (0.679 AC.) 205 SF (0.005 AC.) 4493 178 24,873 SF (0.571 AC.) 424 SF (0.010 AC.) BARDAVON 1869 OPERA HOUSE, INC. (UPAC) 110 110 56.109 19 111 GARRY J. TUMA 56.109 1707 255 9,720 SF (0.223 AC.) 15 SF (0.001 AC.) 111 22 PE BASEMENT REALTY, INC. 56.025 1694 89 3,572 SF (0.082 AC.) 20 SF (0.001 AC.) PE 113 113 KINGSTON BROADWAY FD, LLC (HARLAND GENTILIN) 3490 60 7,928 SF (0.182 AC.) 184 SF (0.004 AC.) 56.109 114 114 MATTEO-BELLA, LLC 56.025 5437 69 4,226 SF (0.097 AC.) 16 SF (0.001 AC.) PE PROPERTY RELEASES RELEASE DATE HOUSE STATION SIDE PROPERTY OWNER(S) COMMENTS, INCLUDE PURPOSE NUMBER TYPE SIGNED

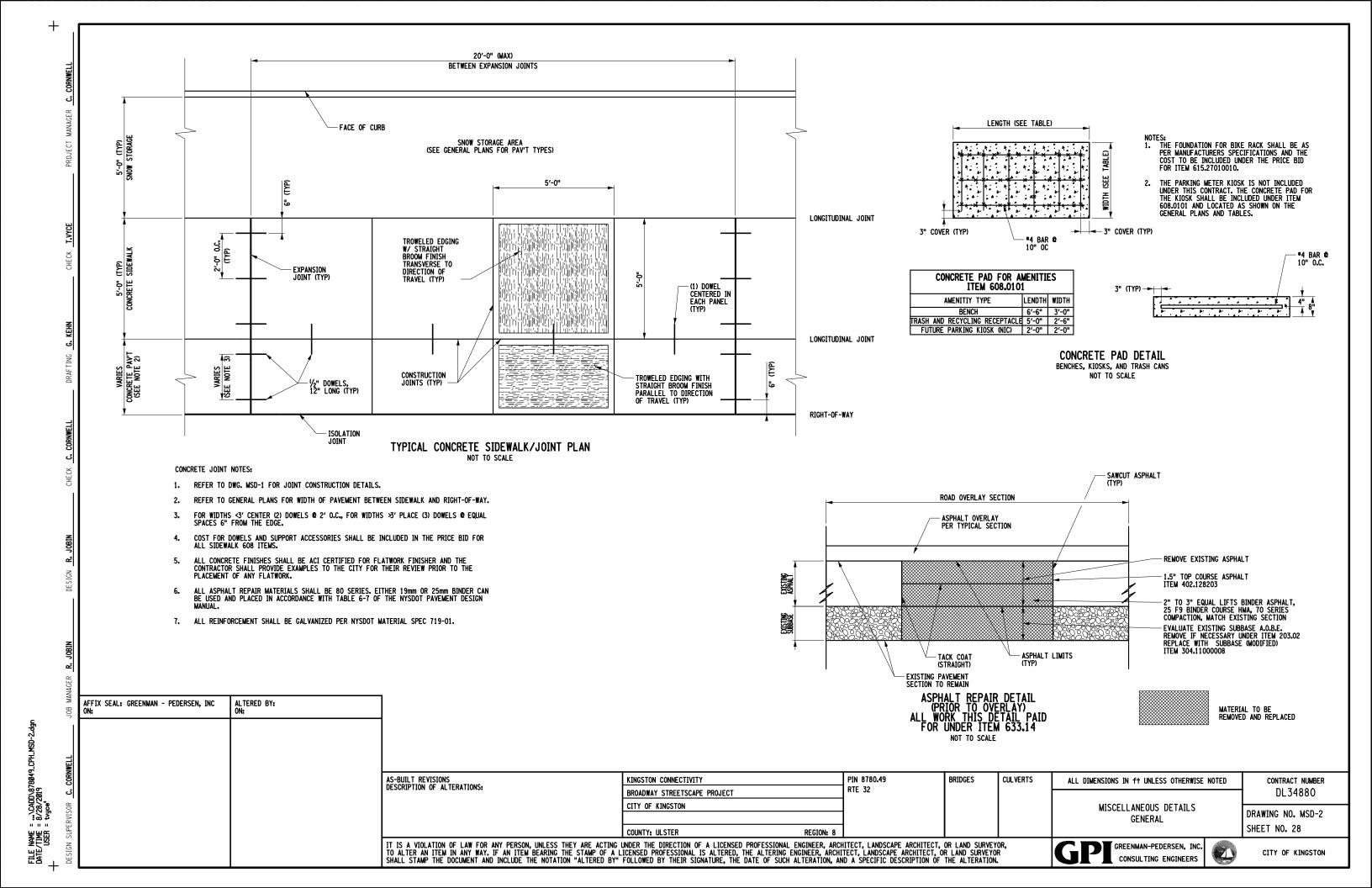
GRADING RE-ESTABLISH APPROACHES TO PRIVATE LAND (DRIVEWAYS/WALKWAYS) 7/5/2019

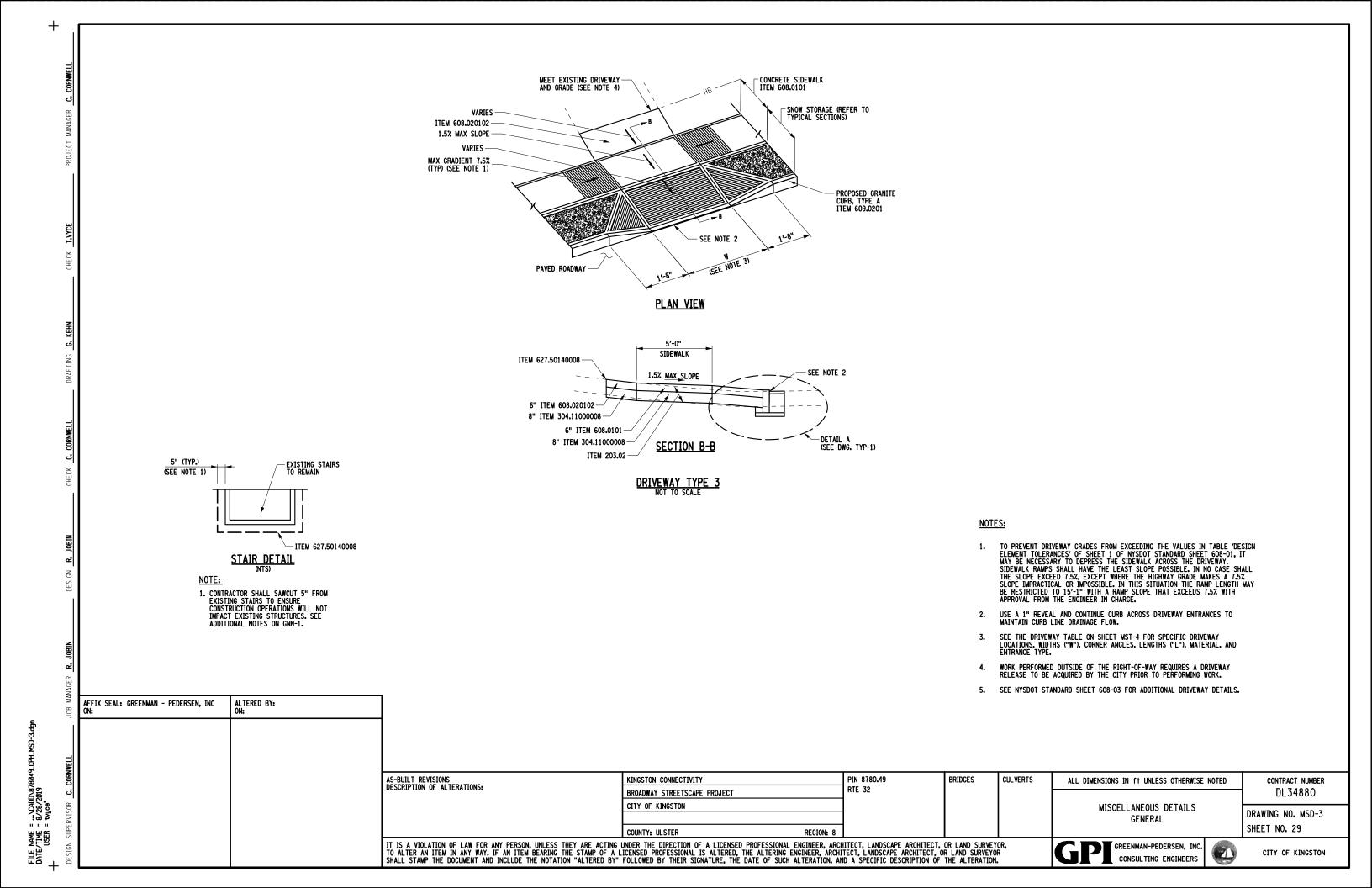
B 14+75 RT 625

KAMRAN KHAN

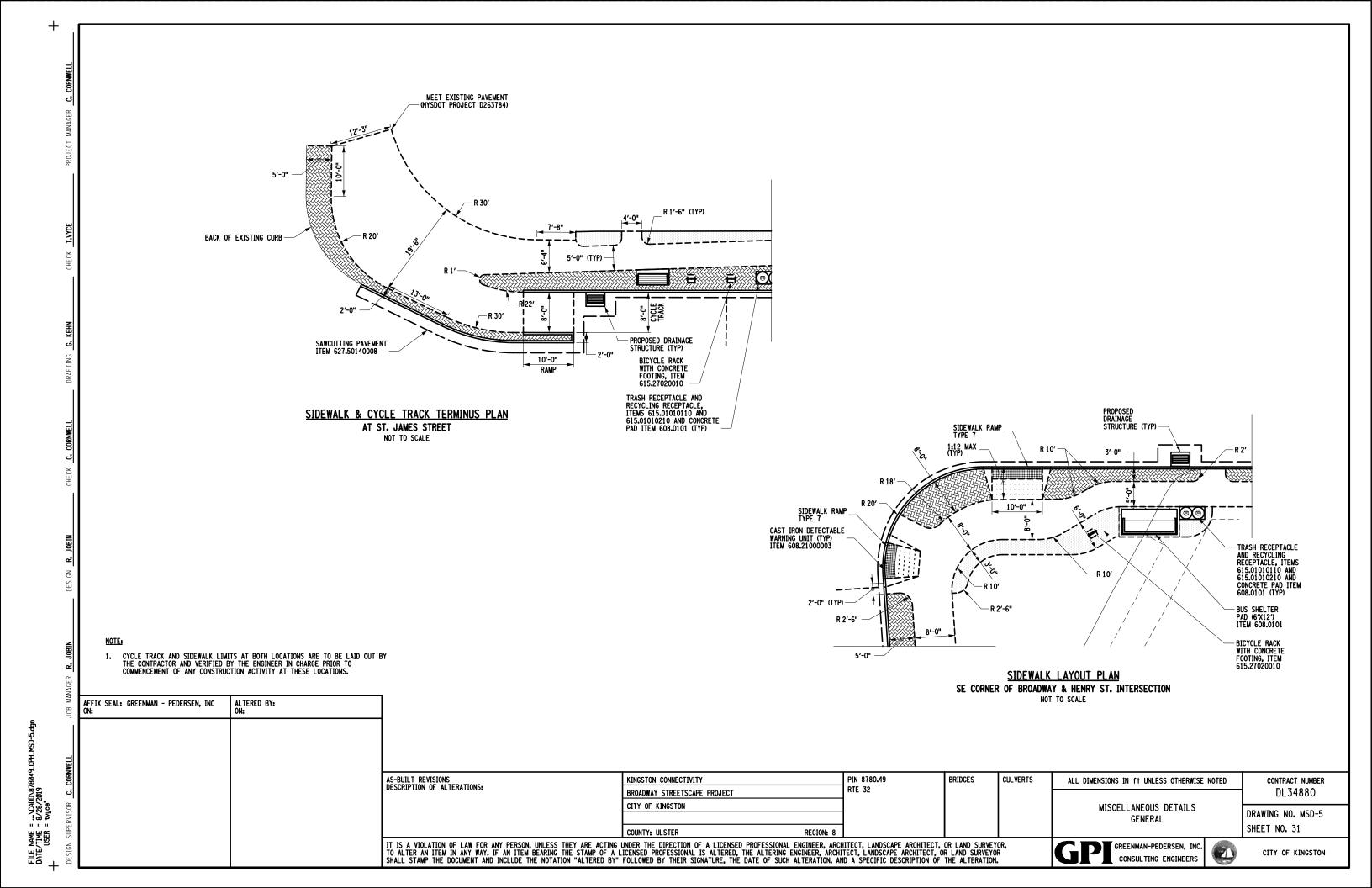
CORNW									
CHECK C. CORNW									
DESIGN R. JOBIN									
OB MANAGER R. JOBIN	AFFIX SEAL: GREENMAN - PEDERSEN, INC	ALTERED BY:]						
OR C. CORNWELL			AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		PIN 8780.49 RTE 32	BRIDGES	CULVERTS	ALL DIMENSIONS IN ++ UNLESS OTHERWISE NOTED	CONTRACT NUMBER DL34880
DESIGN SUPERVISOR C. CORNWELL			IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UTO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LISHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	COUNTY: ULSTER REGION: 8	IITECT, LANDSCAPE ARCHITECT TECT, LANDSCAPE ARCHITECT, D A SPECIFIC DESCRIPTION OF	, OR LAND SURVE OR LAND SURVEY THE ALTERATIO	YOR, OR	MISCELLANEOUS TABLES GPI GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS	DRAWING NO. MST-10 SHEET NO. 26
		•							

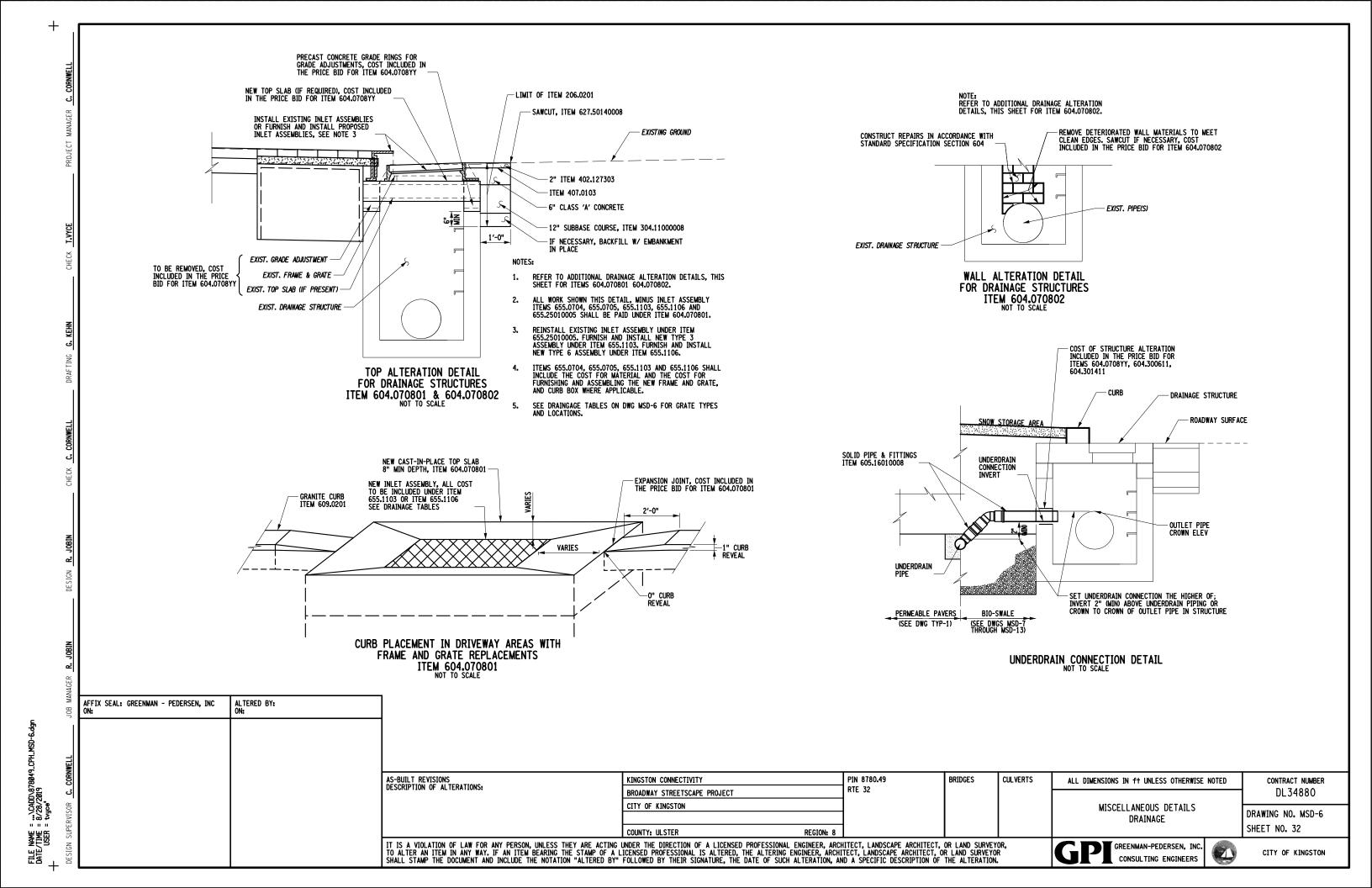


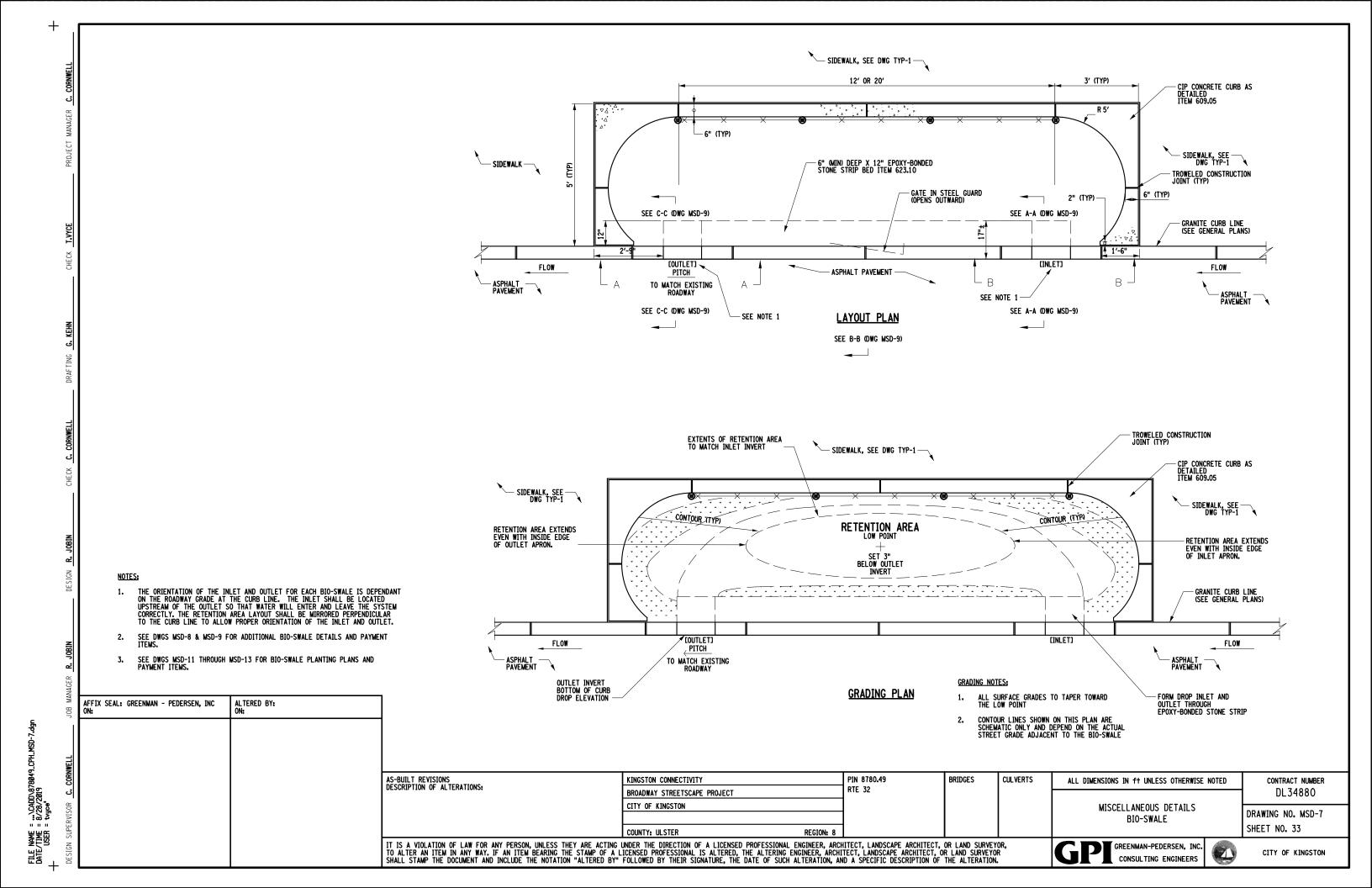




LENGTH OF CONFLICT ZONE (REFER TO GENERAL PLANS) 4" WIDE - WHITE, EDGE LINES, ITEM 685.11 (BOX PERIMETER) DRIVEWAY (SHOWN) INTERSECTION SIMILAR (REFER TO GENERAL PLANS) AT DRIVEWAYS (REFER TO SIGN LOCATION PLANS) (1) WHITE BICYCLE SYMBOL, & (1) WHITE (HALF SIZE) TURNING ARROW PER MODULE, ITEM 685.14 -2' x FULL WIDTH - GREEN SURFACE TREATMENT, ITEM 601.01000018 (SPACED AS SHOWN) MIRROR MODULE AS REQUIRED REFER TO PAVEMENT MARKING PLANS) ₹ V ™ ∰ - MIDPOINT OF CONFLICT ZONE (SEE NOTE) ITEM 645.5102 W/ ITEM 645.81 (1 EACH) - YELLOW CYCLE TRACK CENTER LINE (TYP) ITEM 685.12 CYCLE 1 REFER TO 4" WIDE - YELLOW, NORMAL DOTTED EXTENSION LINE, ITEM 685.12 -4" WIDE - WHITE, NORMAL DOTTED EXTENSION LINES, ITEM 685.11 (EACH SIDE) GREEN SURFACE TREATMENT, ITEM 601.01000018 CENTER CONFLICT ZONE MARKINGS ON THE MIDPOINT OF THE CONFLICT ZONE. SPACE BETWEEN MARKINGS MAY BE REDUCED BY A MAXIMUM OF 6 INCHES TO PROVIDE THE MAXIMUM NUMBER OF MARKINGS WITHIN THE CONFLICT ZONE. 4'-0" 8'-0" (SEE NOTE) MODULE WIDTH CYCLE TRACK CONFLICT ZONE MARKINGS **BIKE BOX MARKINGS** AT INTERSECTIONS, DRIVEWAYS AND BUS LOADING ZONES
NOT TO SCALE AT SIGNALIZED INTERSECTIONS
NOT TO SCALE ALTERED BY: ON: AFFIX SEAL: GREENMAN - PEDERSEN, INC FILE NAME = ...\CADD\878849.CPH_MSD-4.dgn DATE/TIME = 8/28/2019 USER = tvyce' AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 BRIDGES CULVERTS KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON MISCELLANEOUS DETAILS DRAWING NO. MSD-4 GENERAL SHEET NO. 30 COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC. CITY OF KINGSTON CONSULTING ENGINEERS







AFFIX SEAL: GREENMAN - PEDERSEN, INC

EXTENT OF NEW CURB VARIES - (REFER TO GENERAL PLANS) PROPOSED PAVEMENT SURFACE TYPICAL CURB JOINT INLET OR OUTLET -SLOT CUT IN CURB COST INCLUDED IN PRICE BID FOR ITEM 609.0201 - GRANITE CURB

SECTION AT CURB INLET/OUTLET NOT TO SCALE

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

ALTERED BY: ON:

- 1. SEE DWGS MSD-7, MSD-8 & MSD-10 FOR ADDITIONAL BIO-SWALE DETAILS.
- 2. SEE DWGS MSD-11 THROUGH MSD-13 FOR BIO-SWALE PLANTING PLANS.

- D-RING Connector (Typ) - 3/8" GALVANIZED LANDSCAPE CHAIN, POWDER COATED BLACK 2'-0" GALVANIZED I-BEAM TYPE POST, POWDER COATED BLACK, ACTUAL SIZE 2.25"x1.75" 4'-0" -1% CROWN ON CONCRETE FOOTING 6" MIN - CLASS D CONCRETE FOOTING 4'-4"x6" MIN. SET IN SONOTUBE (TYP)

BIO-SWALE BOLLARD NOTES:

PIN 8780.49

RTE 32

- POST AND CAP SHALL BE SNUG COTTAGE HARDWARE CENTRAL PARK STYLE CAP AND POST OR SIMILAR AS APPROVED BY RESIDENT ENGINEER.
- 2. ALL STEEL SHALL CONFORM TO ASTM-A36 SPECIFICATION.
- 3. ALL STEEL TO BE PAINTED BLACK AND FOLLOW SPECIAL SPECIFICATION FOR ITEM 608.72126108.
- 4. BOLLARD SPACING FOR FOUR-BOLLARD BIO-SWALES SHALL BE 6'-8" ON CENTER; SPACING FOR THREE-BOLLARD BIO-SWALES SHALL BE 6'-0" ON CENTER.
- 5. AN ADDITIONAL 10% SHALL BE ADDED TO THE MEASURED LENGTH NEEDED FOR EACH SECTION OF CHAIN TO ALLOW FOR PROPER SAG.
- 6. ALL WORK THIS DETAIL SHALL BE PAID FOR UNDER ITEM 608.72126108.

BIO-SWALE BOLLARDS ITEM 608.72126108

BRIDGES

BIO-SWALE
GP GREENMAN-PEDERSE CONSULTING ENGI

CONTRACT NUMBER DL34880 DRAWING NO. MSD-8

SHEET NO. 34

CULVERTS

SEN, INC. SINEERS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS DETAILS

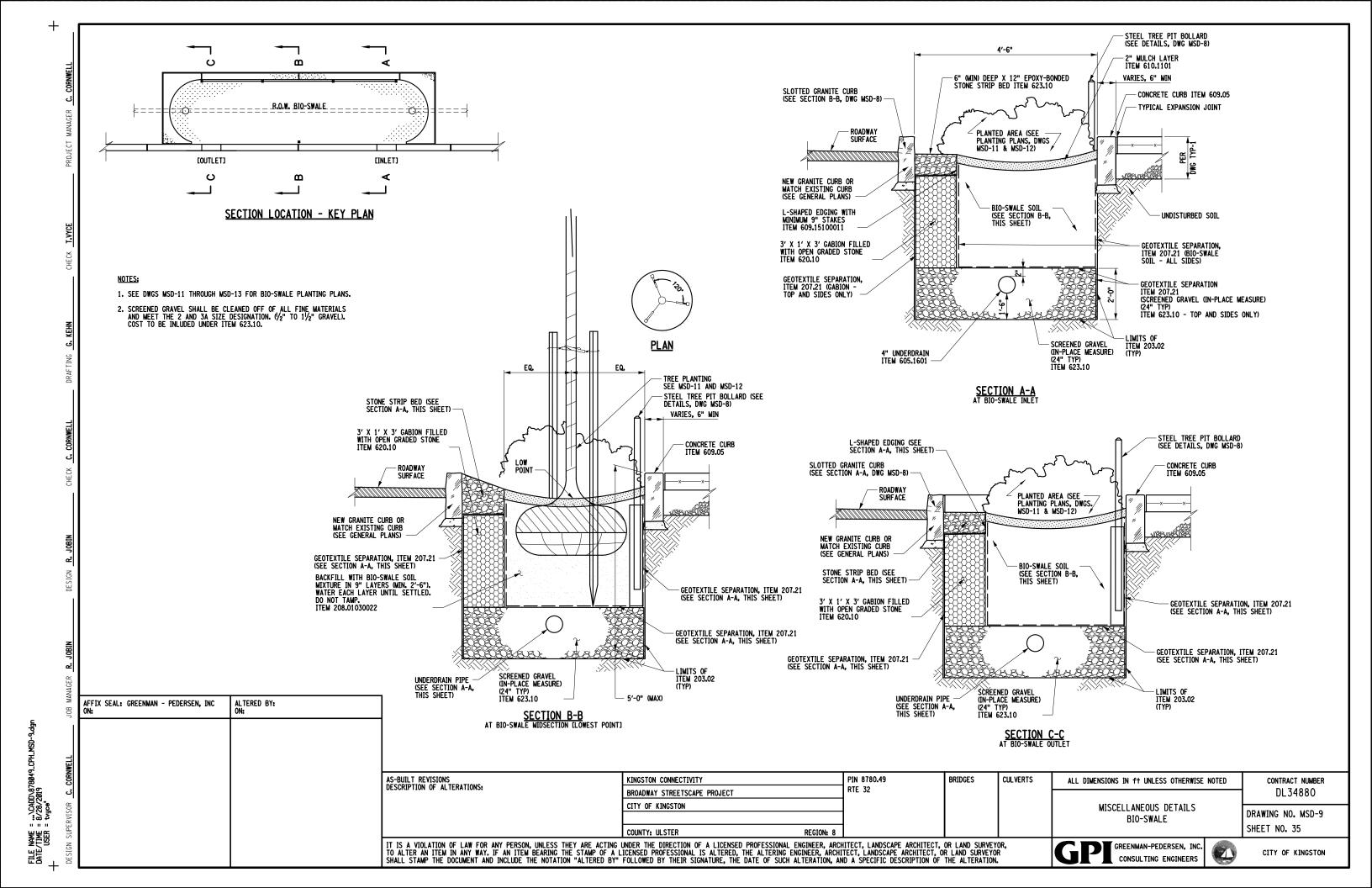


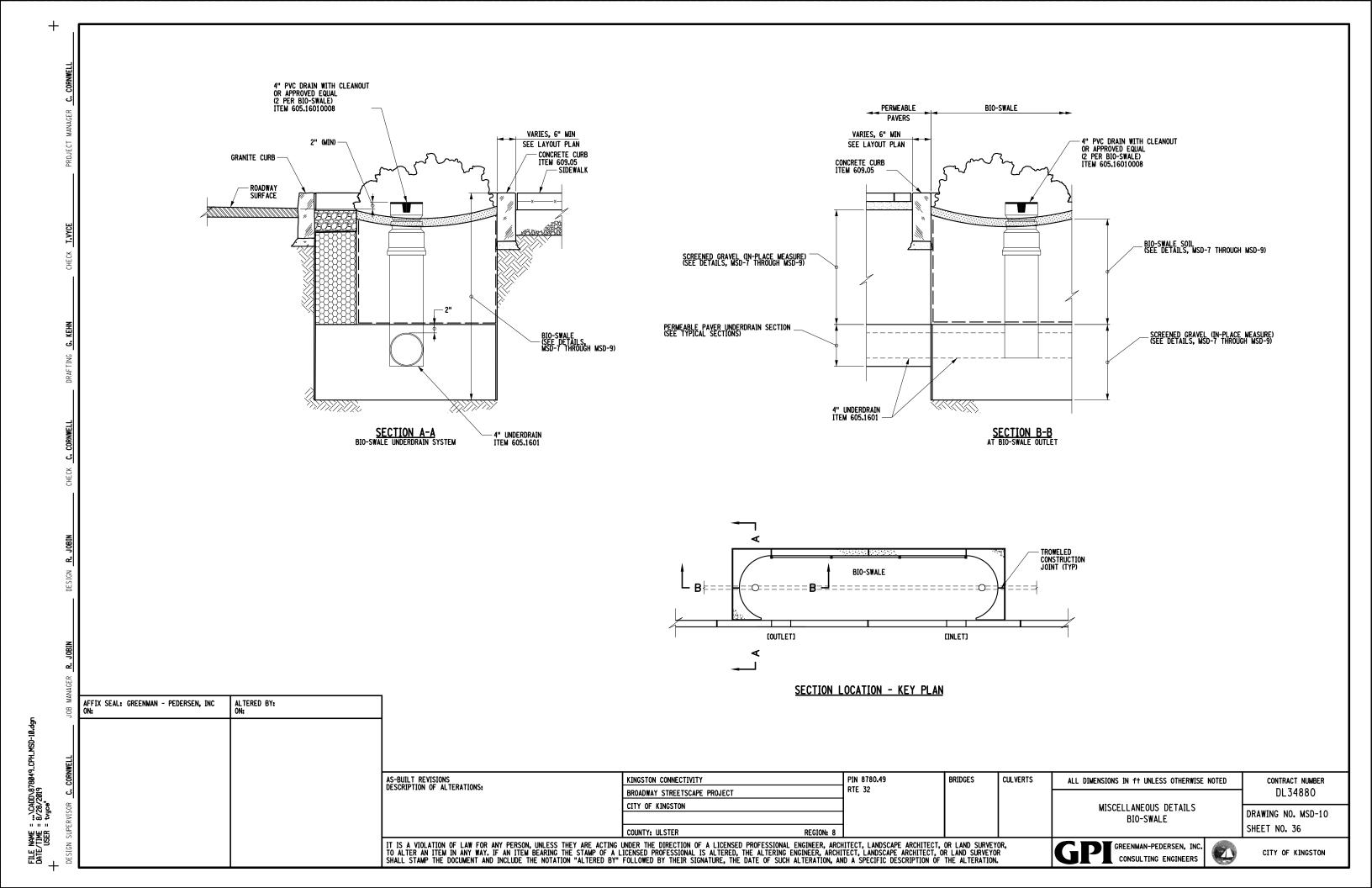
COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

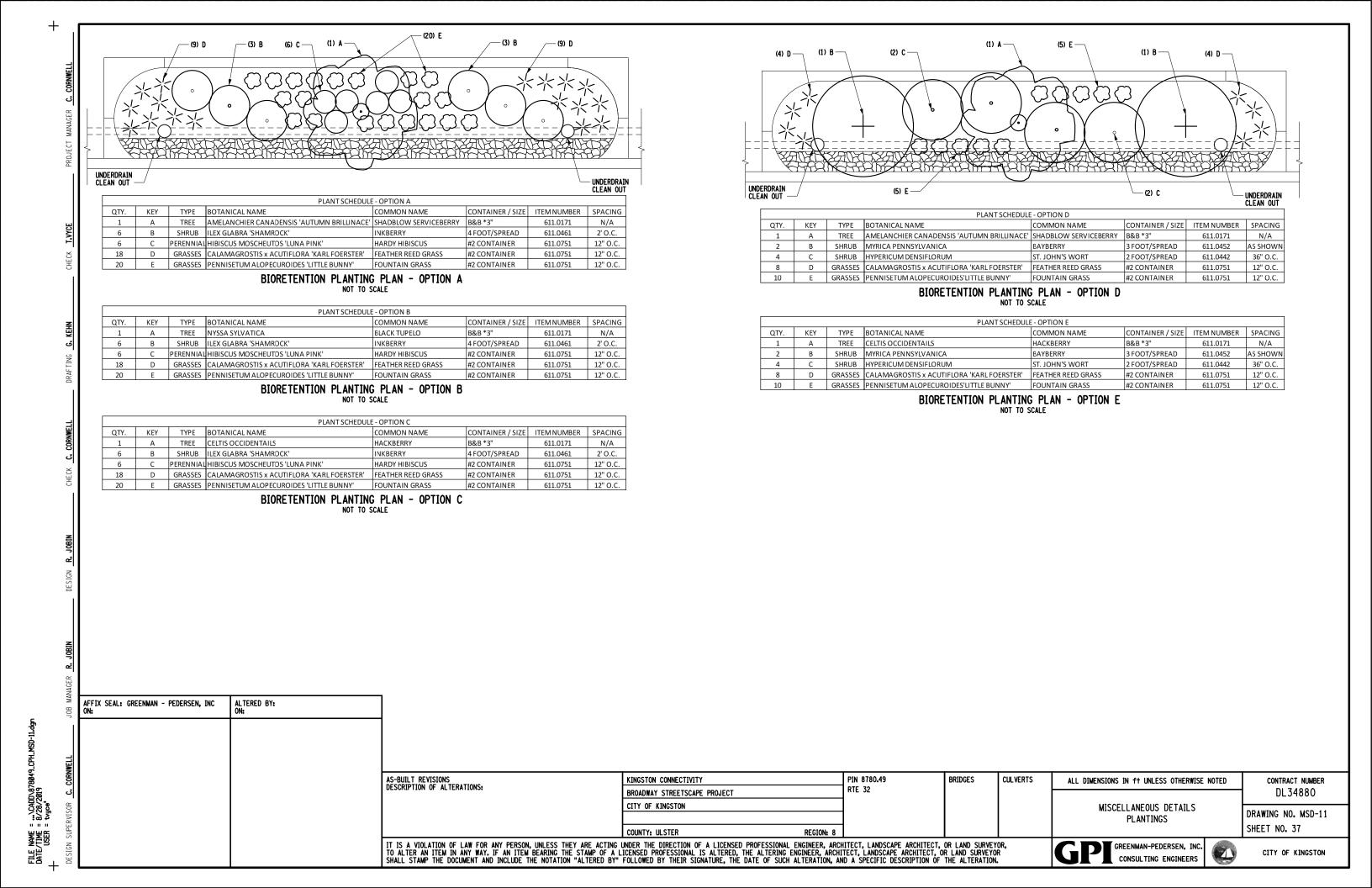
KINGSTON CONNECTIVITY

CITY OF KINGSTON

BROADWAY STREETSCAPE PROJECT









AFFIX SEAL: GREENMAN -

UNDERDRAIN CLEAN OUT Α

(1) B

UNDERDRAIN CLEAN OUT

QTY.

4

QTY.

10

KEY

Α

D

KEY

Α

D

TYPE BOTANICAL NAME TREE AMELANCHIER CANADENSIS 'ROBIN HILL PINK' SHRUB ARONIA ARBUTIFOLIA 'BRILLIANTISSIMA' GRASSES PENNISETUM ALOPECUROIDES' LITTLE BUNNY'

(5) C -

PLANT SCHEDULE - OPTION J COMMON NAME CONTAINER / SIZE | ITEM NUMBER | SPACING SHADBLOW SERVICEBERRY B&B *3" 611.0171 RED CHOKEBERRY 4 FOOT/SPREAD 611.0462 FOUNTAIN GRASS #2 CONTAINER 611.0751 D PERENNIAL HIBISCUS MOSCHEUTOS 'LUNA PINK' HARDY HIBISCUS #2 CONTAINER 611.0751

(1) A-

(5) D-

PHYSOCARPUS OPULIFOLIUS

SHRUB ARONIA ARBUTIFOLIA 'BRILLIANTISSIMA'

E GRASSES PENNISETUMALOPECUROIDES 'LITTLE BUNNY'

CELTIS OCCIDENTAILS

(2) B-

PHYSOCARPUS OPULIFOLIUS

ARONIA ARBUTIFOLIA 'BRILLIANTISSIMA'

GRASSES PENNISETUMALOPECUROIDES 'LITTLE BUNNY'

AMELANCHIER CANADENSIS 'ROBIN HILL PINK'

GRASSES CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER' FEATHER REED GRASS

GRASSES CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER' FEATHER REED GRASS

TYPE BOTANICAL NAME

TYPE BOTANICAL NAME

TREE

SHRUB

TREE

SHRUB

SHRUB

(5) D -

PLANT SCHEDULE - OPTION F

BIORETENTION PLANTING PLAN - OPTION F

BIORETENTION PLANTING PLAN - OPTION G

NOT TO SCALE

NOT TO SCALE PLANT SCHEDULE - OPTION G

COMMON NAME

RED CHOKEBERRY

FOUNTAIN GRASS

COMMON NAME

RED CHOKEBERRY

FOUNTAIN GRASS

COMMON NINEBARK

(2) B -

HACKBERRY

COMMON NINEBARK

SHADBLOW SERVICEBERRY

(4) E -

CONTAINER / SIZE | ITEM NUMBER | SPACING

611.0171

611.0452

611.0462

611.0751

611.0171

611.0452

611.0751

611.0751

- UNDERDRAIN CLEAN OUT

N/A

36" O.C.

12" O.C.

12" O.C.

KINGSTON CONNECTIVITY

— (3) D

CONTAINER / SIZE | ITEM NUMBER | SPACING

UNDERDRAIN

N/A

AS SHOWN

36" O.C.

12" O.C.

12" O.C.

N/A

AS SHOWN

36" O.C.

12" O.C.

12" O.C.

CLEAN OUT

-(2) C (1) B-

B&B *3"

B&B *3"

-(5) C

3 FOOT/SPREAD

4 FOOT/SPREAD

#2 CONTAINER

#2 CONTAINER

3 FOOT/SPREAD

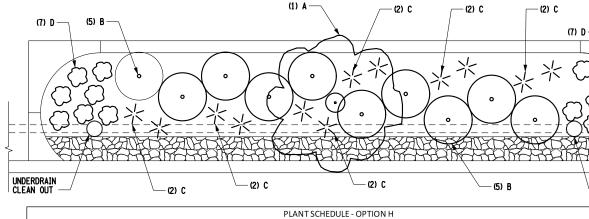
4 FOOT/SPREAD

#2 CONTAINER

#2 CONTAINER

BIORETENTION PLANTING PLAN - OPTION J

	NOT I	JUSCALE
PEDERSEN, INC	ALTERED BY: ON:	
		AS-BUILT REVISIONS
		DESCRIPTION OF ALTERATIONS:

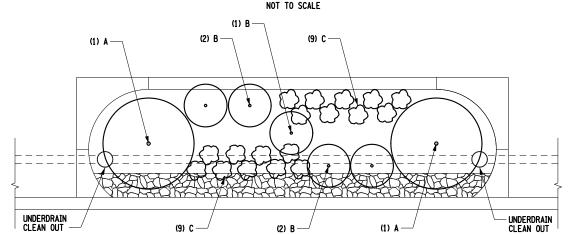


	PLANT SCHEDULE - OPTION H										
QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING				
1	Α	TREE	AMELANCHIER CANADENSIS 'AUTUMN BRILLINACE'	SHADBLOW SERVICEBERRY	B&B *3"	611.0171	N/A				
10	В	PERENNIAL	SYMPHYTRICHUM NOVAE-ANGLIAE	NEW ENGLAND ASTER	3" POT	611.0751	24" O.C.				
12	С	PERENNIAL	HIBISCUS MOSCHEUTOS 'LUNA PINK'	HARDY HIBISCUS	#2 CONTAINER	611.0751	12" O.C.				
14	D	GRASSES	PENNISETUM ALOPECUROIDES' LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.				

BIORETENTION PLANTING PLAN - OPTION H NOT TO SCALE

	PLANT SCHEDULE - OPTION I										
QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING				
1	А	TREE	CELTIS OCCIDENTAILS	HACKBERRY	B&B *3"	611.0171	N/A				
10	В	PERENNIAL	SYMPHYTRICHUM NOVAE-ANGLIAE	NEW ENGLAND ASTER	3" POT	611.0751	24" O.C.				
12	С	PERENNIAL	HIBISCUS MOSCHEUTOS 'LUNA PINK'	HARDY HIBISCUS	#2 CONTAINER	611.0751	12" O.C.				
14	D	GRASSES	PENNISETUM ALOPECUROIDES' LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.				

BIORETENTION PLANTING PLAN - OPTION I

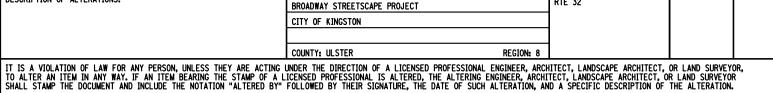


PLANT SCHEDULE - OPTION P											
QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING				
2	Α	SHRUB	ITEA VIRGINICA "HENRY'S GARNET"	VIRGINIA SWEETSPIRE	#3 CONTAINER	611.0461	48" O.C.				
5	В	SHRUB	XANTHORHIZA SIMPLICISSIMA	YELLOWROOT	#1 CONTAINER	611.0442	36" O.C.				
18	C	PERENNIAL	MEEHANIA CORDATA	CREEPING MINT	FLATS	611 0751	12" O.C.				

CULVERTS

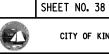
BIORETENTION PLANTING PLAN - OPTION P NOT TO SCALE

BRIDGES



PIN 8780.49

RTE 32



ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS DETAILS

GREENMAN-PEDERSEN, INC.

CONSULTING ENGINEERS

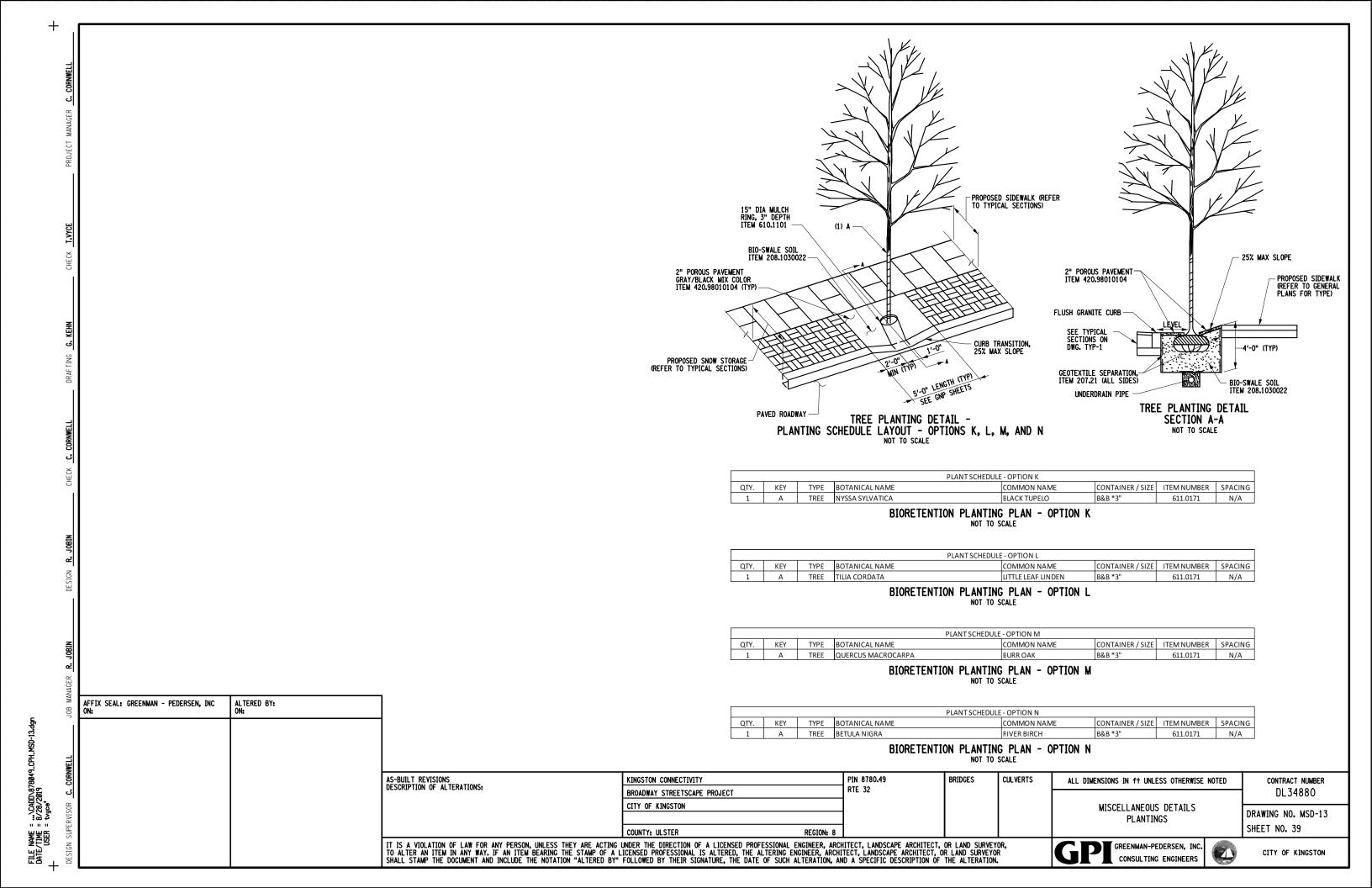
PLANTINGS

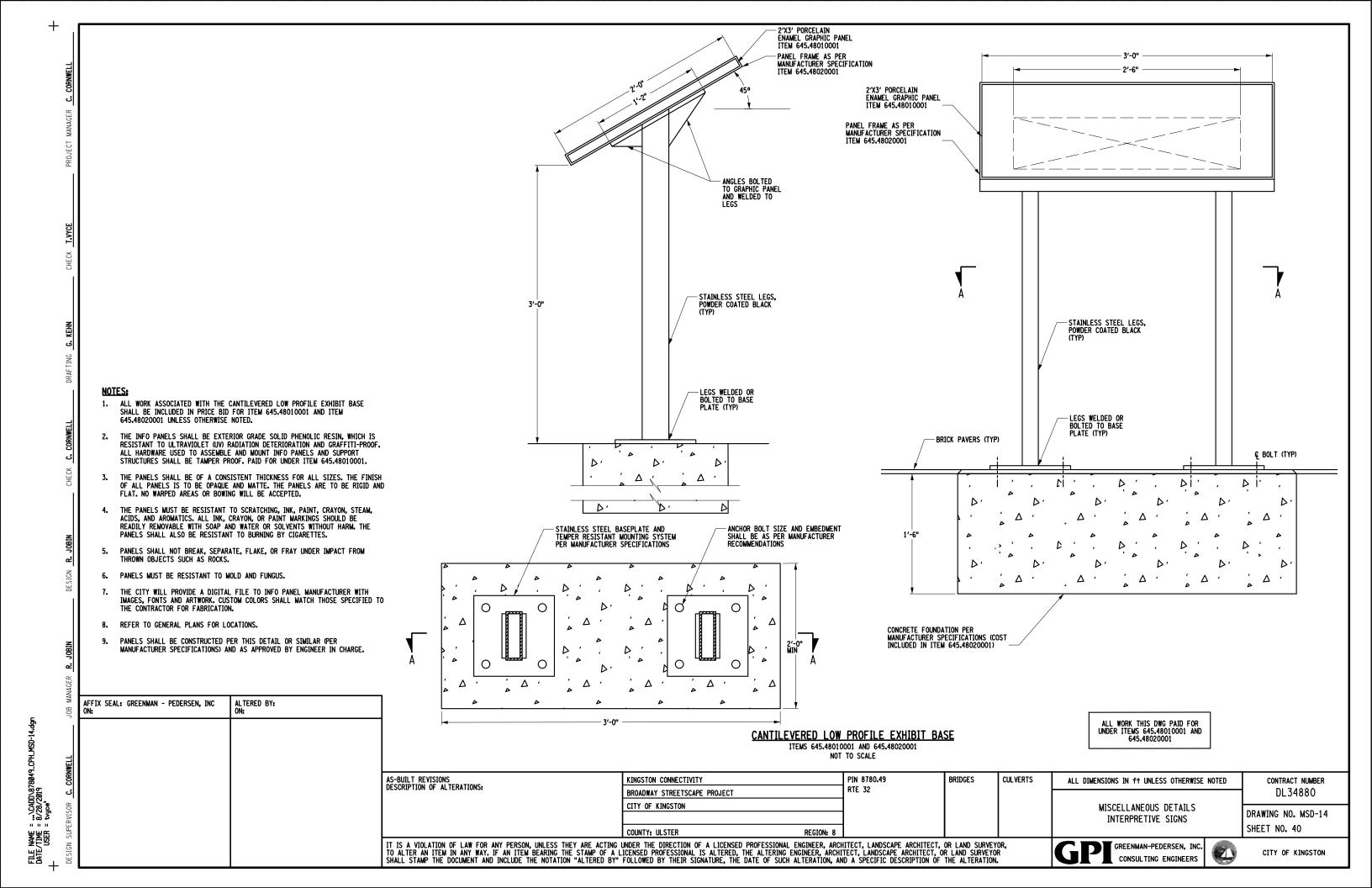
-UNDERDRAIN CLEAN OUT

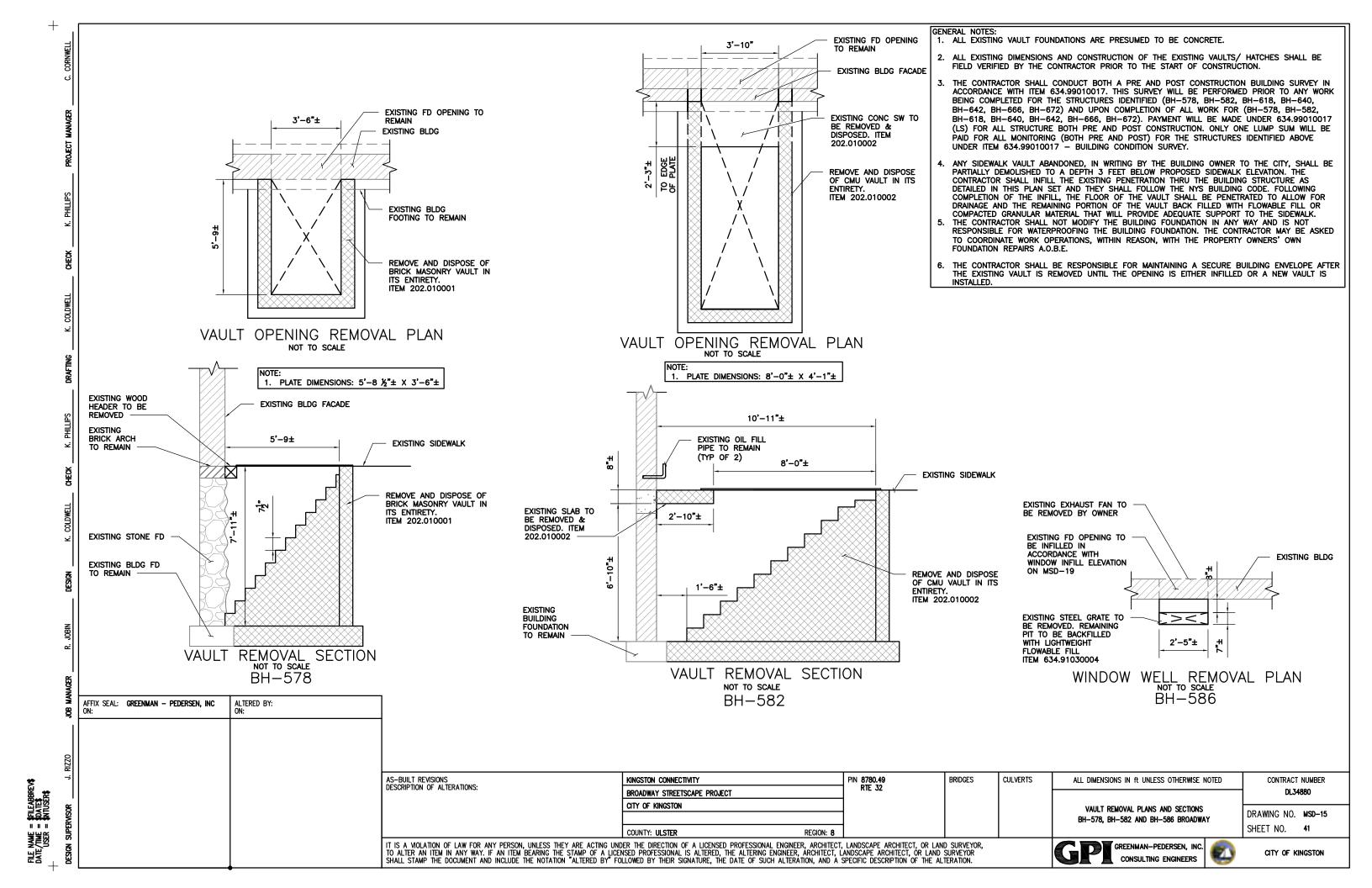
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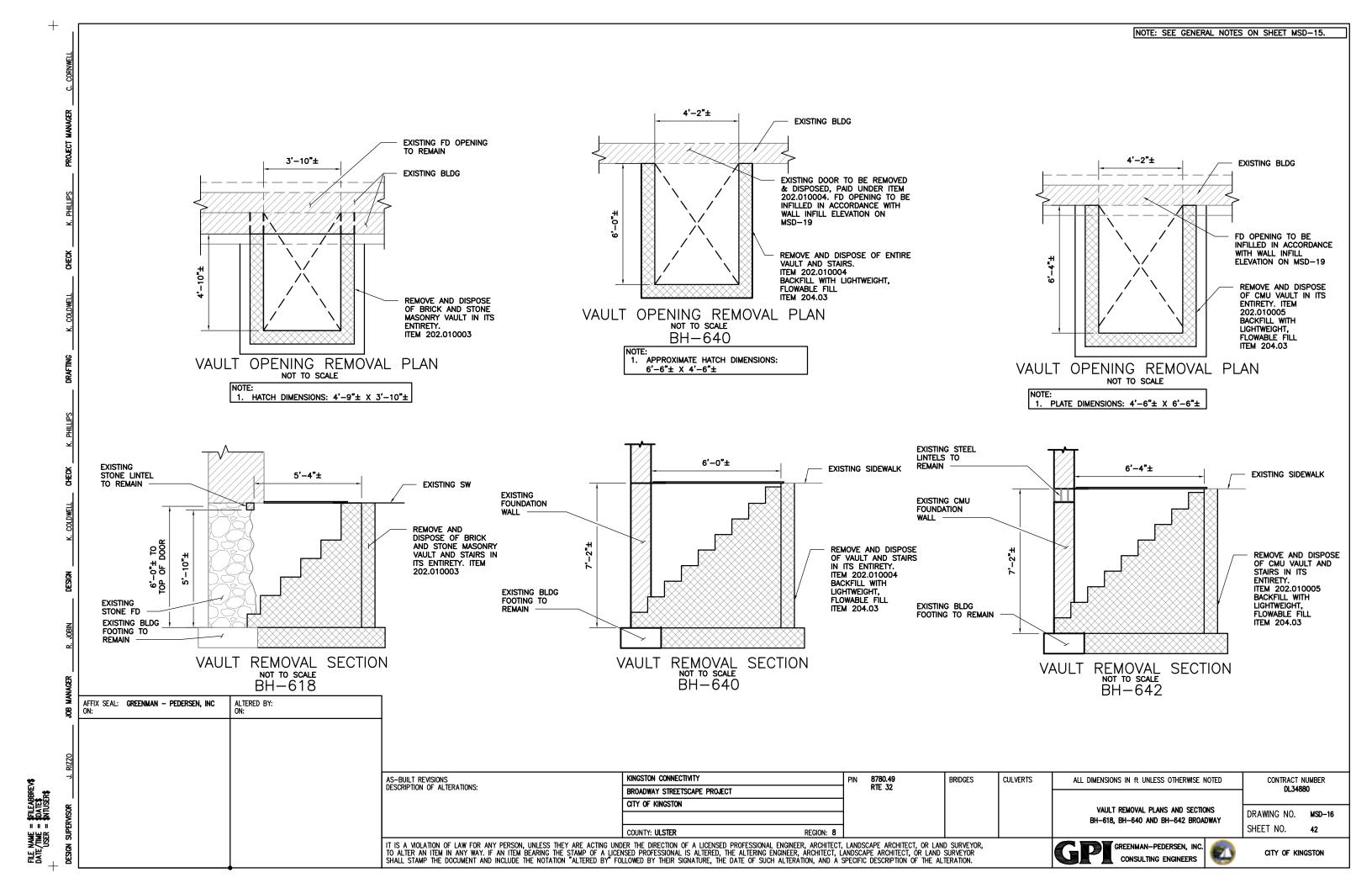
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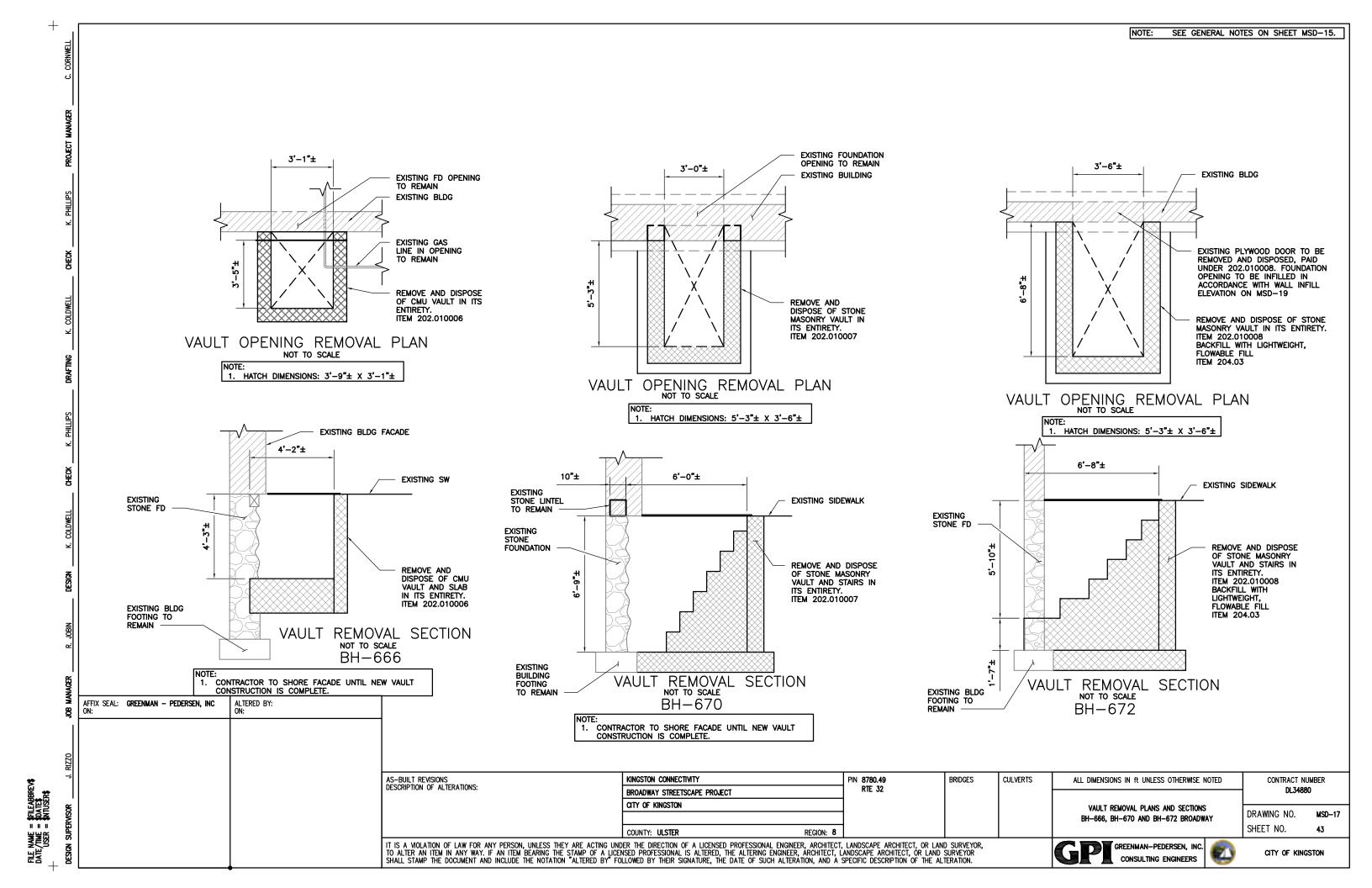
DL34880

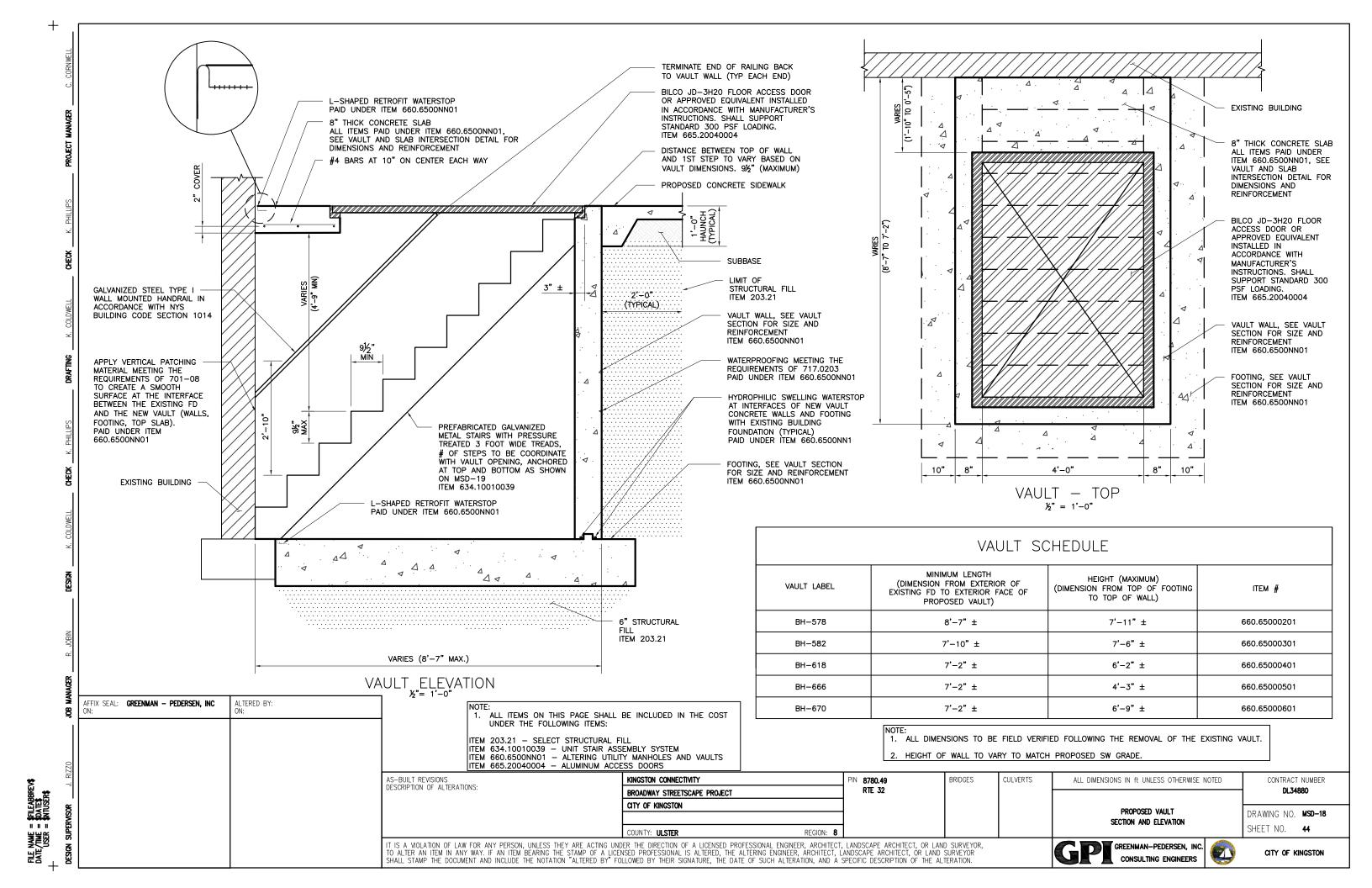


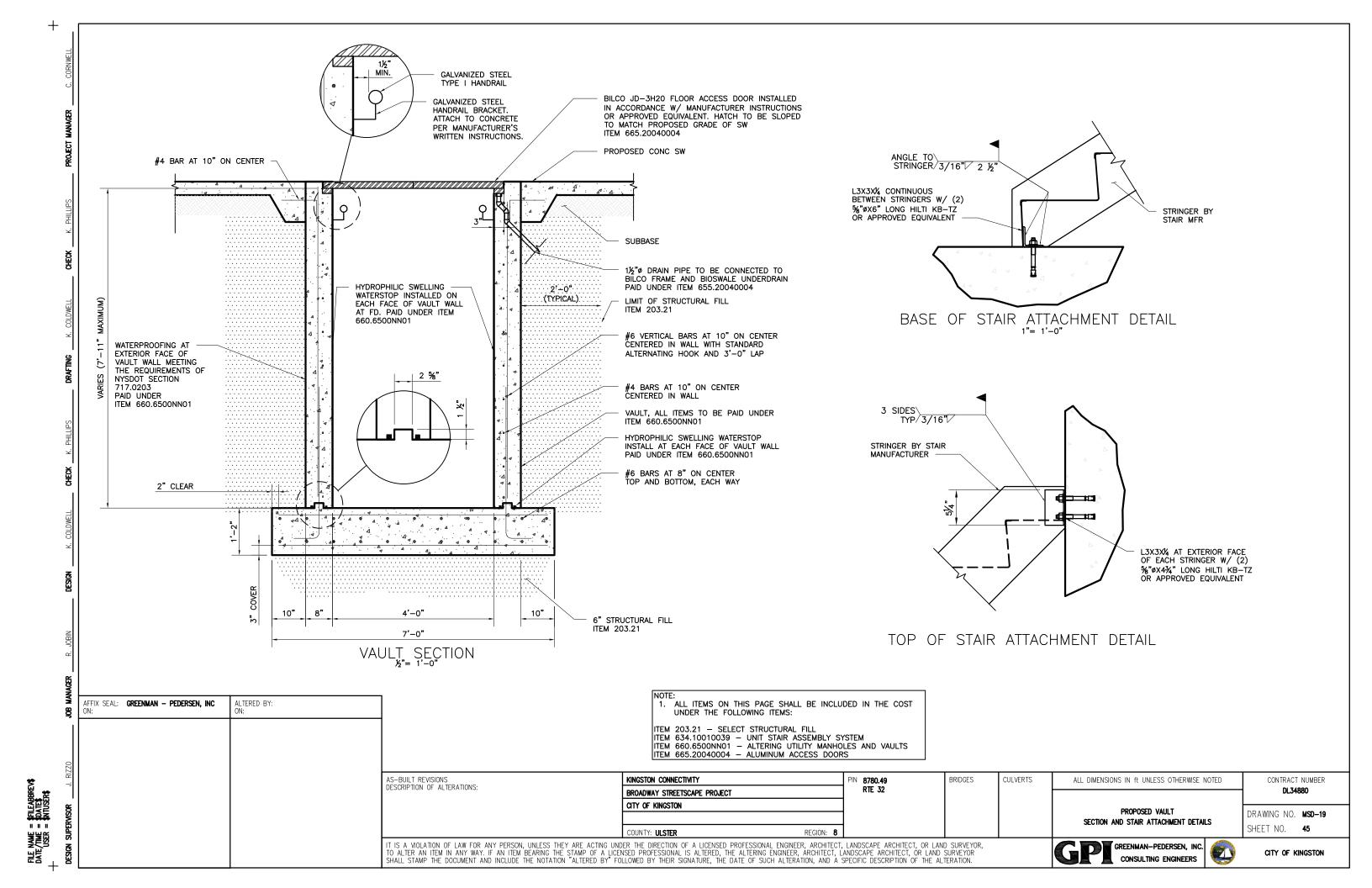


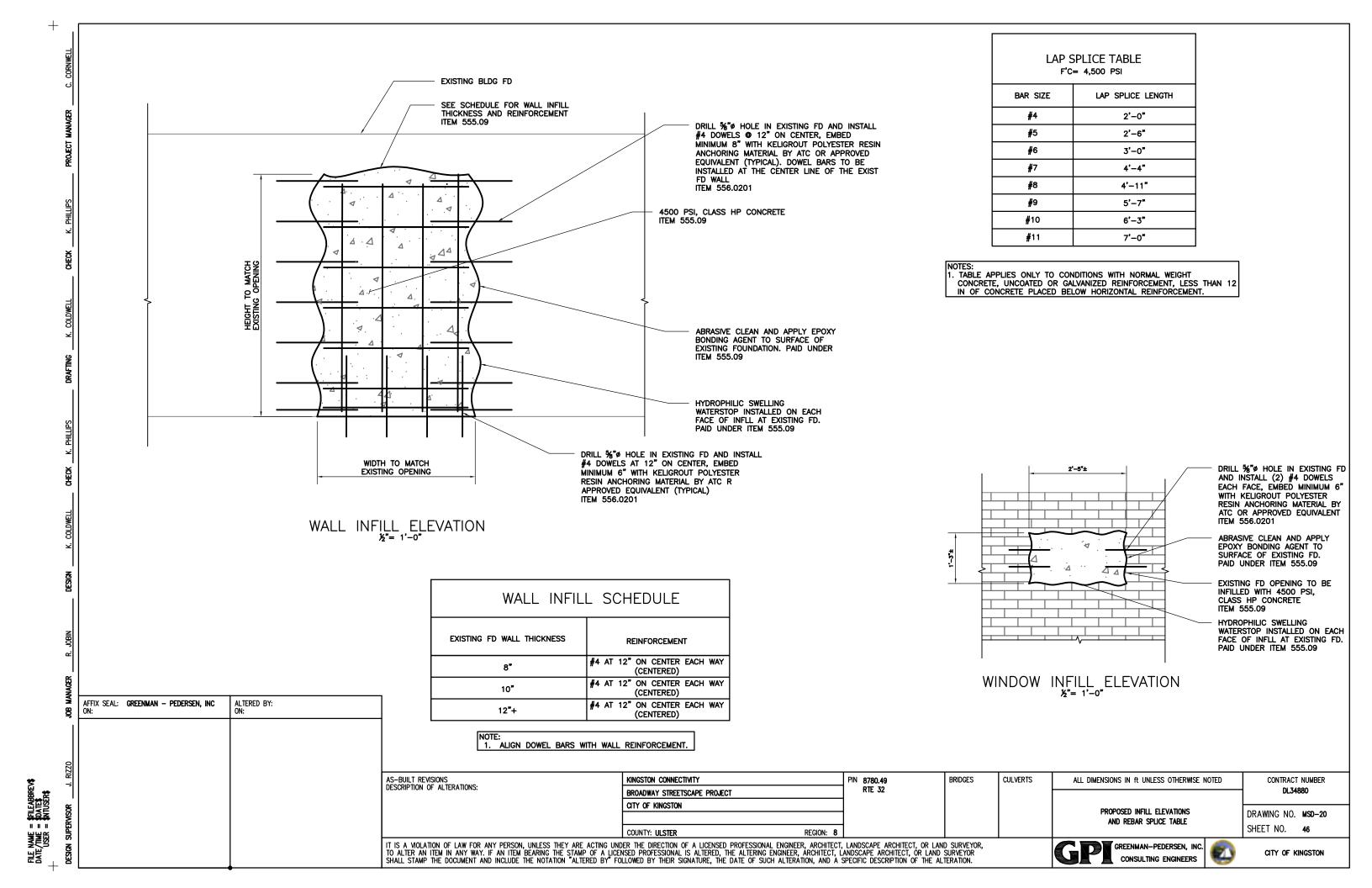


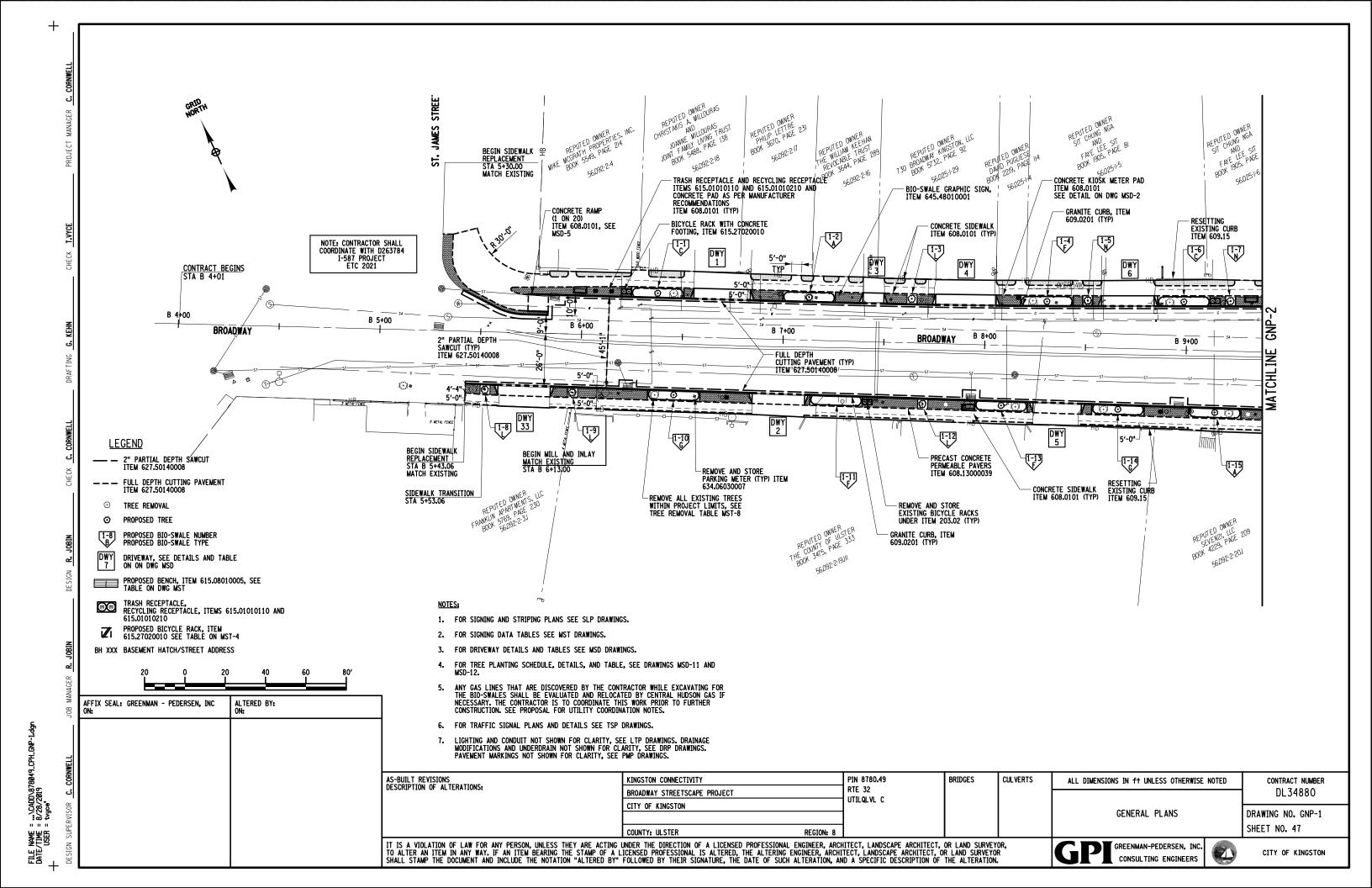


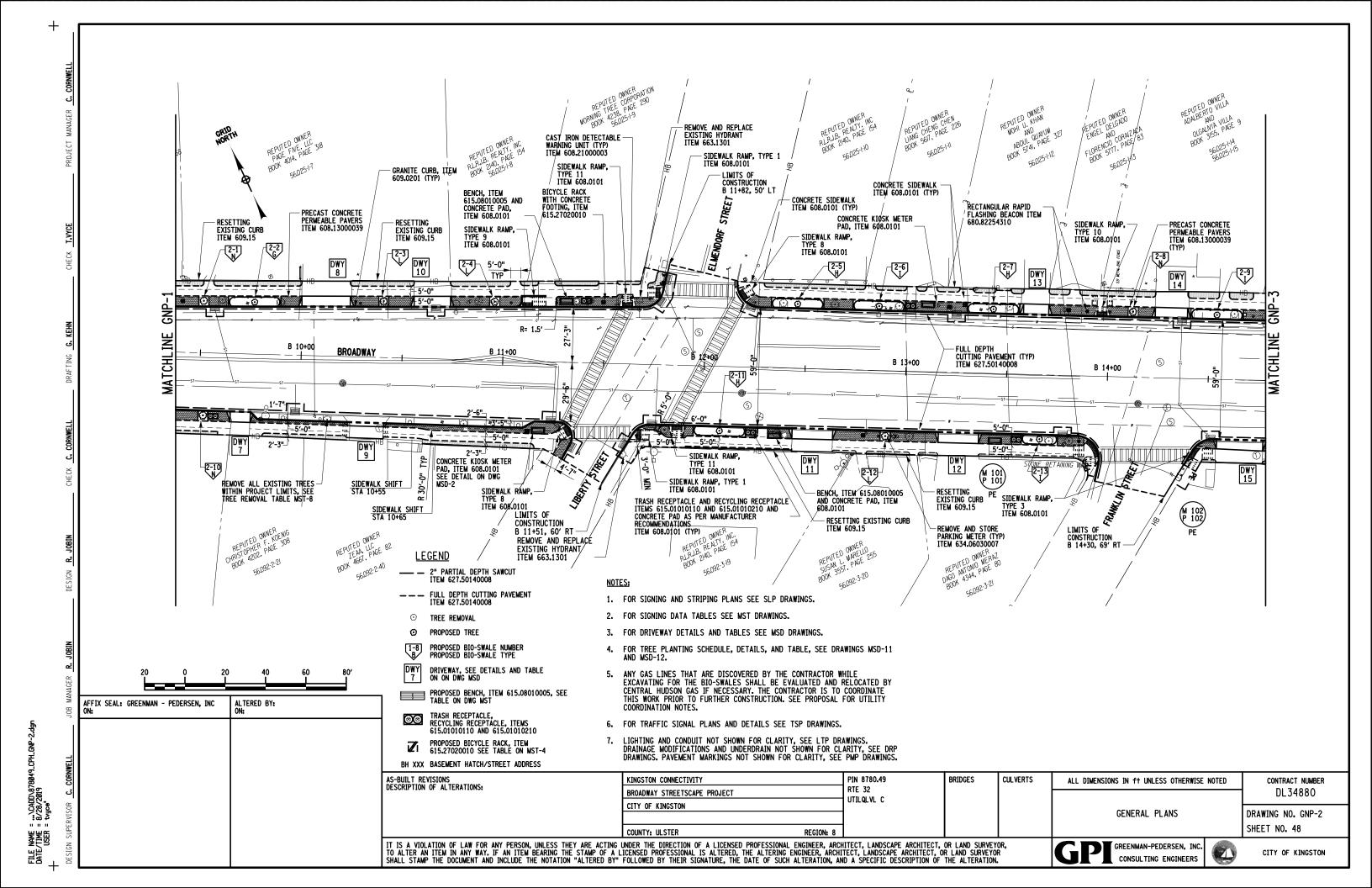


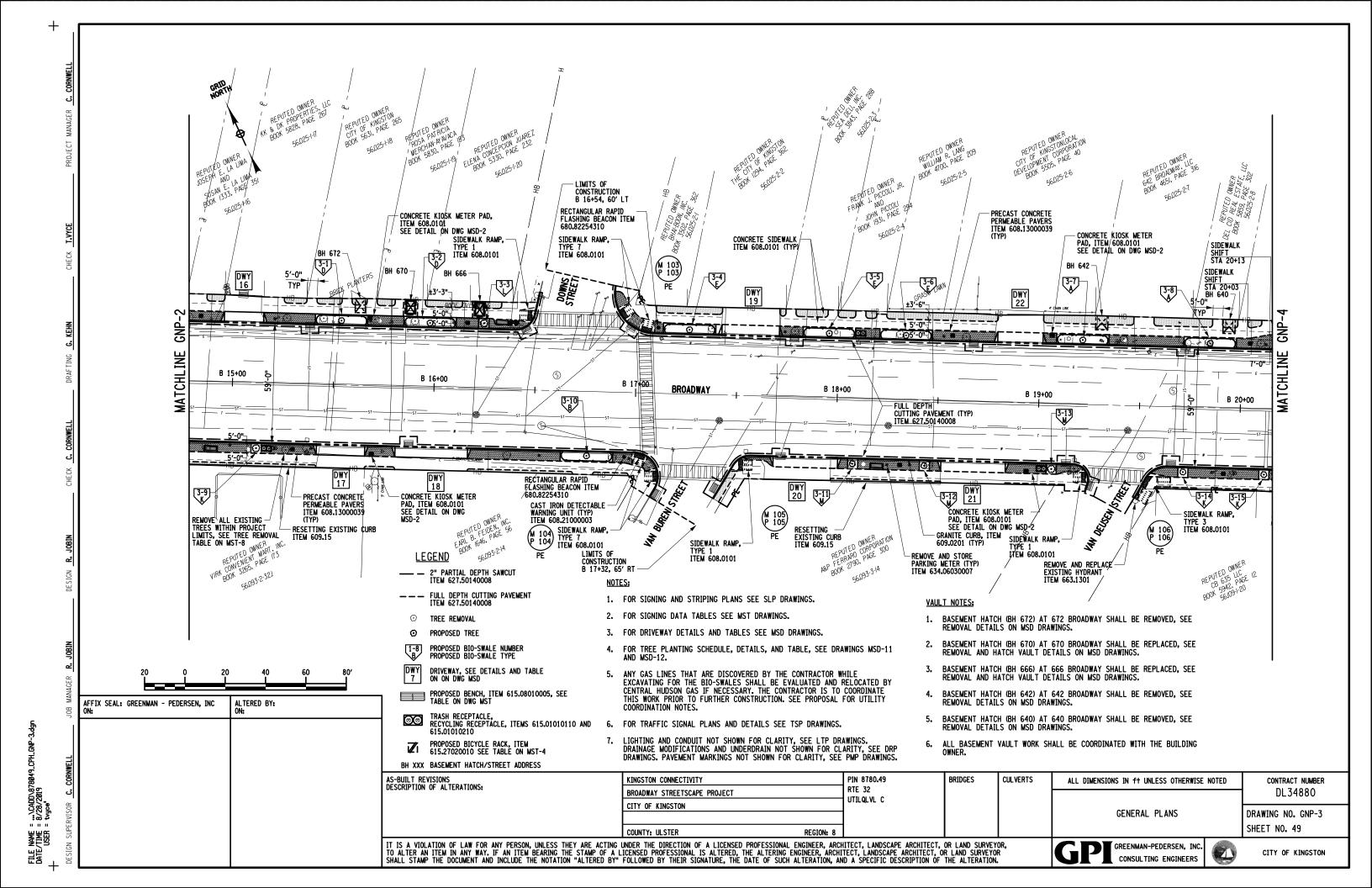


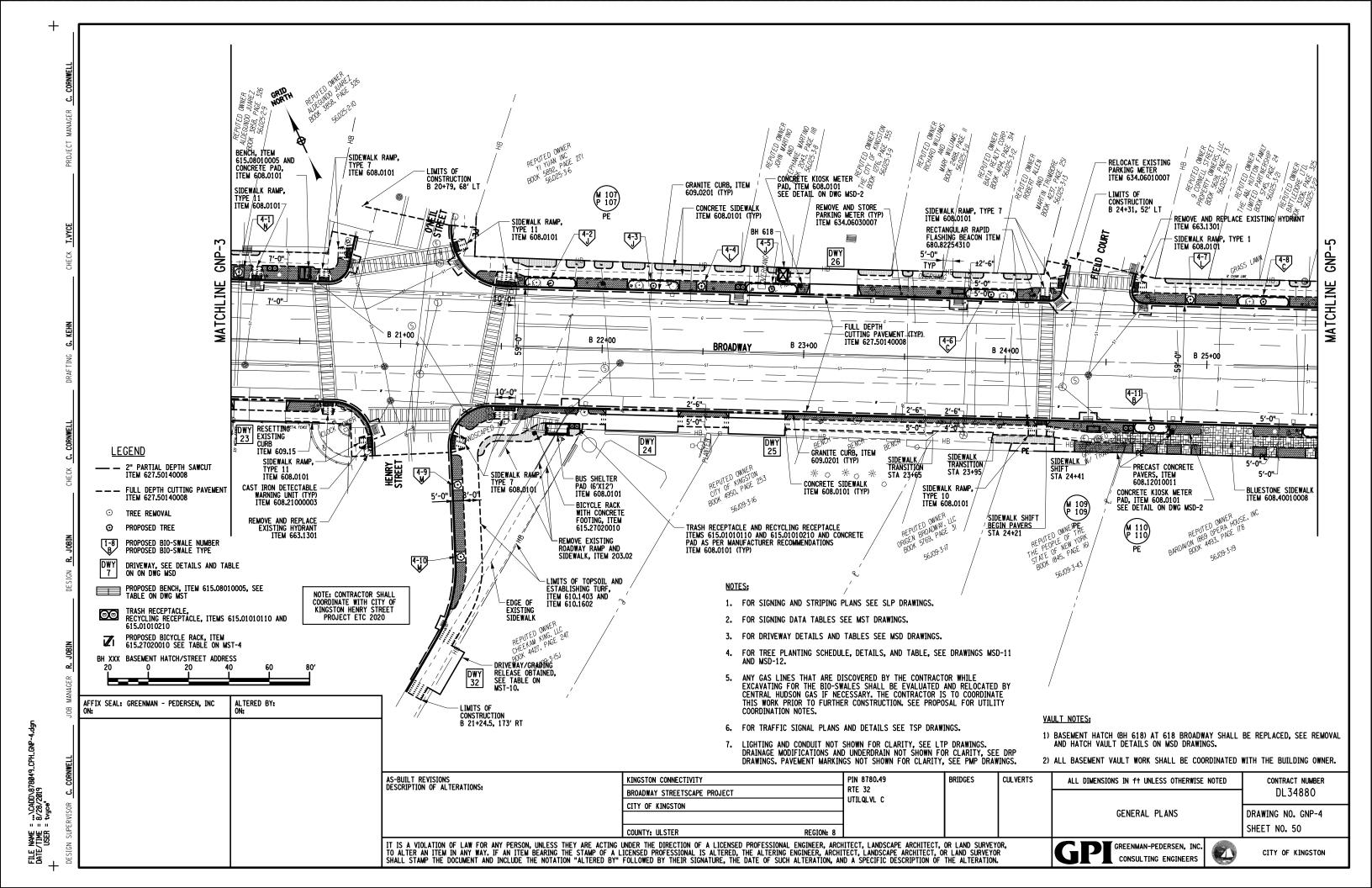


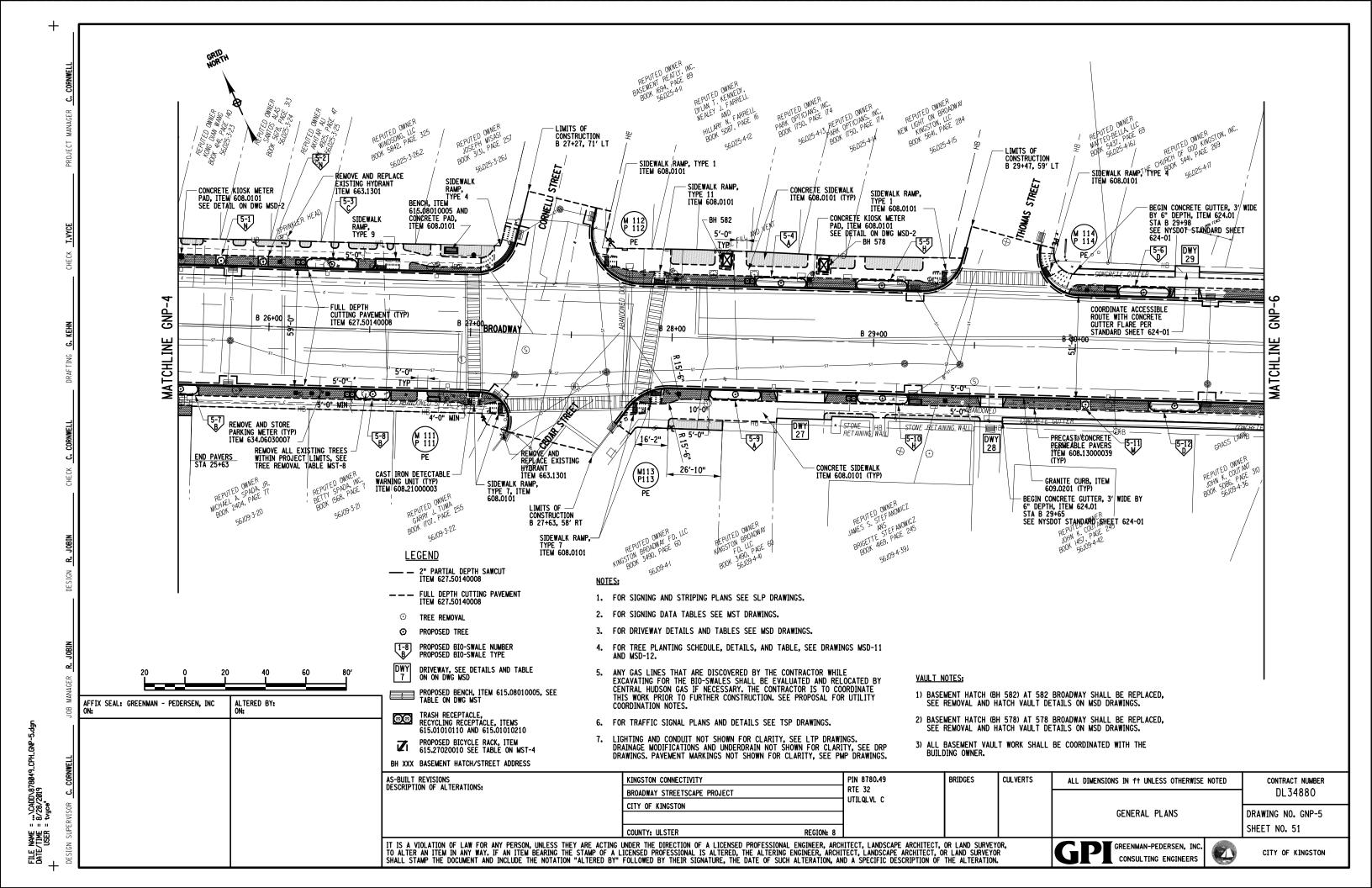


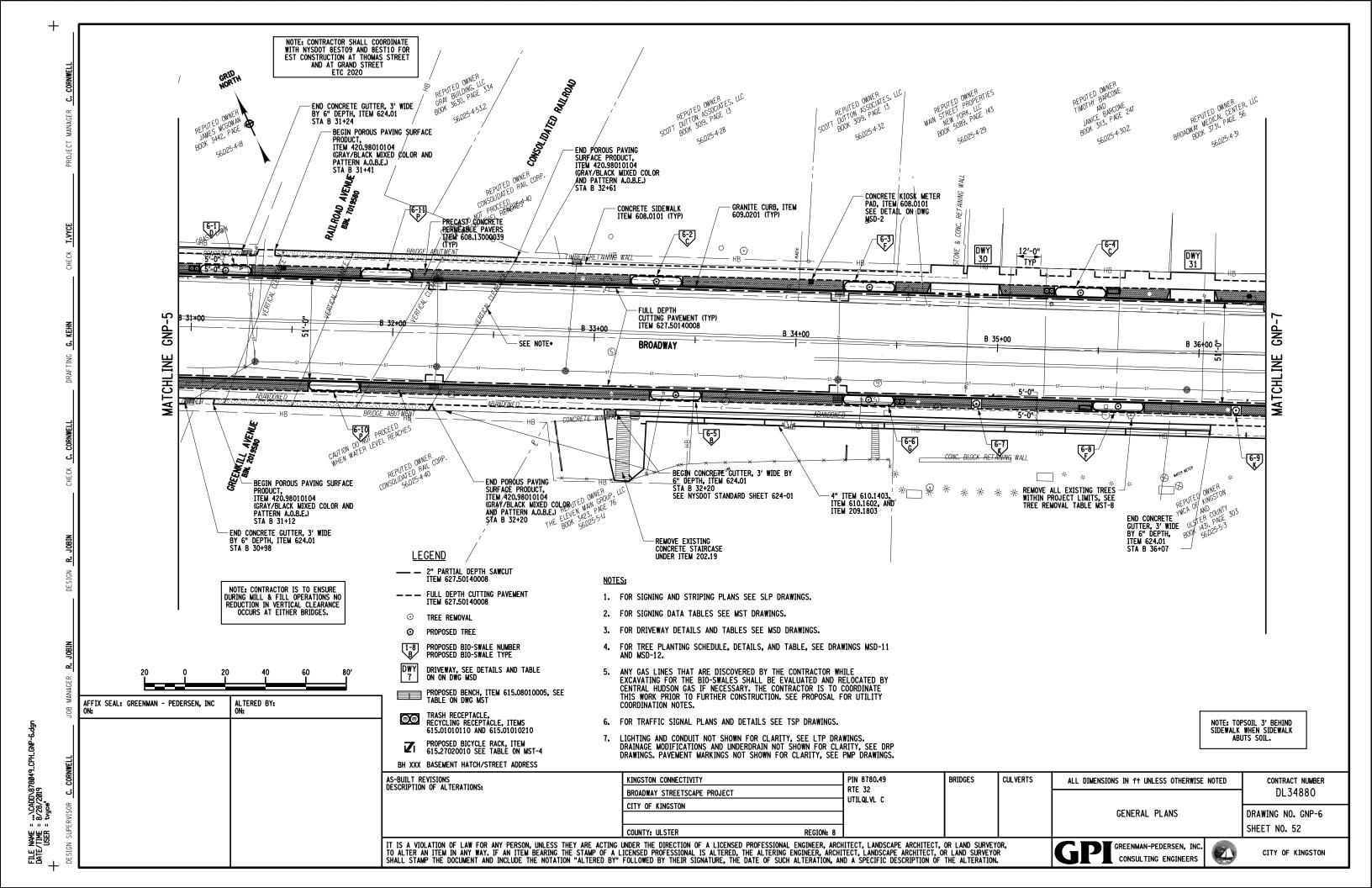


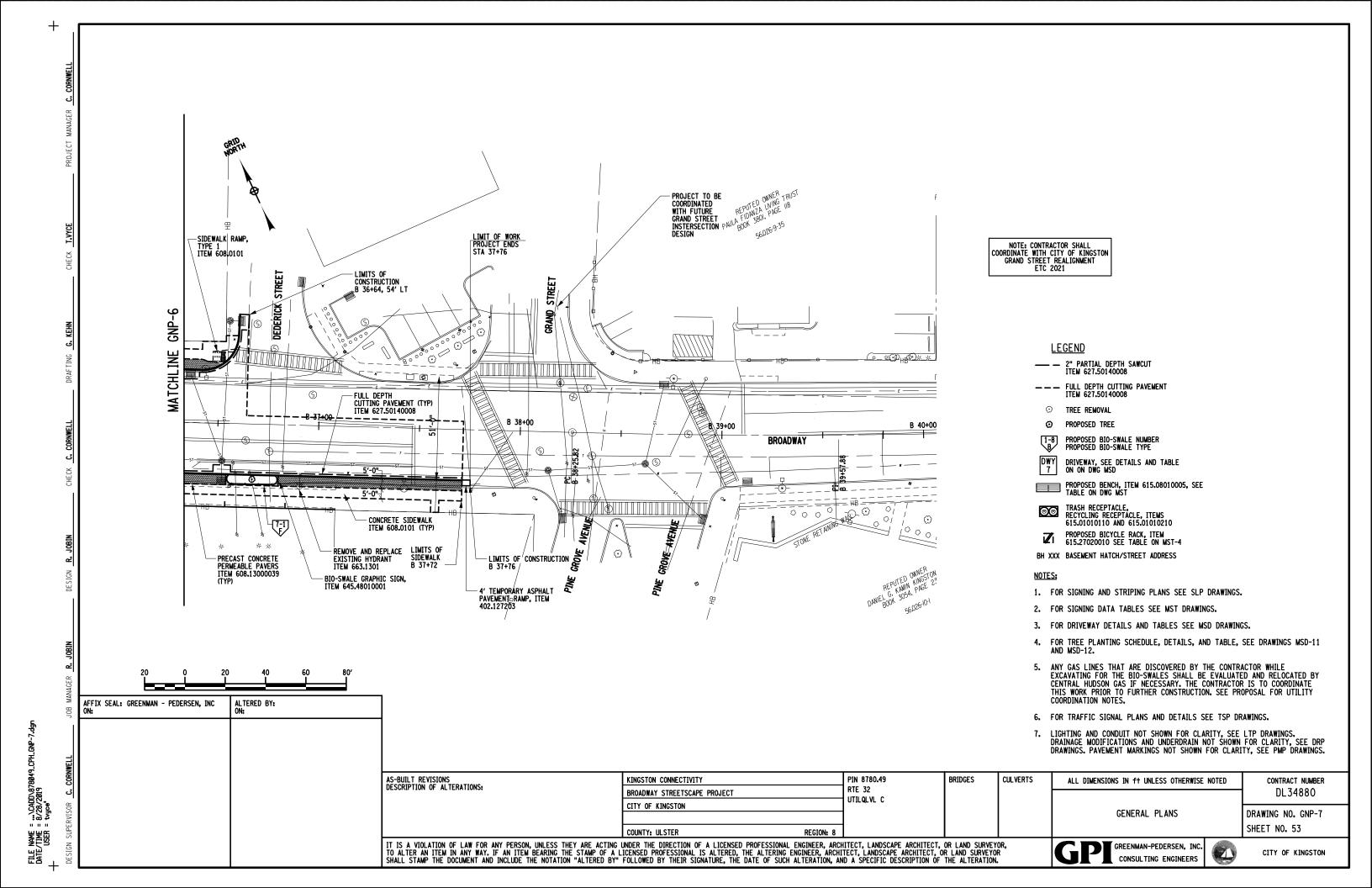


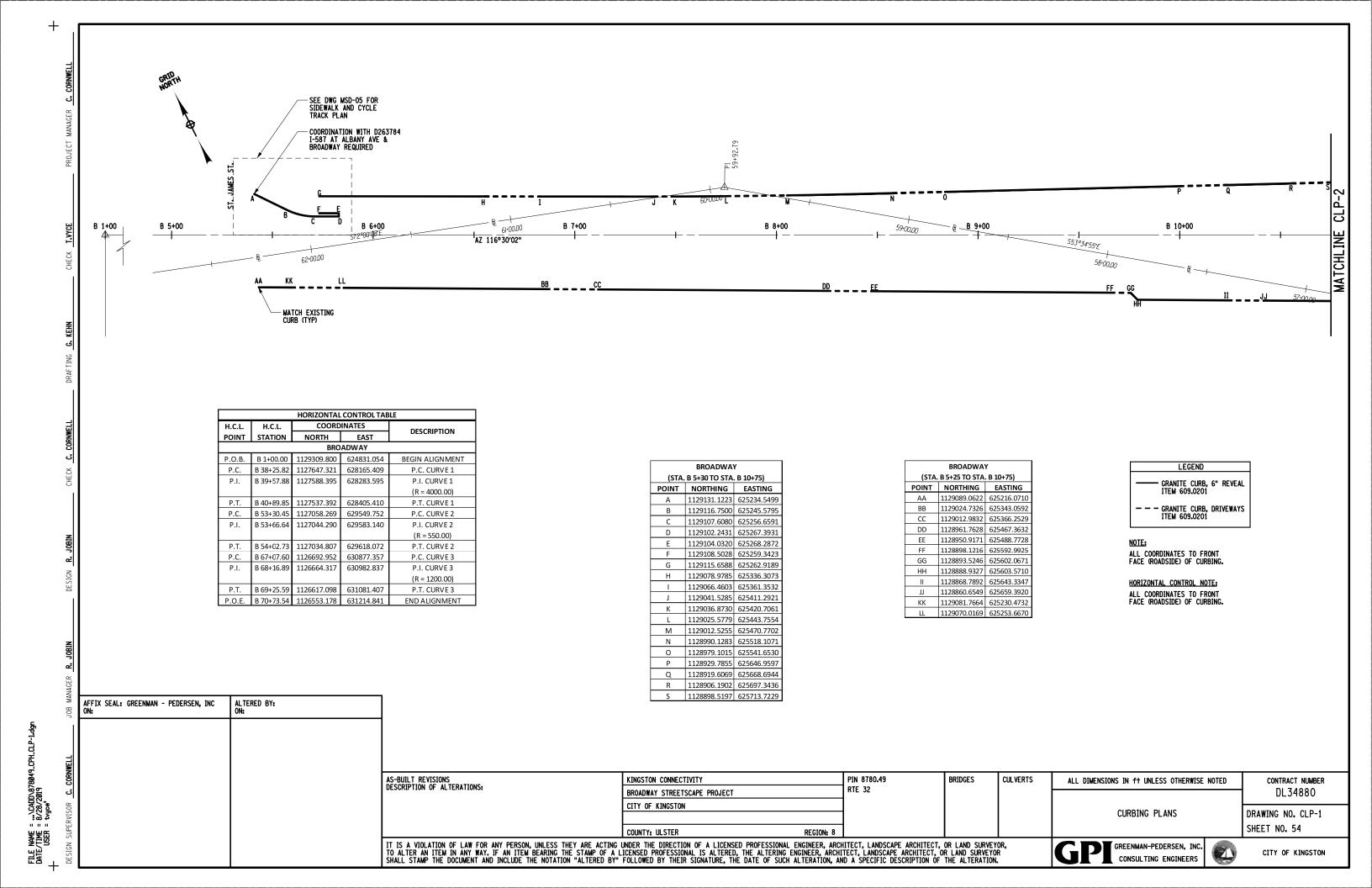


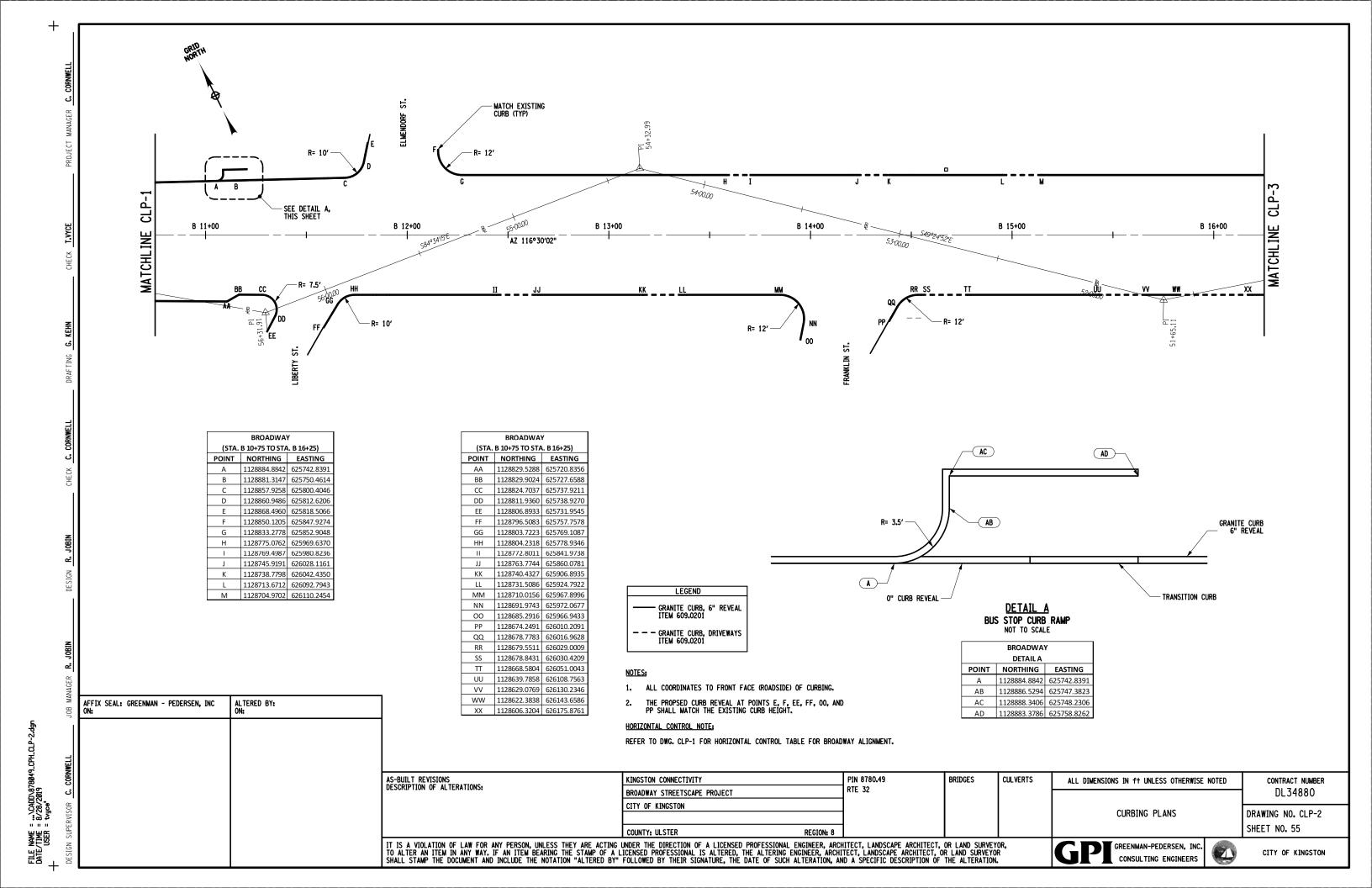


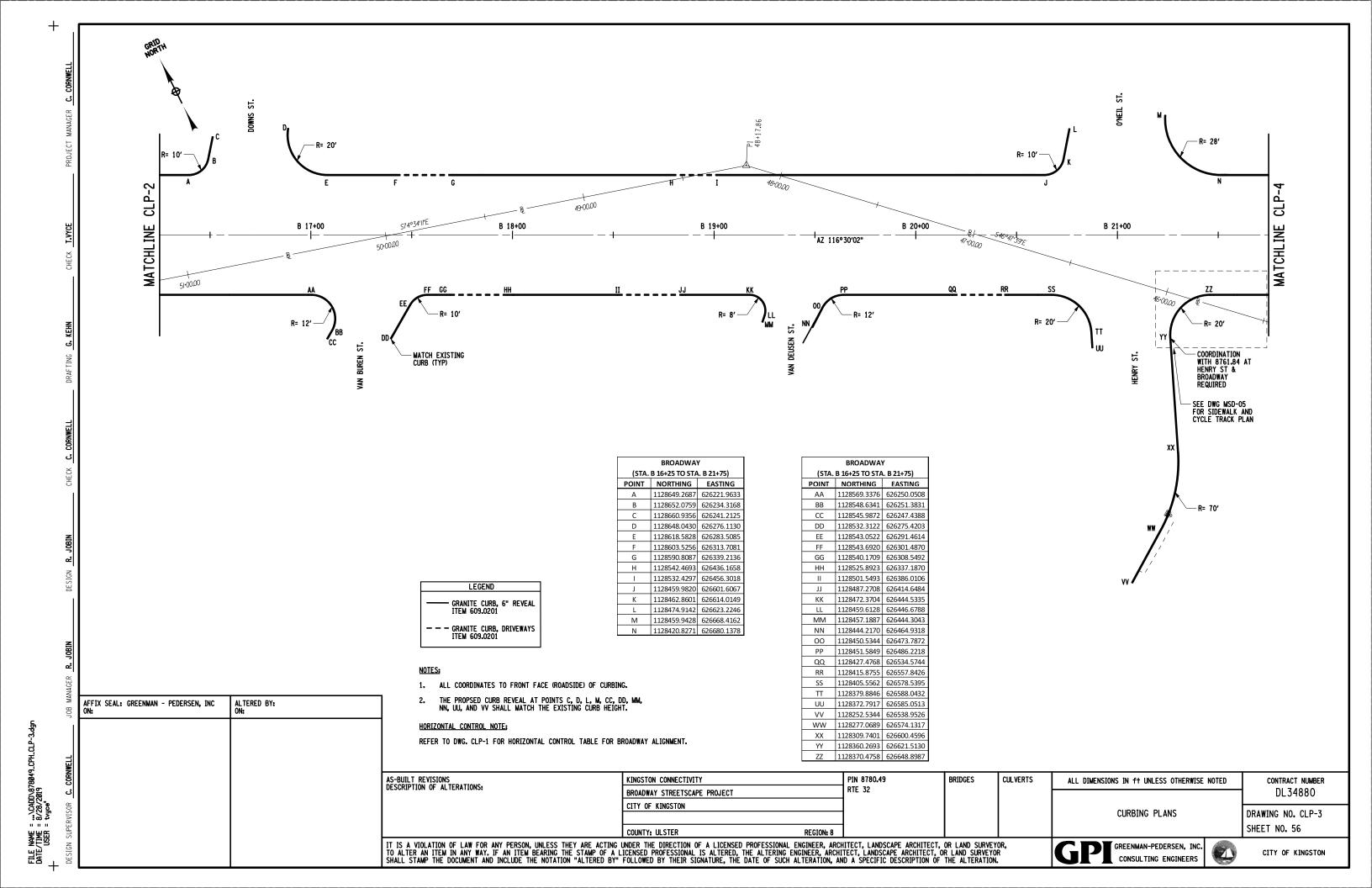


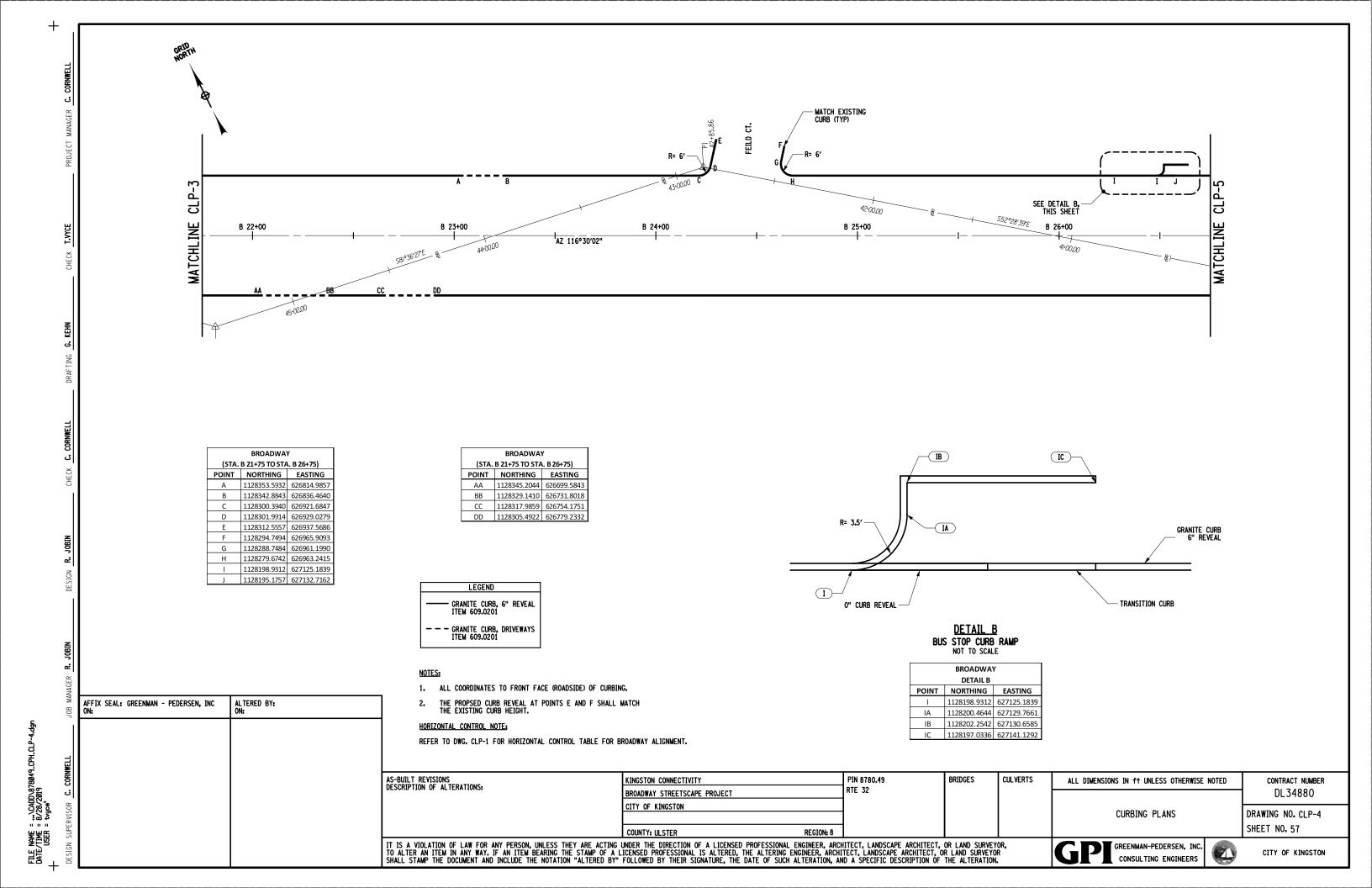


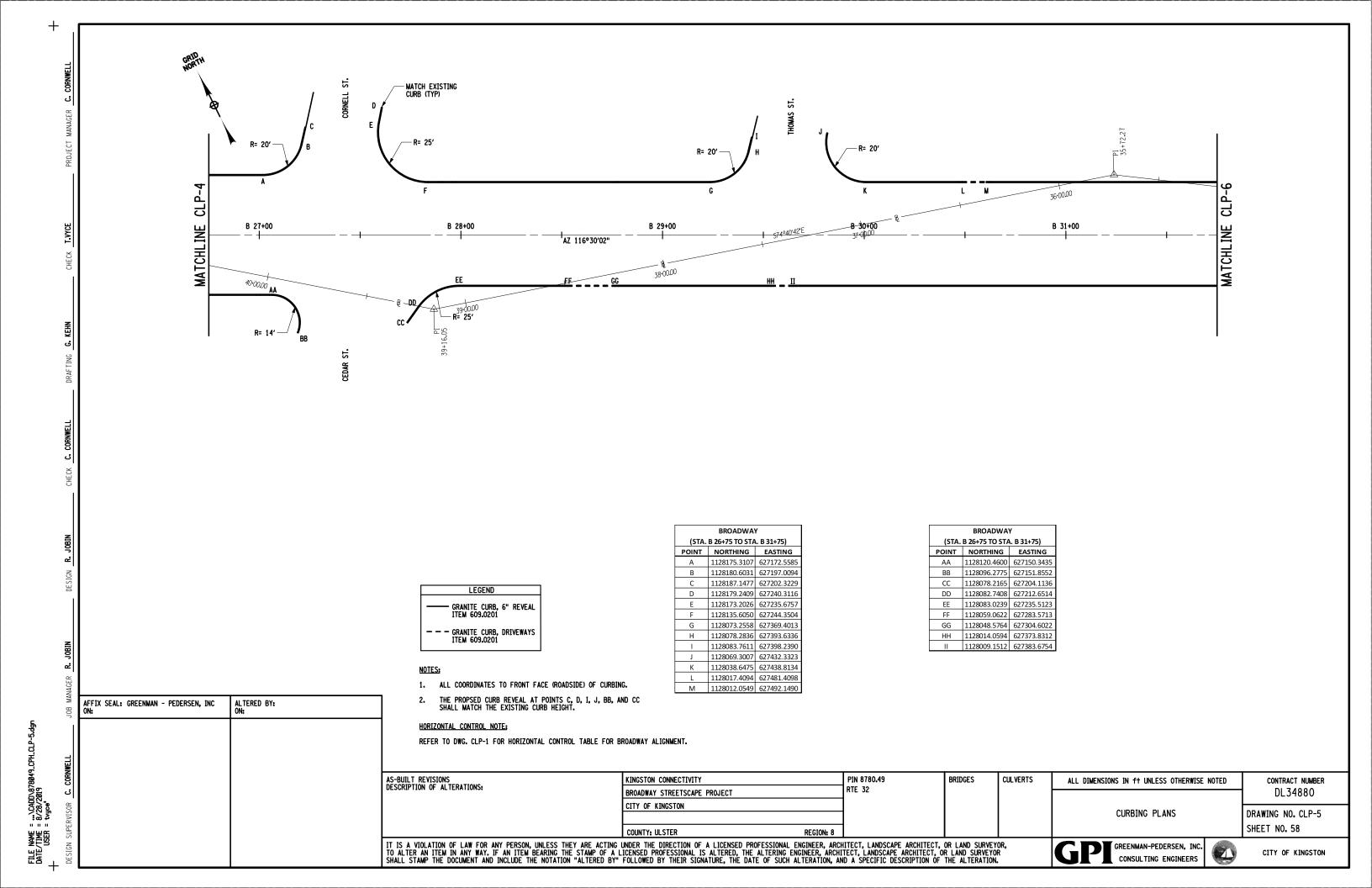


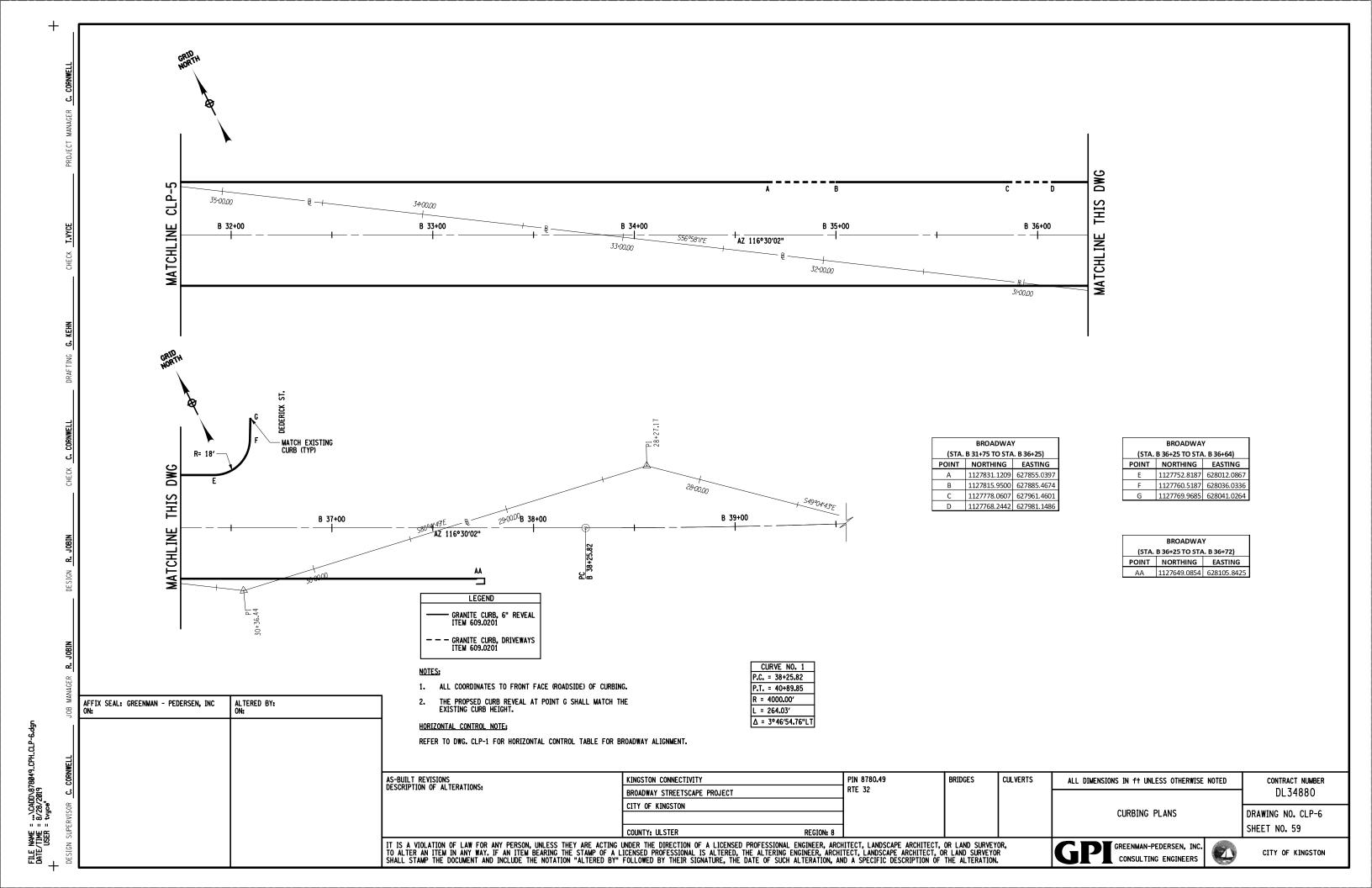












& COLOR & COLOR AREA TOTAL PAYMENT AREA TOTAL PAYMEN TOTAL PAYMENT & COLOR AREA SEE NOTE 2) SEE NOTE 2) SEE NOTE 2 AREA NO PARKING 24" x 24" 1-2, 1-4, 1-5, 2-4, 2-8, 3-2, 1.5 SF 5.0 SF NYP2-3R TURN R10-11 3-5, 3-6, 3-11, 3-15, 4-1, D9-11B 5-15, 5-16 645.5101 PAY AT 645.5101 2-19 645.5102 (MOD) 4-2, 4-7, 4-8, 5-4, 5-11, WHITE-BLACK/RED ON RED WHITE-GREEN STATION 4.0 SF 8.0 SF 5-12, 6-1, 6-6, 6-7, 6-8 1.5 SF 31.5 SF 5.0 SF 5.0 SF 1-9, 2-1, 2-10, 2-11, 2-12, 21" x 15" 0.7 SF 30" x 30" 6.3 SF 2-14, 2-15, 3-9, 3-10, R10-15 1-3, 1-6, 1-8, 2-2, 2-3, 2-6, **VEHICLES** M6-1I 645.5101 3-14, 3-17, 3-18, 4-5, 5-16 (MOD) 2-7, 3-1, 3-4, 3-7, 4-4, 5-7, 645.5102 BLUE-WHITE 4-10, 4-11, 5-2, 5-3, 5-6, WHITE-BLACK/RED 6-11, 6-14 🔽 то 🎺 NYP3-2 2.2 SF 88.2 SF 6-3, 6-4, 6-5, 6-10, 6-12, 9AM-6PM (MOD) 645.5101 6-13, 1-2, 1-4, 1-5, 2-4, 2-MON-SAT WHITE-GREEN 8, 3-2, 3-5, 3-6, 3-11, 3- 45 31.5 SF 6.3 SF 21" x 15' 2.2 SF 30" x 30" 15, 4-1, W11-2 M6-1R 2-20, 3-21, 3-22, 3-23, 4-4-2, 4-7, 4-8, 5-4, 5-11 5-15 645.5202 HI-VIZ 645.5202 **BLUE-WHITE** 12, 4-13 5-12, 6-1, 6-6, 6-7, 6-8 YELLOW-BLACK 2.2 SF 37.8 SF 6.3 SF **NO** PARKING 12" x 18" 2.0 SF R1-1 2-9, 3-12, 4-6, 7-1 645.5202 W16-9P NYP1-2D 1-10, 1-11, 3-20, 4-15, 2-20, 3-21, 3-22, 3-23, 4-ANY 645.5101 AHEAD 645.5202 25.2 SF WHITE-RED 5-8, 6-15, 6-16, 6-17 12, 4-13 YELLOW-BLACK 1.5 SF 12.0 SF 2.0 SF 12.0 SF 4.0 SF (I) PARKING R3-2 645.5202 5-9 NYP1-2I ANY 1-1, 1-12 645.5101 WHITE-RED 4.0 SF 4.0 SF TIME 1.5 SF 3.0 SF 3.0 SF MARKING 12" x 18 1.5 SF 645.5102 R6-1L 2-21 ONE WAY NYP1-2R ANY 2-16 645.5101 3.0 SF WHITE-RED TIME 1.5 SF 36" x 12" 3.0 SF NO PARKING ONE WAY R6-1R 2-21 645.5102 NYP1-9L HERE TO 7-2 645.5101 3.0 SF WHITE-RED CORNER 1.5 SF 1.5 SF RESERVE 1.5 SF PARKING (1) PARKING R7-8L WHITE-4-8, 4-10 645.5101 F NYP1-9R HERE TO GREEN/BLUE 645.5101 5-10 3.0 SF WHITE-RED CORNER 1.5 SF 1.5 SF 3.0 SF 1-9, 2-1, 2-10, 2-11, 2-12, R7-20 METER 1-7, 2-5, 2-13, 3-3, 3-8, 2-14, 2-15, 3-9, 3-10, 12" x 18 1.5 SF PARKING CUSTOM 3-13, 3-16, 4-3, 4-9, 5-1, 645.5102 NYP2-3L 3-14, 3-17, 3-18, 4-5, WHITE-GREEN 5-5, 6-2, 6-9 PAY AT 645.5101 (MOD) 39.0 SF 4-10, 4-11, 5-2, 5-3, 5-6, STATION 6-3, 6-4, 6-5, 6-10, 6-12, 1.5 SF 36.0 SF 6-13 1.5 SF R9-23al LEFT TURN 2-18, 4-14, 5-14 645.5101 WHITE-BLACK SIGN LOCATIONS AS SHOWN ON PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL INSTALL NEW SIGNS AND RELOCATED EXISTING SIGNS IN ACCORDANCE WITH THE MUTCD AND NYS SUPPLEMENT. MUST USE 1.5 SF 4.5 SF TURN BOX THE AREA AND PAYMENT AREA FOR SIGNS ARE FROM THE APPLICABLE STANDARD SHEETS OR SIGN FACE LAYOUTS. AFFIX SEAL: GREENMAN - PEDERSEN, INC ALTERED BY: 1.5 SF E = ...\CADD\878049_CPH_SDS-1.dgn E = 8/28/2019 R = tvyce' 3. ADDITIONAL SIGNS AT SIGNALS SHOWN ON TSP-DWGS. R9-23aR RIGHT TURI 2-17, 3-19, 5-13 645.5101 FOR SIGNS NYP2-3R (MOD) AND NYP2-3L (MOD), USE LAYOUT FOR SIGN NYP2-3. TEXT HEIGHT OF WHITE-BLACK MUST USE 4.5 SF TURN BOX AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 BRIDGES **CULVERTS** KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON SIGN DATA SHEET DRAWING NO. SDS-1 SHEET NO. 60 COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC CITY OF KINGSTON CONSULTING ENGINEERS

SIGN DATA TABLE

TEXT

ITEM

PAYMENT AREA

DESIGNATION

LOCATION

SIGN DATA TABLE

TEXT

ITEM

PAYMENT AREA

SIGN DATA TABLE

TEXT

DESIGNATION

LOCATION

PAYMENT AREA

DESIGNATION

LOCATION

FILE NAME = ...\CADD\878849.CPH_SDS-2.dgn DATE/TIME = 8/28/2019 H USER = tvyce

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:

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	4-11	B 20+84 ±
	4-12	B 20+60 ±
	4-13	B 20+52 ±
	5-1	B 26+54 ±
ıl	5-2	B 29+29 ±

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	4-3	B 24+29 ±	LT	STOP		1	
	4-4	B 24+66 ±	LT	FIRE HYDRANT	1		
	4.5	B 24+76±		YIELD TO PEDESTRIAN IN		1	
	4-5		LT	CROSSWALK		1	
	4-6	B 25+06 ±	LT	JCT RT 32 + JCT RT 213	2		
	4-7 B 23+94 ±	D 22 - 04 -	DT	YIELD TO PEDESTRIAN IN			
		RT	CROSSWALK		1		
				HERITAGE AREA + KINGSTON			

VISITOR CENTER + STRAIGHT

ONE WAY (x2) + STOP RT 32 + LEFT + SENATE HOUSE + LEFT + WATERFRONT + BIKE 32 +

STRAIGHT

FIRE HYDRANT

BROADWAY + HENRY ST + KINGSTON

1777 TRAIL + BUS STOP

RT 32 + RIGHT + RT213 + RIGHT LT BUS STOP + NO PARKING ANY TIME

KINGSTON 1777 TRAIL

NO LEFT TURN

BROADWAY + THOMAS ST

NO PARKING BETWEEN SIGNS NO PARKING BETWEEN SIGNS

NO LEFT TURN

BUS ROUTE

BRIDGE AHEAD CLEARANCE 12'-10" +

NO PARKING BETWEEN SIGNS

ALTERED BY: ON:

1

2

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

- 1		5-3	B 19+10 ±	LI	RESERVED PARKING			1	
		3-4	B 19+31 ±	LT	RESERVED PARKING			1	
		3-5	B 19+52 ±	RT	STOP + DO NOT ENTER	1		1	
푎		DO NOT ENTER + ONE WAY (x2		DO NOT ENTER + ONE WAY (x2) +	2				
	3-6 B 19+18 ±		RT	BROADAY + VAN DEUSEN ST	2				
ات		2.7	D 17. FF 1	RT	SNOW EMERGENCY ROUTE + ONE	1			
TING	3-7 B 17+55 ±		KI	WAY + BROADWAY + VAN BUREN ST	2				
√F T		3-8	B 17+11 ±	RT	ONE WAY	1			
DRAF.		4-1	B 21+54 ±	LT	BROADWAY + NEIL ST	1			
- 1		4-2	R 21+92+	ıт	SOUTH RT 32 LEFT + NORTH RT 32	2			

SIGN REMOVAL/RELOCATE TABLE										
CATION STATION SIDE		SIDE	DESCRIPTION	ITEM 647.31	ITEM 647.41	ITEM 647.51	ITEM 647.61	ITEM 645.81		
1-1	B 5+88 ±	LT	NO PARKING ANY TIME			1				
1-2	B 6+52 ±	LT	20 MIN PARKING			1				
1-3	B 7+34 ±	LT	RT 32 + SLIGHT RIGHT + NO PARKING IN DRIVEWAY	1		1		1		
1-4	B 7+92 ±	RT	HOSPITAL + *WEEKEND SHUTTLE PICK UP HERE	1	1			1		
2-1	B 10+28 ±	LT	JCT I-587 + JCT RT 28	1				1		
2-2	B 10+78 ±	LT	KINGSTON 1777 TRAIL		1					
2-3	B 11+66 ±	LT	FIRE HYDRANT	1				1		
2-4	B 11+72 ±	LT	ELMENDORF ST + BROADWAY	1				2		
2-5	B 12+89 ±	LT	NO PARKING HERE TO CORNER			1				

2

4-13

7-1

TOTAL:

20 EA

*THE KINGSTON LIBRARY +

EMERGENCY SNOW ROUTE

STOP

KINGSTON 1777 TRAIL BROADWAY + LIBERTY ST + SNOW

EMERGENCY ROUTE + *BROADWAY THEATER PARKING

BROADWAY + DOWNS ST

BUS STOP

DECEDVED DADVING

B 14+78 ±

B 14+39 ±

B 13+82 ±

B 11+81 ±

B 16+40 ±

B 17+14 ±

D 10 10 ±

B 21+92 ±

B 21+88 ±

B 21+33 ±

B 29+90 ±

B 29+97 ±

B 30+38 ±

B 29+80 ±

B 29+77 ±

B 29+10 ±

B 28+02 ±

RT

RT

RT

LT

RT

RT

RT

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LT

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RT

RT

2-7

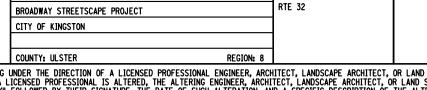
2-8

2-9

3-2

	COUNTY: ULSTER	REGION: 8			
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UT OF ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	ICENSED PROFESSIONAL IS ALTERED,	THE ALTERING ENGINEER, ARCHI	TECT, LANDSCAPE ARCHITECT, O	R LAND SURVEYOR	

KINGSTON CONNECTIVITY



PIN 8780.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

SIGN DATA SHEET

GREENMAN-PEDERSEN, INC.

CONSULTING ENGINEERS

CONTRACT NUMBER

DL34880

CITY OF KINGSTON

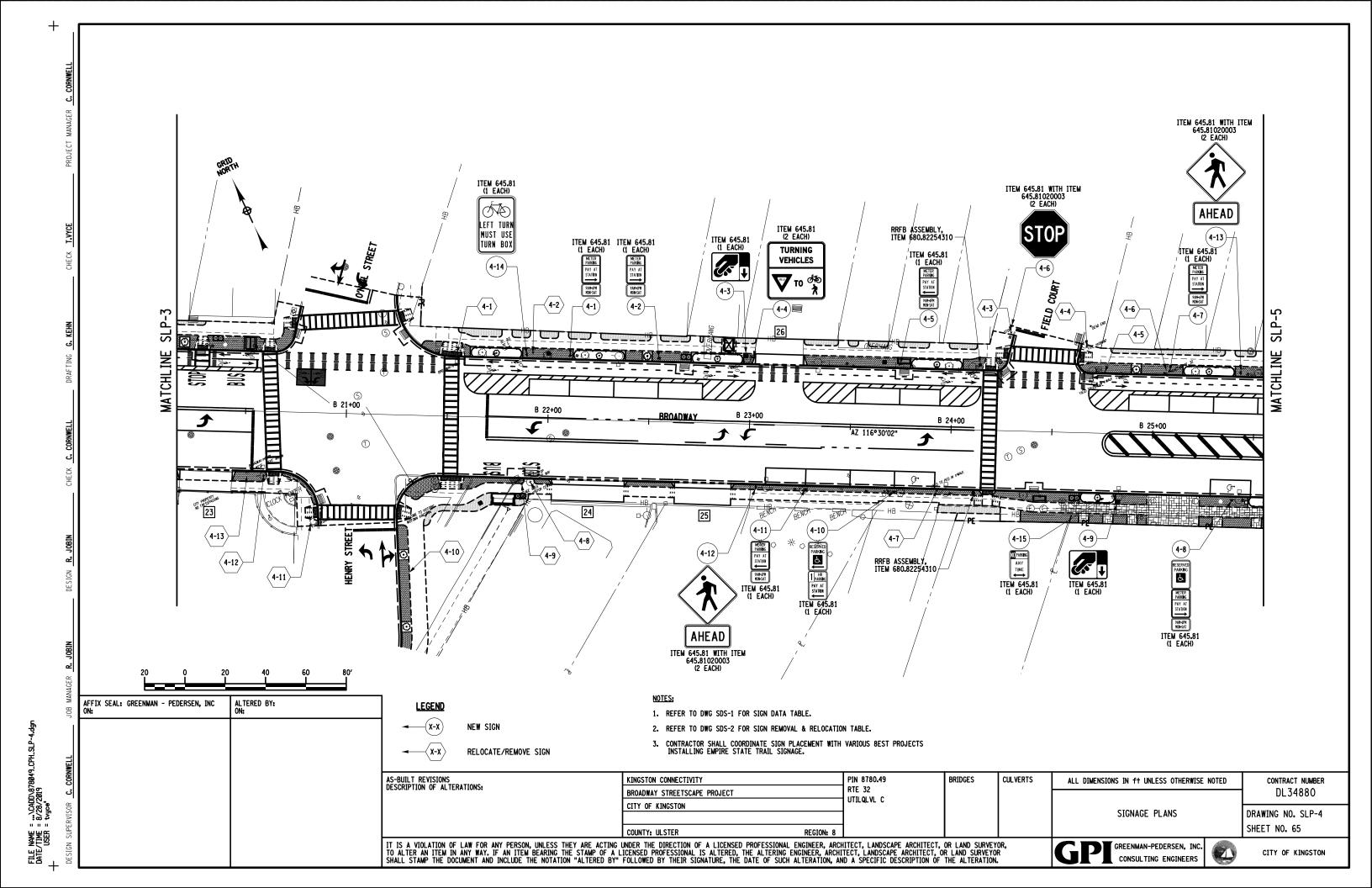
DRAWING NO. SDS-2 SHEET NO. 61

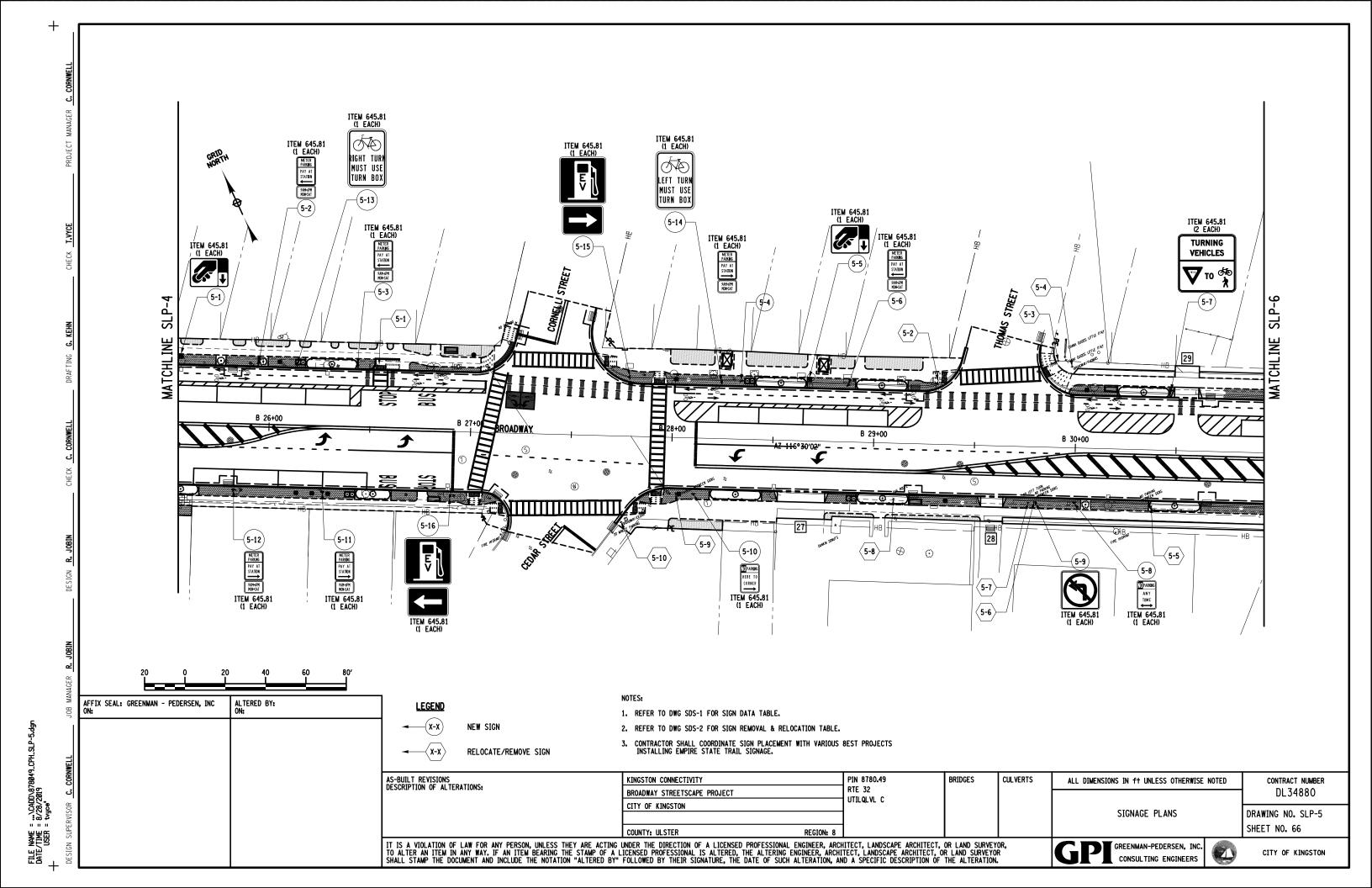
			PARKIN	IG				
7-2 B 36+46	± LT	BRIDGE AHEA	AD CLEA	RANCE 12'-10"	1			
7-3 B 36+57	± LT		STOP				1	
				TOTALS	31	18	25	
				-				
REFLECTIVE SIG	ON POST STR	IPS		SIG	NING SUN	/IMARY TA	BLE	
IGN STRUCTURE	ITEM 645.8	1020003 (EA)		ITEM NUM	1BER	QUANTITIY		
2-9		2		645.510	1		141 SF	
2-20		2		645.510)2		138 SF	
3-12		2		645.520)2		81 SF	
3-21		2		645.81			161 EA	
3-22		2		645.81020	0003		20 EA	
3-23		2		647.31			31 EA	
4-6		2		647.41			18 EA	
4-12		2		647.51			25 EA	

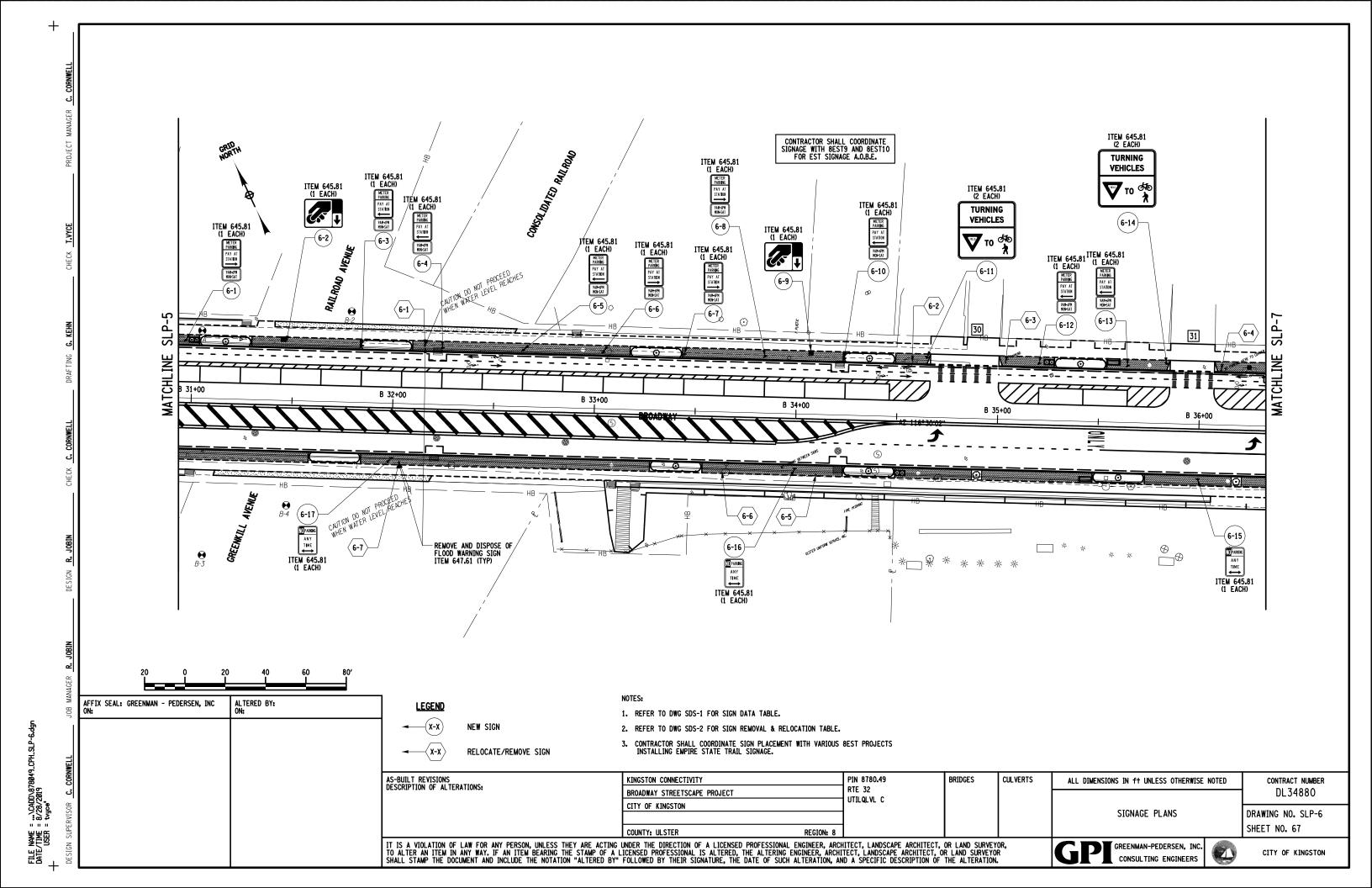
647.61

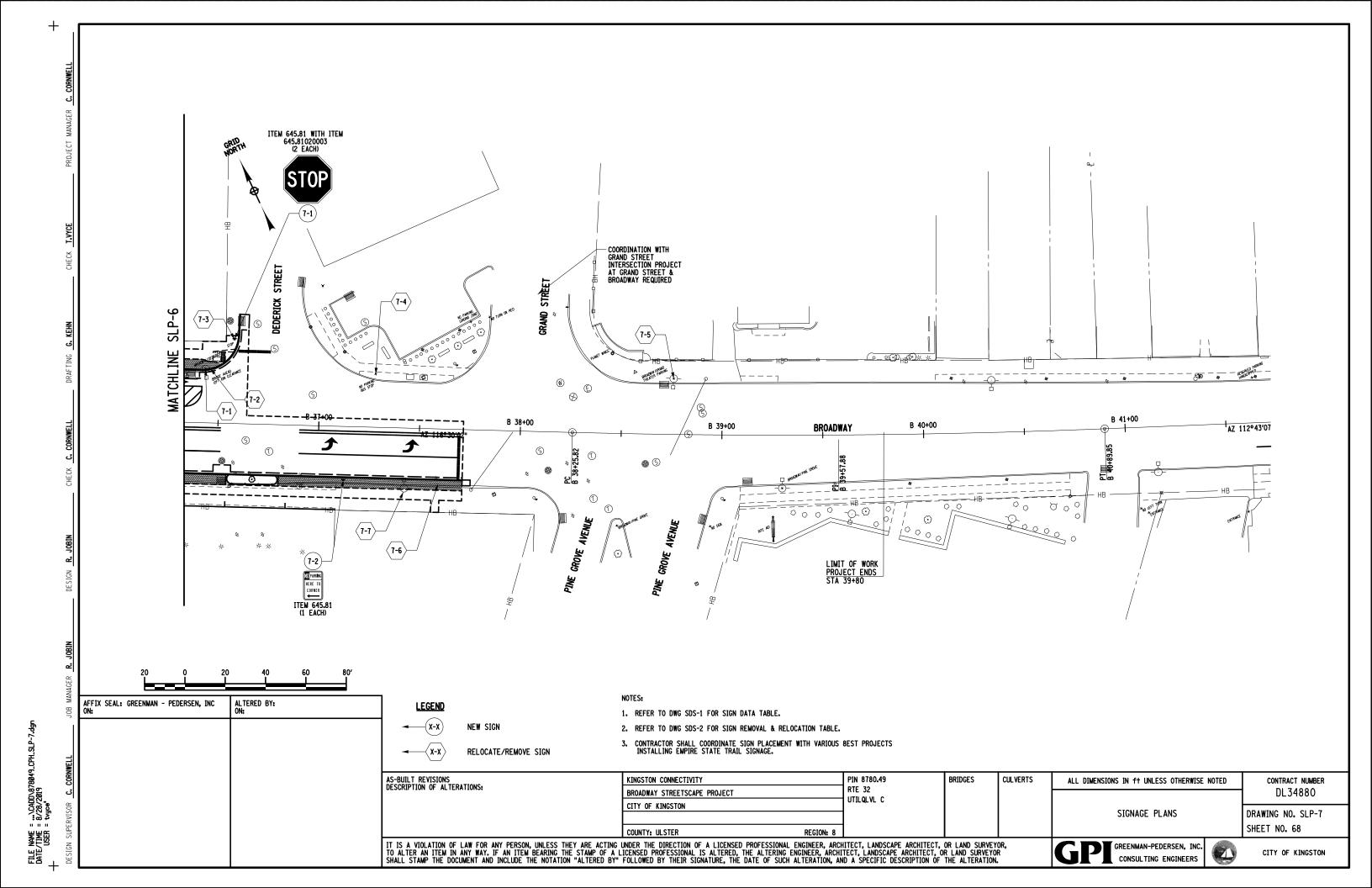
2 EA

			SIGN REMOVAL/RELOCATE TABLE	(CONT.)				
LOCATION NUMBER	STATION SIDE		DESCRIPTION	ITEM 647.31	*ITEM 647.41	ITEM 647.51	ITEM 647.61	ITEM 645.81
5-10	B 27+76 ±	RT	BROADWAY + CEDAR ST	1				
6-1	B 32+15 ±	LT	CAUTION DO NOT PROCEED WHEN WATER LEVEL REACHES				1	
6-2	B 34+55 ±	LT	NO PARKING BETWEEN SIGNS			1		
6-3	B 35+03 ±	LT	NO PARKING BETWEEN SIGNS			1		
6-4	B 36+10 ±	LT	NO PARKING FROM HERE TO CORNER			1		
6-5	B 34+11 ±	RT	NO PARKING BETWEEN SIGNS			1		
6-6	B 33+66 ±	RT	SNOW EMERGENCY ROUTE + KINGSTON 1777 TRAIL	1	1			1
6-7	B 32+04 ±	RT	CAUTION DO NOT PROCEED WHEN WATER LEVEL REACHES				1	
7-1	B 36+43 ±	LT	BROADWAY + DEDRICK ST + KINGSTON 1777 TRAIL + NO PARKING	1	1	1		1
7-2	B 36+46 ±	LT	BRIDGE AHEAD CLEARANCE 12'-10"	1				1
7-3	B 36+57 ±	LT	STOP			1		
			TOTALS	31	18	25	2	25









- UNLESS OTHERWISE NOTED, THE CITY OF KINGSTON SHALL HAVE MAINTENANCE JURISDICTION OVER ALL SIGNALS IN THIS CONTRACT UPON COMPLETION OF THEIR INSTALLATION AND OFFICIAL ACCEPTANCE.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF KINGSTON ENGINEERING DEPARTMENT (845-334-3967) 48 HOURS IN ADVANCE OF ANY SCHEDULED
- UNDER NO CONDITION SHALL THE CONTRACTOR MAKE THEIR OWN SERVICE CONNECTIONS. ALL SERVICE CONNECTIONS SHALL BE MADE BY CENTRAL HUDSON GAS AND ELECTRIC WEST, (845) 334-3575.
- IF, AT ANY TIME DURING THE INSTALLATION OF THE TRAFFIC SIGNAL POLES, OR SIGNAL SPAN WIRES AND MESSENGER CABLES, IT IS DISCOVERED THAT THE MINIMUM CLEARANCES FROM PRIMARY/SECONDARY POWER CONDUCTORS AS REQUIRED BY SECTION 23 OF THE OF THE NATIONAL ELECTRICAL SAFETY CODE (ANSI STANDARD C2) AND LOCAL UTILITY CODES CAN NOT BE ACHIEVED, THE UTILITY COMPANY OWNING SUCH CONDUCTORS SHALL BE NOTIFIED IN WRITING OF THE PROBLEM. NO FURTHER WORK SHALL BE DONE UNTIL SAID POWER LINES HAVE BEEN RELOCATED TO PROVIDE THE PROPER CLEARANCES.
- THE LOCATIONS OF ALL UNDERGROUND OBJECTS HAVE NOT BEEN SHOWN ON THE PLANS. THE CONTRACTOR SHALL SATISFY THEMSELVES OF EXISTING CONDITIONS, AND SUPPORT AND PROTECT ALL FACILITIES ENCOUNTERED IN THE TRENCHING AND EXCAVATING OPERATIONS.
- WHERE UNFORESEEN UNDERGROUND UTILITIES MAKE IT NECESSARY TO RELOCATE SIGNAL POLES MORE THAN 5 FEET FROM THEIR ORIGINAL LOCATION AS SHOWN ON THE PLANS, THE CITY OF KINGSTON SHALL BE MEDIATELY NOTIFIED AND ALTERNATE LOCATIONS WILL BE SUPPLIED BY
- A CARD SHALL BE PLACED ON THE INSIDE DOOR OF EACH CONTROLLER CABINET LISTING ALL THE COMPONENT PARTS BY SERIAL NUMBER, THE DATE OF INSTALLATION AND DATE OF OFFICIAL OPERATION.
- TERMINAL BLOCKS PROVIDED IN THE CONTROLLER CABINETS SHALL BE OF THE BINDER HEAD SCREW TYPE AND SHALL BE MOUNTED WITH THE SCREW HEADS ON THE FRONT FACE OF THE BLOCKS FACING THE OPPOSITE WALL OF
- WHERE MICROCOMPUTER EQUIPMENT IS TO BE INSTALLED, THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING THAT EQUIPMENT IN THE NEW SYSTEM UPON THE COMPLETION OF THE TESTS SET FORTH IN SECTION 680-3.32 OF THE STANDARD SPECIFICATIONS. IF DURING THE PERIOD BETWEEN THE START OF THE MAINTENANCE AND THE COMPLETION OF THE CONTRACT, A MATERIAL OR WORKMANSHIP DEFECT IS FOUND OUTSIDE THE MICROCOMPUTER EQUIPMENT SUPPLIED, THE CONTRACTOR WILL BE DESPONSIBLE FOR CORPECTING SUCH DEFECT AT THEIR OWN EMPENSE RESPONSIBLE FOR CORRECTING SUCH DEFECT AT THEIR OWN EXPENSE
- BALANCE ADJUSTERS AND SWIVEL BALANCERS WILL BE INSTALLED AT EACH SIGNAL HEAD ASSEMBLY. ASSOCIATED PIPE EXTENSIONS SHALL NOT EXCEED 30", UNLESS ORDERED BY THE CITY.
- THE COST OF THE CONDUIT WITHIN THE POLE FOUNDATION SHALL BE INCLUDED IN THE PRICE BID FOR THE FOUNDATION. THE COST OF THE CONDUITS WITHIN A GROUND-MOUNTED CONTROLLER BASE SHALL BE INCLUDED IN THE PRICE BID FOR THE CONTROLLER BASE. ALL POLE BASES SHALL HAVE ONE UNUSED CONDUIT IN THE BASE CAPPED FOR FUTURE USE. THIS SHALL BE IN ADDITION TO THE CONDUITS NECESSARY FOR THE OPERATION OF THE WORKING SIGNAL SYSTEM.
- NO SPLICES SHALL BE ALLOWED IN THE SIGNAL CONTROL CABLE WITH THE EXCEPTION OF THE LEAD-IN CABLE TO THE INDUCTANCE LOOP CABLE.
- UPON COMPLETION OF THE WORK AT THE INTERSECTION, THE CITY OF KINGSTON MAINTENANCE PERSONNEL WILL PERFORM AN INSPECTION OF THE SIGNAL SYSTEM. AT THE TIME OF THE INSPECTION, CONTRACTOR SHALL PROVIDE ACCESS TO ALL PULLBOXES FOR INSPECTION PURPOSES. THE CONTRACTOR WILL BE NOTIFIED OF ANY DEFECTS FOUND DURING THE INSPECTION AND WILL MAKE NECESSARY CORRECTIONS BEFORE THE INTERSECTION WILL BE ACCEPTED.
- POWER SHALL BE SUPPLIED TO THE SIGNALS VIA 1 NPS METAL STEEL CONDUIT PAID FOR UNDER ITEM 680.520103 CONDUIT, METAL STEEL ZINC COATED, 1 NPS. THE PRICE BID UNDER THIS ITEM SHALL INCLUDE THE COST OF INSTALLING THE CONDUIT AS SHOWN IN THE PLANS. 15.

JOB MAI	AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:	ALTERED BY: ON:
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C. CORNWELL		
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- 16. VEHICLE SENSORS SHALL BE CENTERED IN THEIR RESPECTIVE LANES AS DETAILED ON DWG. TSD-1. THE CONTRACTOR SHALL COORDINATE WITH CITY OF KINGSTON ENGINEERING DEPARTMENT PERSONNEL FOR LAYOUT APPROVAL
- TEN *2 AND TEN POLICE DOOR KEYS SHALL BE SUPPLIED WITH THE CONTROLLER CABINET.
- 18. FOR NEW ANCHOR BASE POLE INSTALLATIONS:
- THE CONCRETE POLE FOUNDATION SHALL CURE FOR A MINIMUM OF 14 DAYS BEFORE THE SIGNAL POLE IS ERECTED, AND SHALL CURE FOR A MINIMUM OF 28 DAYS BEFORE THE SPAN WIRE AND TRAFFIC SIGNAL HEADS ARE
- THE COST OF THE ANCHOR BOLTS AND THE STEEL REINFORCING SHALL BE INCLUDED IN THE PRICE BID UNDER ITEM 680.5001-POLE EXCAVATION AND
- 19. CONSTRUCTION SIGNS SHALL BE INSTALLED ON ALL LEGS OF AN INTERSECTION WHILE THE CONTRACTOR IS WORKING AT THAT INTERSECTION. ADDITIONAL CONSTRUCTION SIGNS SHALL BE INSTALLED AS REQUIRED BY THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, COST SHALL BE INCLUDED WITH THE PROPERTY OF THE IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
- 20. UNLESS OTHERWISE NOTED, ITEM 680.510501-PULLBOXES, RECTANGULAR, 18"x26", REINFORCED CONCRETE, SHALL BE USED IN ALL SIGNAL
- 21. ALL ANCHOR BASE TRAFFIC SIGNAL POLES AND PEDESTRIAN SIGNAL POLES SHALL BE EQUIPPED WITH ANCHOR BOLT COVERS.
- ALL HOLES FOR LB CONDUITS, CONDUITS, EYEBOLTS, AND POLE MOUNTED SIGNAL AND/OR PEDESTRIAN HEADS SHALL BE DRILLED. THE USE OF CUTTING TORCHES SHALL NOT BE PERMITTED UNLESS WRITTEN PERMISSION IS GRANTED IN ADVANCE BY THE CITY OF KINGSTON.
- 23. IN AREAS WHERE SIGNAL POLES ARE INSTALLED OR WHERE SIGNAL EQUIPMENT IS REMOVED, ANY DISTURBED GROUND SHALL BE RESTORED TO A CONDITION MATCHING ADJACENT AREA. PAYMENT FOR WORK AND MATERIALS IS TO BE INCLUDED IN THE BID PRICE AS APPLICABLE.
- 24. IN THE EVENT THE UNSUITABLE MATERIALS SUCH AS SILTS, SOFT CLAY OR ORGANIC SOILS ARE ENCOUNTERED IN THE EXCAVATION FOR A POLE FOUNDATION, THE CITY OF KINGSTON ENGINEERING DEPARTMENT SHALL BE
- WHERE SOUND BEDROCK IS ENCOUNTERED DURING POLE EXCAVATION OPERATIONS AND THE DEPTH OF THE SOIL OVER THE ROCK IS ONE-THIRD OR LESS THAN THE "MINIMUM EMBEDMENT" LENGTH GIVEN IN THE TABLE ON STANDARD SHEET 680-01, THEN THE FOOTING LENGTH MAY BE DECREASED SO AS TO PROVIDE AN EMBEDMENT INTO THE ROCK EQUAL TO THE FOOTING DIAMETER PLUS 2 FT. FOR DEEPER OVERBURDENS, THE FOOTING LENGTH MAY BE DECREASED TO PROVIDE EMBEDMENT INTO ROCK EQUAL TO THE FOOTING DIAMETER. IF THE RESULTING DEPTH IS LESS THAN THE LENGTH OF THE ANCHOR BOLTS, THE CONTRACTOR SHALL EXCAVATE TO THE LENGTH THE ANCHOR BOLTS PLUS 6 INCHES.
- 26. PRIOR TO INSTALLATION THE CONTRACTOR SHALL VERIFY THE SUITABILITY OF EACH LOCATION WHERE EACH TRAFFIC SIGNAL POLE AND SPAN WIRE ASSEMBLY IS TO BE LOCATED AT THE INTERSECTION.
- 27. ALL THIMBLE BOLTS USED FOR TRAFFIC SIGNAL SPAN WIRE AND/ OR ALL HIMBLE BOL'S USED FOR TRAFFIL SIGNAL SPAN WIRE AND/ OR MESSENGER CABLE ATTACHMENTS, AS SHOWN ON STANDARD SHEETS 680-06, 680-07, AND 680-16 SHALL BE 3/4" AND SHALL HAVE A BREAKING STRENGTH EQUAL TO THAT OF ATTACHED SPAN WIRE AS SHOWN IN NOTE 6 OF STANDARD SHEET 680-680-07. THIMBLE BOLTS ARE TO BE USED IN PLACE OF THE EYEBOLTS SPECIFIED ON THE STANDARD SHEET.
- 28. WHERE SIGNAL CABLE IS INSTALLED ON SPAN WIRE OR MESSENGER CABLE, IT SHALL BE SUPPORTED AT INTERVALS OF NOT GREATER THAN 15" BY STAINLESS STEEL CABLE RINGS.
- 29. ON ALL SIGNAL HEAD ASSEMBLIES ALL FEMALE THREADED CONNECTING HARDWARE SHALL BE MANUFACTURED WITH TWO SET SCREWS AND ALL THREADED PIPE NIPPLES SHALL HAVE TAPERED THREADS.
- THE BOTTOM OF ALL TRAFFIC SIGNAL HEADS SHALL BE LOCATED A MINIMUM OF 16'-6" ABOVE THE PAVEMENT SURFACE.
- ONCE TRAFFIC SIGNAL HEAD ASSEMBLIES HAVE BEEN INSTALLED AND ADJUSTED IN THE FIELD TO THE SATISFACTION OF CITY MAINTENANCE PERSONNEL, THE CONTRACTOR SHALL APPLY A BEAD OF SILICONE SEALANT AROUND THE SERRATED LOCK RING AT ALL LOCATIONS WHERE VERTICAL PIPE NIPPLES ARE ATTACHED TO THE TOP SURFACE OF BOTH TRAFFIC SIGNAL SECTIONS AND PIPE CROSS BODIES, TO PREVENT WATER INFILTRATION INTO
- ALL POLES SHALL BE EQUIPPED WITH A GROUNDING TERMINAL ACCESSIBLE THROUGH THE HAND HOLE IN THE POLE PER SECTION 724-03 OF THE STANDARD SPECIFICATIONS, THIS TERMINAL SHALL BE LOCATED ON THE OPPOSITE SIDE OF THE POLE FROM THE HAND HOLE.

- 33. ALL TRAFFIC SIGNAL POLES SHALL BE INSTALLED SO THAT THE POLES HAVE A MINIMUM RAKE OF ONE QUARTER THE DIAMETER OF THE SIGNAL POLE MEASURED AT ITS BASE AFTER LOADING.
- 34. THE CONTRACTOR SHALL SALVAGE, AND DELIVER TO THE CITY, THE FOLLOWING EQUIPMENT REMOVED UNDER ITEM 680.79XX0008. CONTRACTOR SHALL COORDINATE DELIVERY LOCATION WITH THE CITY
 - A) TRAFFIC SIGNAL POLES, CONTROLLER CABINETS, TRAFFIC SIGNAL CONTROLLERS AND ALL ASSOCIATED EQUIPMENT, TRAFFIC SIGNAL HEADS, AND PULLBOX FRAMES AND COVERS.
- ALL SIGNAL POLES TO BE REMOVED UNDER ITEM 680.79XX0008 SHALL BE REMOVED AS DETAILED BELOW UNLESS OTHERWISE ORDERED BY THE
- EMBEDDED POLES SHALL BE CUT OFF TO A MINIMUM OF 6 INCHES
- ANCHOR BASE POLES SHALL BE REMOVED FROM THEIR BASE AND ANCHOR BOLTS SHALL BE CUT OFF TO A MINIMUM DEPTH OF 6 INCHES
- FOR BOTH TYPES OF POLES, THE FOUNDATION SHALL BE CHIPPED DOWN TO A MINIMUM DEPTH OF 6 INCHES BELOW GRADE.
- ALL HOLES IN THE BASE SHALL BE FILLED WITH CONCRETE AND THE AREA SHALL BE RESTORED TO A CONDITION MATCHING ADJACENT AREAS. PAYMENT FOR WORK AND MATERIALS IS TO BE INCLUDED IN THE PRICE
- ALL PULLBOXES TO BE REMOVED UNDER ITEM 680.79XX0008 SHALL BE REMOVED AS FOLLOWS: PULLBOX COVER AND FRAME SHALL BE REMOVED AND RETURNED TO THE CITY OF KINGSTON. THE PULLBOX SHALL BE FILLED WITH SUITABLE FILL MATERIAL MEETING THE REQUIREMENTS OF SECTION 733-08; EMBANKMENT IN PLACE OF THE NYSDOT STANDARD SPECIFICATIONS. THE AREA SHALL BE RESTORED TO A CONDITION
- 37. THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE INSTALLATION OF ALL PEDESTRIAN CROSSING CONTROLS SHALL BE IN COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT AS OUTLINED COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT AS OUTLIN IN THE PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY, JULY 2011 (PROWAG). PEDESTRIAN PEDESTALS SHALL BE LOCATED ADJACENT TO THE SIDEWALK SUCH THAT A 30"×48" LEVEL SIDEWALK SURFACE IS LOCATED WITHIN 10 INCHES OF EACH PEDESTRIAN PUSHBUTTON PER FEDERAL GUIDELINES. PUSHBUTTONS SHALL BE APS COMPLIANT WITH AUDIBLE AND TACTILE FEEDBACK.
- 38. WHEN CONDUIT EXCAVATION AND POLE EXCAVATION IS REQUIRED IN SIDEWALK AREAS, CONTRACTOR SHALL REPLACE THE ENTIRE SIDEWALK SLAB TO THE EXPANSION JOINT, SAWCUT AND REPAIR OF SIDEWALK FOR CONDUIT INSTALLATION AND POLE INSTALLATION IS UNACCEPTABLE.
 COST FOR SIDEWALK REPLACEMENT SHALL BE PAID FOR AS PART OF ITEM 206.03 - CONDUIT EXCAVATION AND BACKFILL.
- 39. IT SHALL BE THE CONTRACTORS RESPONSIBILITY, IN CONJUNCTION WITH THE SUPPLIER/MANUFACTURER, TO DETERMINE THE APPROPRIATE MAST ARM POLE STRENGTH AND FOOTING MOMENT FOR THE LOADING SHOWN IN ARM POLE STRENGTH AND FOOTING MOMENT FOR THE LUADING SHOWN IN THE PLANS. THE FOOTING DATA SHALL BE INCLUEDE IN THE SHOP DRAWING THE CONTRACTOR SUBMITS FOR APPROVAL BY THE ENGINEER. IN NO CASE SHALL THE FOUNDATION FOR ANY MAST ARM POLE BE SMALLER THAN A K-8 PER NYSDOT STANDARD SHEET 680-01. ALL MAST ARM FOUNDATIONS ARE TO BE SQUARE.
- 40. MAST ARM MOUNTING BRACKETS SHALL BE PELCO ASTRO-BRAC OR APPROVED EQUAL.
- 41. ALL MAST ARM POLES, SIGNAL POLES AND PEDESTRIAN POLES SHALL BE FLUTED WITH DECORATIVE BASE IN A STYLE APPROVED BY THE CITY ENGINEER. POLES SHALL BE COLORED BLACK AT THE FACTORY.
- SIGNAL CONTROLLERS PROVIDED UNDER ITEM NUMBERS 680.80149305 AND 680.93200101 SHALL BE ECONOLITE COBALT OR APPROVED EQUAL, CAPABLE OF COORDINATED SIGNAL OPERATIONS WITHIN THE CORRIDOR. ALL TRAFFIC SIGNALS ASSOCIATED WITH THIS PROJECT (SEE DWGS.
 TSP-1 THRU TSP-4) SHALL BE INTERCONNECTED WIRELESSLY, WITH THE
 CONTROLLER AT GRAND STREET SERVING AS THE SYNCING MASTER
 CONTROLLER FOR THE SYSTEM.

	SIG	NAL PLAN LEGEND
PROPOSED	EXISTING	DESCRIPTION
•	0	SIGNAL POLE
—		GROUND MOUNTED CONTROLLER
		GROUND MOUNTED CONTROLLER
•	80	POLE MOUNTED CONTROLLER
		SPAN WIRE ASSEMBLY
—s—	—-s—	MESSENGER CABLE INSTALLATION
		CONDUIT
€	3	PULLBOX (TRAFFIC SIGNALS & NUMBER)
•	0>	TRAFFIC SIGNAL HEAD - 1 WAY
← ••	4-0>	TRAFFIC SIGNAL HEAD - 2 WAY
+ ‡+	4♣	TRAFFIC SIGNAL HEAD - 3 WAY
←	4- ∳→	TRAFFIC SIGNAL HEAD - 4 WAY
•	0-00	PROGRAMMED SIGNAL HEAD - 1 WAY
î.,	\$ o–bb	PROGRAMMED SIGNAL HEAD - 2 WAY
3	3	SIGNAL HEAD ASSEMBLY NUMBER
	-	INDUCTANCE LOOP & ALUMINUM JUNCTION BOX
4	4	SIGN FACE NUMBER
ø	Ø	PHASE
		REFLECTORIZED PAV'T STRIPES
þ	٥	TRAFFIC SIGN (GROUND MOUNTED)
	Т	OVERHEAD TRAFFIC SIGN
> —	> ─	MICROWAVE DETECTOR
		WIRELESS VEHICLE DETECTION SYSTEM SENSOR
Ø	\boxtimes	WIRELESS MICRO-RADAR BICYCLE SENSOR
SSP 💻	SSP □─	WIRELESS VEHICLE DETECTION ANTENNA
	++++	DIRECTIONAL ANTENNA FOR WIRELESS INTERCONNECT
3 3		SIGN CALLOUT LOCATION NUMBER TEXT NUMBER

ABBREVIATIONS

2SC = 2 CONDUCTOR SHIELDED LEAD-IN CABLE 3SC = 3 CONDUCTOR SHIELDED CABLE 4SC = 4 CONDUCTOR SHIELDED CABLE SCC = SHIELDED COMMUNICATIONS CABLE = 3 CONDUCTOR SIGNAL CABLE 5C = 5 CONDUCTOR SIGNAL CABLE 1OC = 10 CONDUCTOR SIGNAL CABLE 15C = 15 CONDUCTOR SIGNAL CABLE 19C = 19 CONDUCTOR SIGNAL CABLE ECC = ETHERNET COMMUNICATIONS CABLE

= RED = YELLOW = GREEN = ORANGE = WHITE

= BLACK FR = FLASHING RED FY = FLASHING YELLOW P.B. = PUSH BUTTON

O.H. = OVERHEAD N.T.S= NOT TO SCALE

NFS = NATIONAL PIPELINE STANDARD
PE = PREEMPTION
OVLP= OVERLAP PED = PEDESTRIAN

TRAFFIC SIGNAL NOTES

DL34880 DRAWING NO. TSN-1

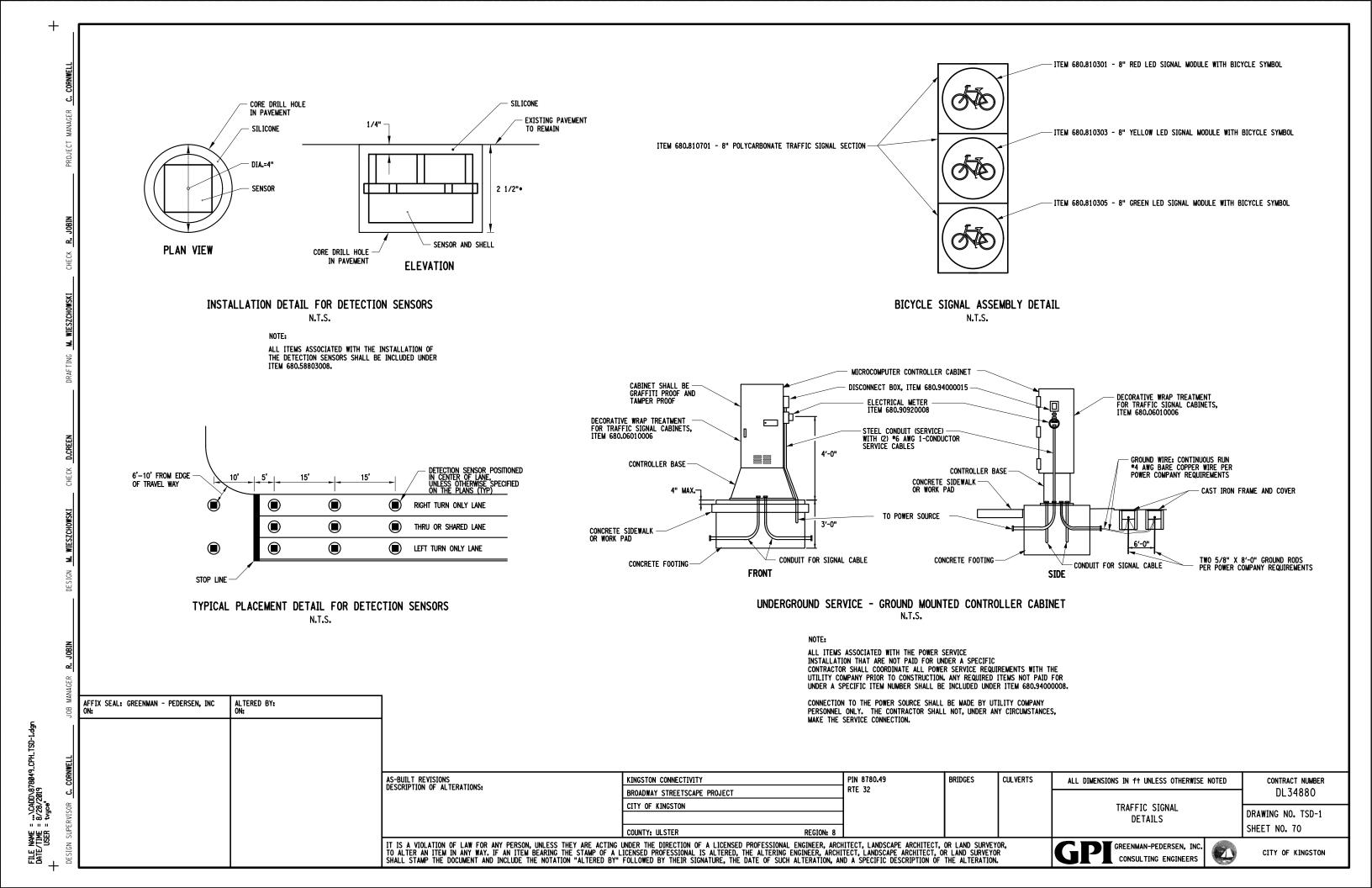
CONTRACT NUMBER

SHEET NO. 69

COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

CONSULTING ENGINEERS

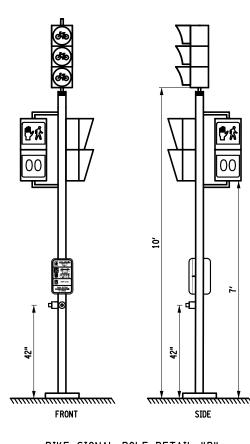
ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED



FRONT SIDE FRONT SIDE FRONT SIDE FRONT SIDE BIKE SIGNAL POLE DETAIL "A" BIKE SIGNAL POLE DETAIL "B" BIKE SIGNAL POLE DETAIL "C" BIKE SIGNAL POLE DETAIL "D" N.T.S. N.T.S. N.T.S. N.T.S. FRONT VIEW IS LOOKING AT POLE FROM THE ADJACENT SIDE STREET CROSSWALK.
SIDE VIEW IS LOOKING AT POLE FROM THE ADJACENT BROADWAY CROSSWALK. AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON: FILE NAME = ...\CADD\878849_CPH_TSD-2.dgr DATE/TIME = 8/28/2019 USER = tvyce' AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 BRIDGES CULVERTS KINGSTON CONNECTIVITY CONTRACT NUMBER ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON TRAFFIC SIGNAL DRAWING NO. TSD-2 DETAILS SHEET NO. 71 COUNTY: ULSTER REGION: 8 IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC. CITY OF KINGSTON CONSULTING ENGINEERS

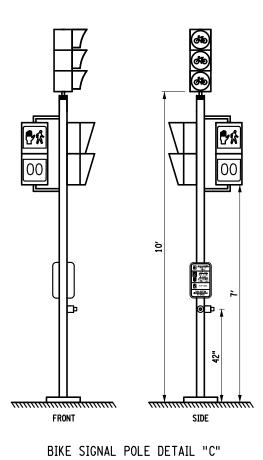
SEE DWG. TSP-1

PLAN SCHEMATIC



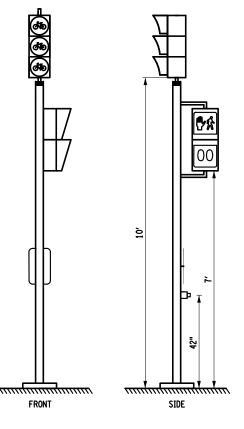
PLAN SCHEMATIC

SEE DWG. TSP-1



PLAN SCHEMATIC

SEE DWG. TSP-1



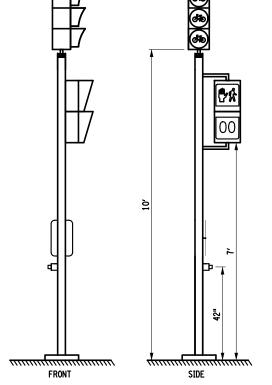
PLAN SCHEMATIC

SEE DWGS. TSP-2 & TSP-3

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NOTE: FRONT VIEW IS LOOKING AT POLE FROM THE ADJACENT SIDE STREET CROSSWALK. SIDE VIEW IS LOOKING AT POLE FROM THE ADJACENT BROADWAY CROSSWALK. AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: ALTERED BY: ON: AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

BIKE SIGNAL POLE DETAIL "E" N.T.S.



PLAN SCHEMATIC

SEE TSP-2



KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

BROADWAY STREETSCAPE PROJECT

PIN 8780.49 RTE 32

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

TRAFFIC SIGNAL

DETAILS

GREENMAN-PEDERSEN, INC.

CONSULTING ENGINEERS

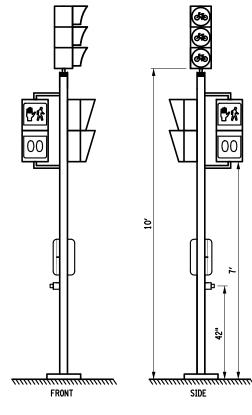
CONTRACT NUMBER

DL34880

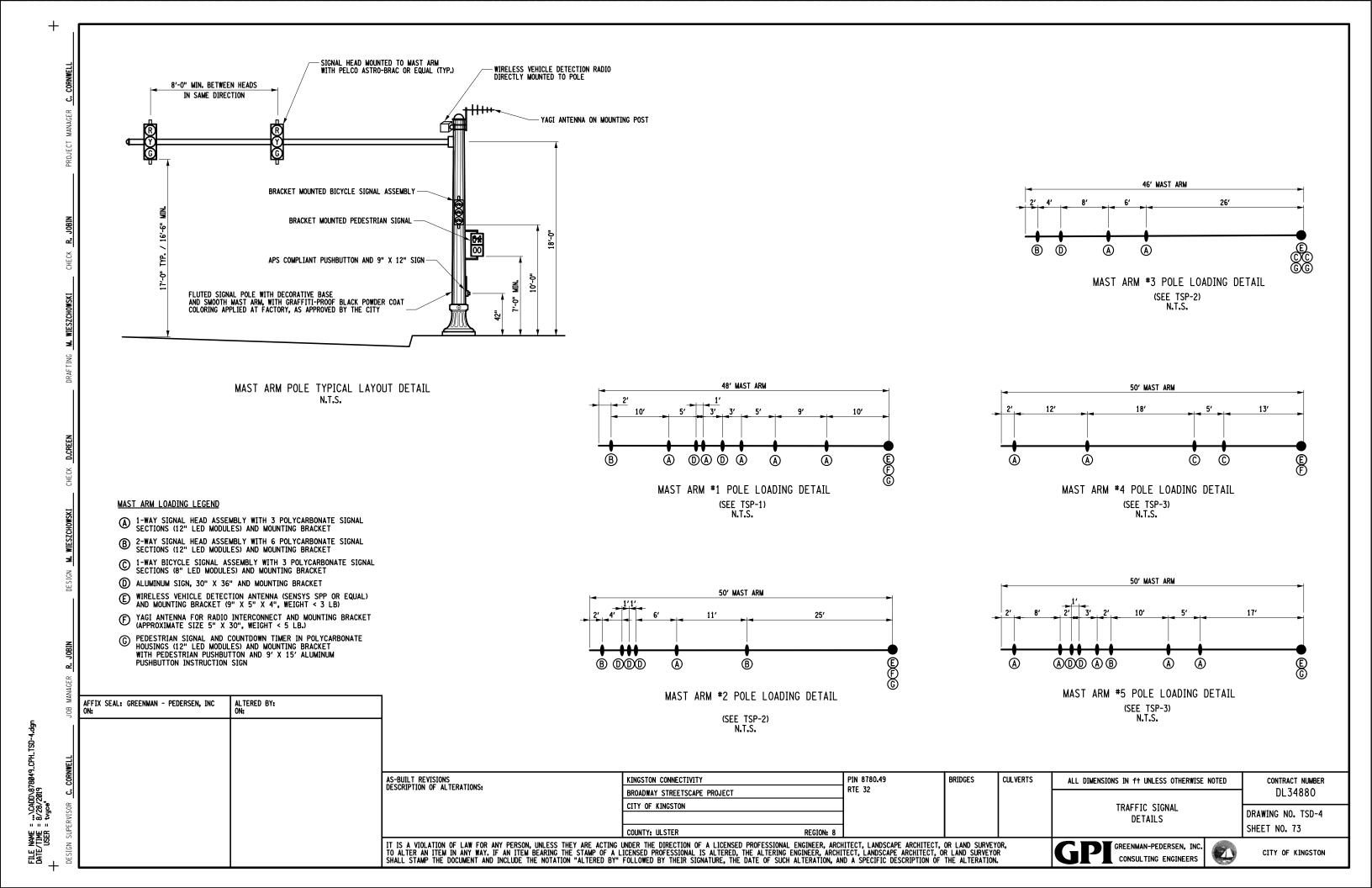
CITY OF KINGSTON

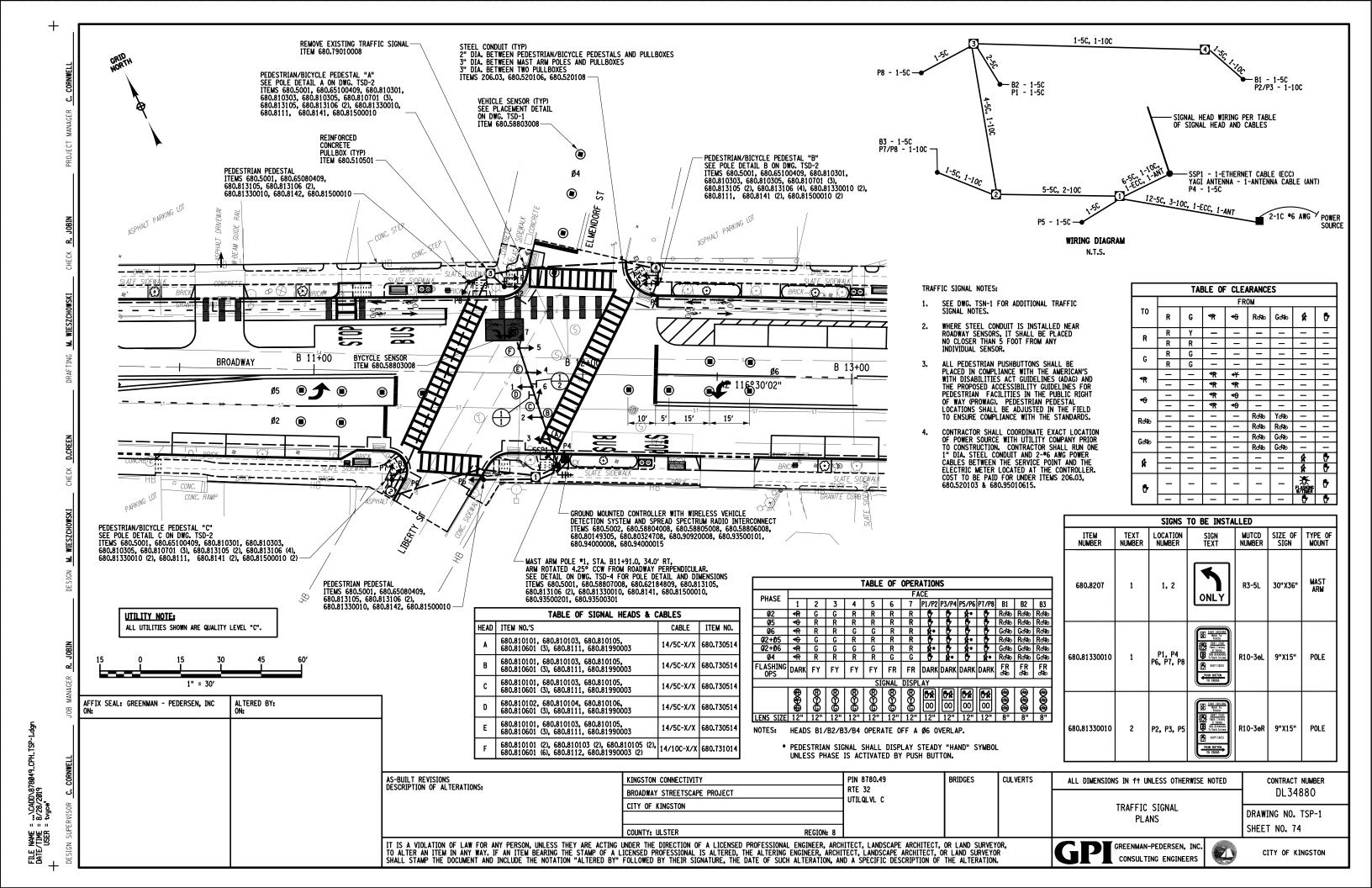
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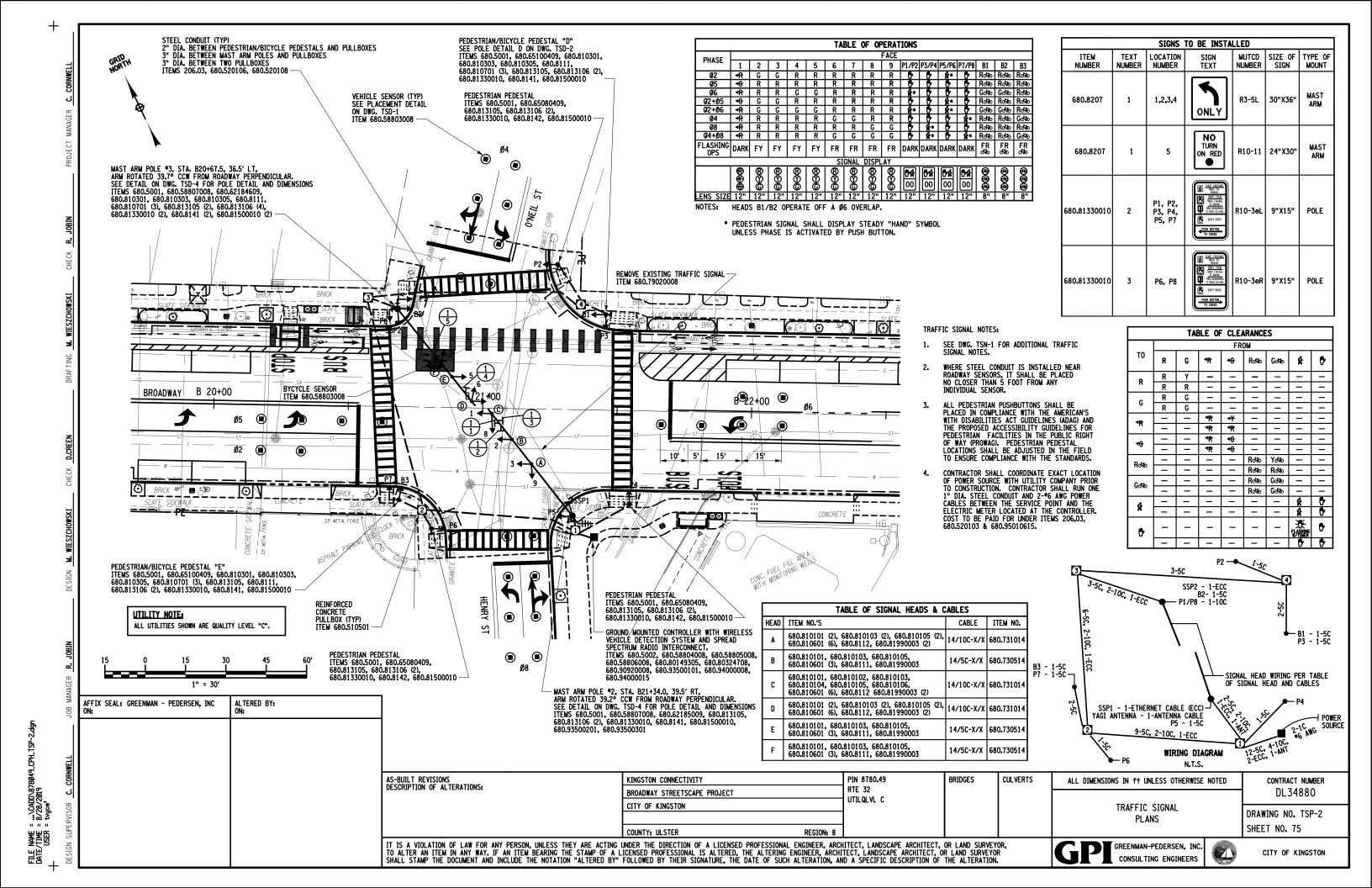
SHEET NO. 72

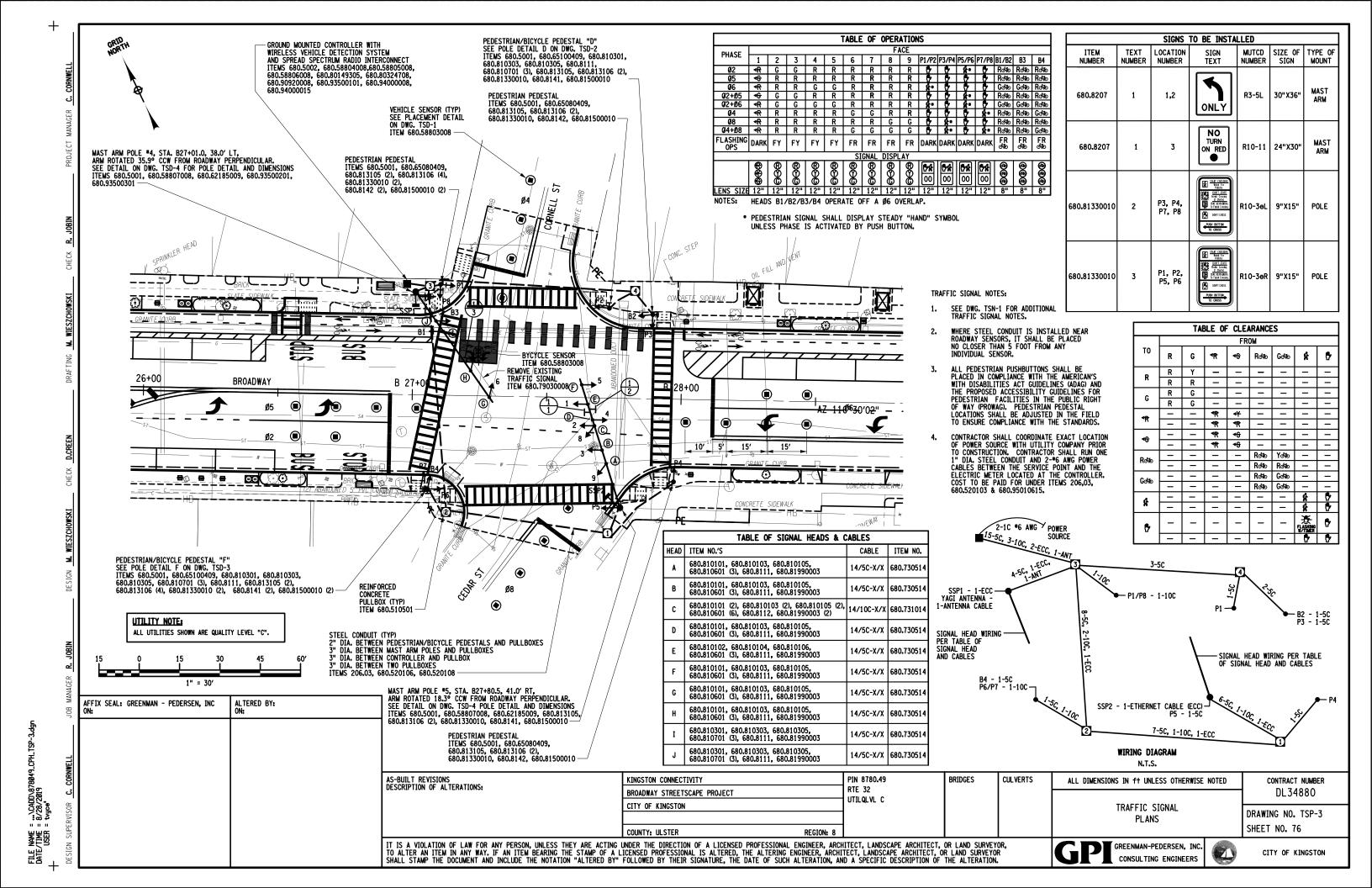


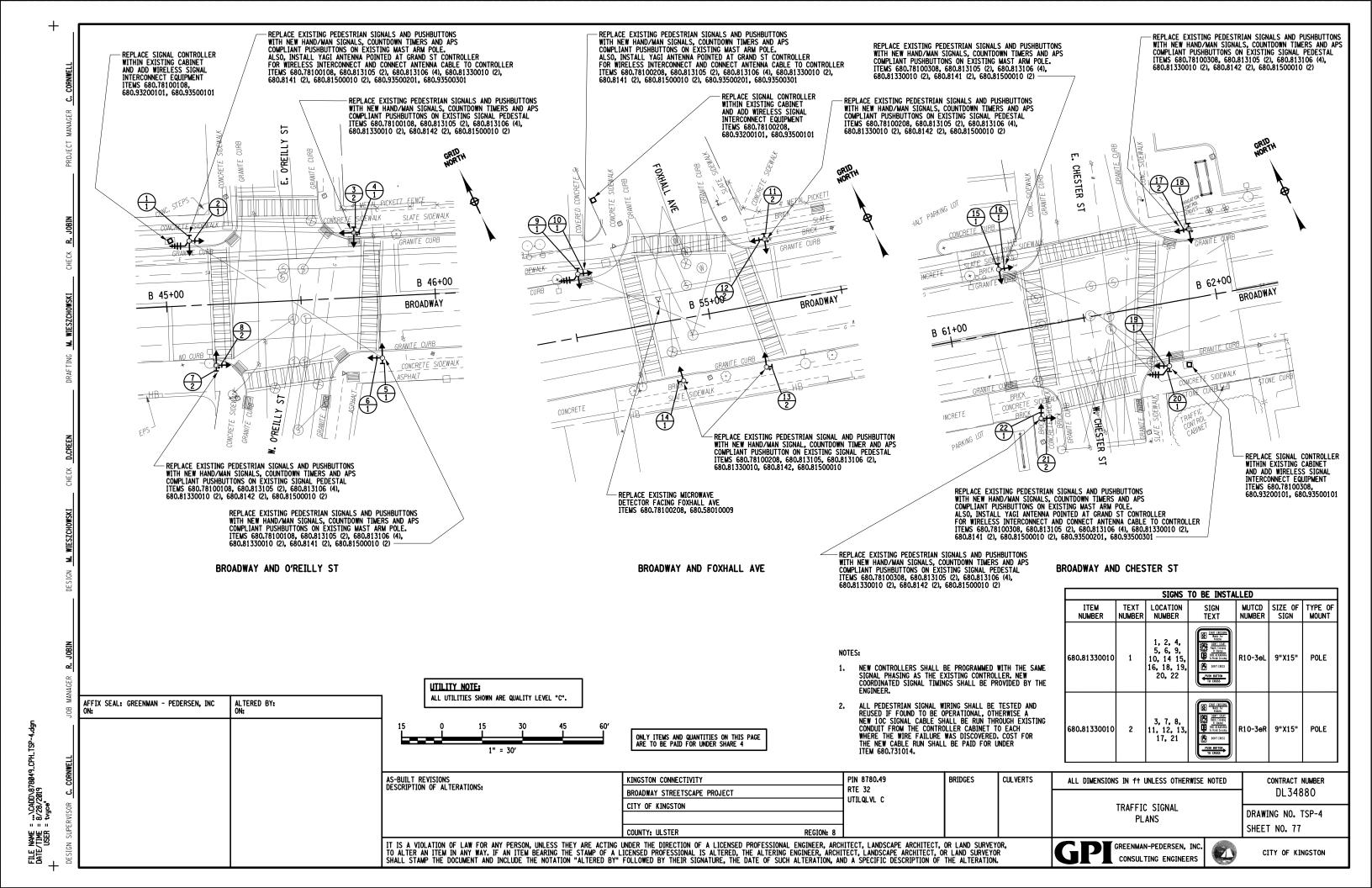






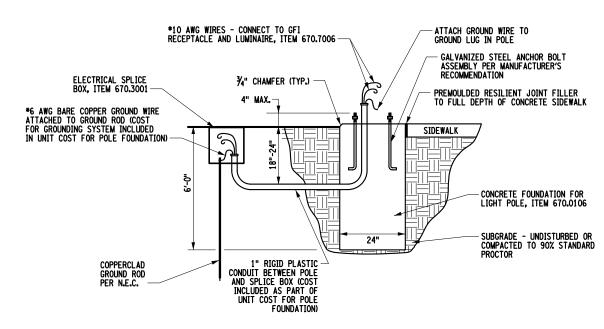




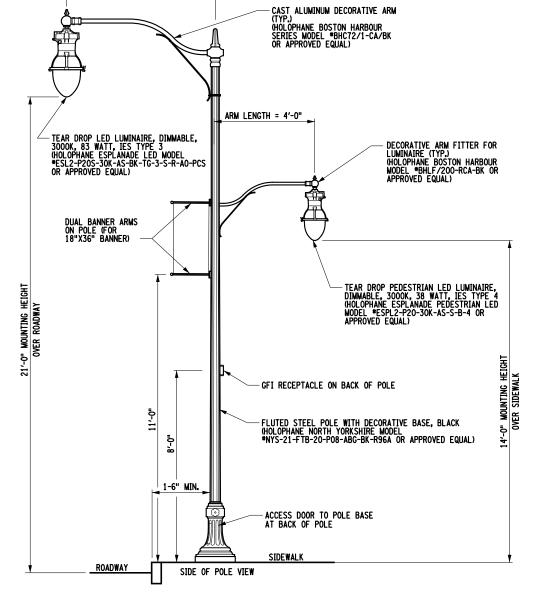


- 1. ALL WORK PERFORMED UNDER THIS CONTRACT SHALL BE IN CONFORMANCE WITH THE CURRENT NEW YORK STATE DEPARTMENT OF TRANSPORTATION (NYSDOT) STANDARD SPECIFICATIONS AS PROVIDED ON THE NYSDOT WEBSITE, UNLESS MODIFIED IN THE
- 2. ALL LIGHTING EQUIPMENT SHALL BE INSTALLED IN ACCORDANCE WITH NEW YORK STATE STANDARDS SHEETS 670-01 TO 670-03, UNLESS MODIFIED IN THE CONTRACT PLANS.
- 3. CONTRACTOR SHALL CONFIRM POWER SERVICE REQUIREMENTS AND LOCATION WITH CENTRAL HUDSON GAS & ELECTRIC CORPORATION PRIOR TO CONSTRUCTION.
- 4. UNDER NO CONDITION SHALL THE CONTRACTOR MAKE THE SERVICE CONNECTIONS. THE CONTRACTOR SHALL RUN CONDUCTORS UP POLE THROUGH RISER AND PROVIDE ENOUGH SLACK AT THE TOP OF THE POLE FOR CENTRAL HUDSON GAS & ELECTRIC CORPORATION TO MAKE CONNECTION. CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH POWER COMPANY FOR
- ANY FOOTINGS PLACED IN SIDEWALKS WILL BE CONSTRUCTED FLUSH TO THE SIDEWALK SURFACE AND BITUMINOUS JOINT FILLER SHALL BE PLACED BETWEEN THE SIDEWALK AND
- 6. EACH LIGHT SHALL BE INSTALLED WITH THE CAPABILITY OF BEING DIMMED. CONTRACTOR SHALL ADJUST DIMMING LEVEL TO THE SATISFACTION OF THE ENGINEER IN CHARGE AND CITY REPRESENTATIVE PRIOR TO FINAL INSPECTION. DIMMER CONTROLS AT EACH POLE SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 670.15100108.

	ESTIMATE OF ROADWAY LIGHTING QUANTITIES		
ITEM NO.	DESCRIPTION	UNIT	GRAND TOTAL
206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	LF	6,100
670.0106	FOUNDATION FOR LIGHT POLE, 6 FOOT LONG	EA	45
670.15100108	DECORATIVE STREET LIGHT POLE AND LUMINAIRE ASSEMBLY, TYPE A	EA	45
670.2602	RIGID PLASTIC CONDUIT, 2" DIA.	LF	6,100
670.3001	LIGHTING PULLBOX - LESS THAN 5 CF INSIDE VOLUME	EA	40
670.3010	LIGHTING PULLBOX - 7.5 TO 10 CF INSIDE VOLUME	EA	9
670.53070104	POWER POINT DISCONNECT ASSEMBLY - LOCATION A	EA	1
670.53070204	POWER POINT DISCONNECT ASSEMBLY - LOCATION B	EA	1
670.53070304	POWER POINT DISCONNECT ASSEMBLY - LOCATION C	EA	1
670.7002	SINGLE CONDUCTOR CABLE, *2 AWG	LF	800
670.7004	SINGLE CONDUCTOR CABLE, *6 AWG	LF	33,800
670.7006	SINGLE CONDUCTOR CABLE, *10 AWG	LF	4,600
670.7501	GROUND WIRE #6 AWG	LF	6,800
670.77000105	MODIFY LIGHTING INSTALLATION, TYPE 1 (REMOVE LIGHT POLE)	EA	44
670.77000205	MODIFY LIGHTING INSTALLATION, TYPE 2 (CIRCUIT TESTING)	EA	5
670.77000305	MODIFY LIGHTING INSTALLATION, TYPE 3 (BILLBOARD CONNECTION)	EA	2
680.700606	RISER ASSEMBLY, 2 INCH DIAMETER	EA	3
680.90920008	ELECTRIC METER SOCKET, 200 AMP, SINGLE PHASE 240 VOLT	EA	3



POLE FOUNDATION DETAIL NOT TO SCALE



TYPE A - LIGHT POLE DETAIL DOUBLE ARM ASSEMBLY

NOT TO SCALE

JUB M	AFFIX SEAL: GREENMAN - PEDERSEN, INC ON:	ALTERED BY: ON:	
C. COKNWELL			<i>1</i>
N SUPERVISOR			
z			1

AS-BUILT REVISIONS	KINGSTON CONNECTIVITY
DESCRIPTION OF ALTERATIONS:	BROADWAY STREETSCAPE PROJECT
	CITY OF KINGSTON
	COUNTY: ULSTER REGION:

PIN 8780.49 RTE 32

BRIDGES CULVERTS

ARM LENGTH = 6'-0"

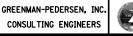
ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

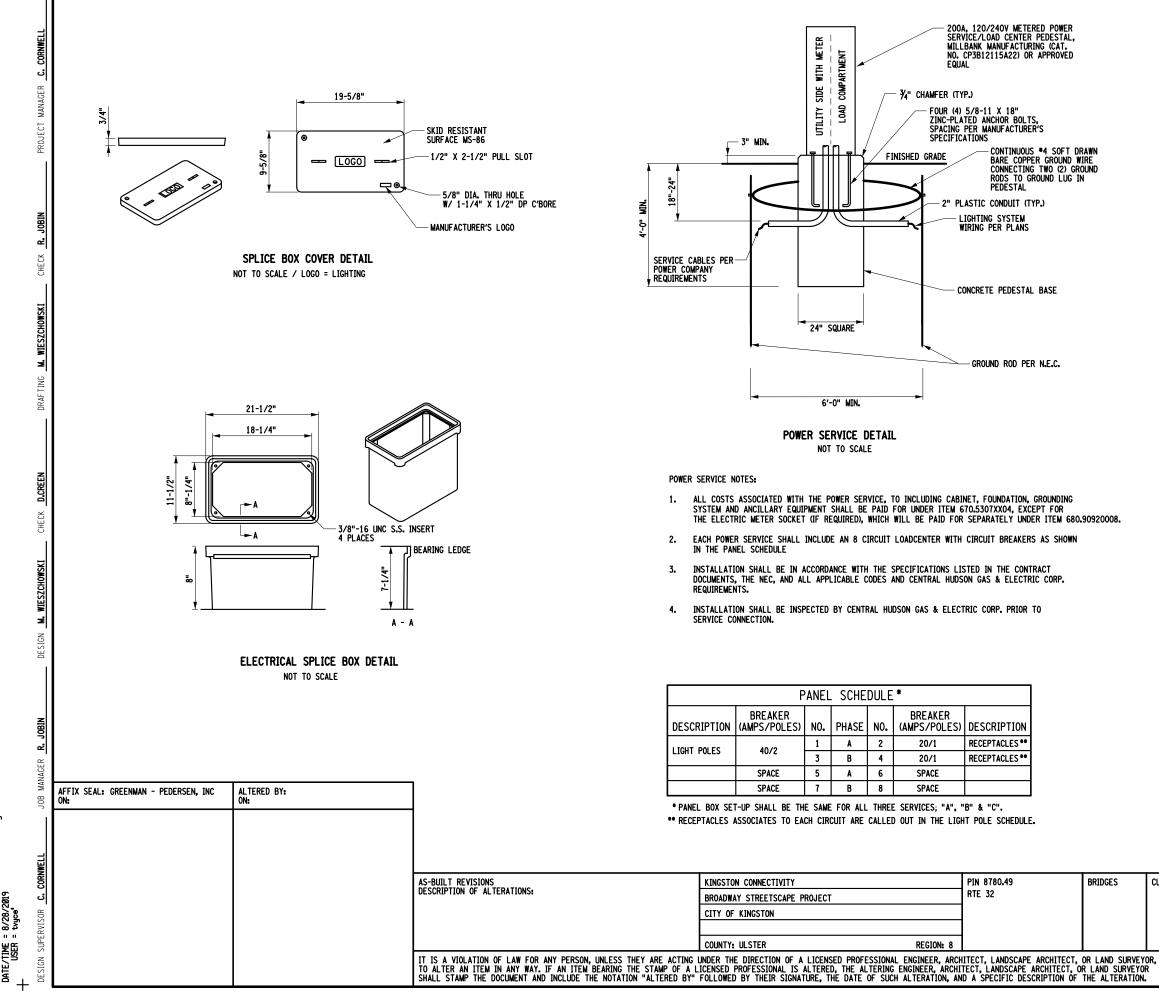
LIGHTING DETAILS

CONTRACT NUMBER DL34880

DRAWING NO. UTD-1 SHEET NO. 78

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.





SERVICE CABLES PER POWER COMPANY REQUIREMENTS	200A, 120/240V METERED POWER SERVICE/LOAD CENTER PEDESTAL, MILLBANK MANUFACTURING (CAT. NO. CP3B12115A22) OR APPROVED EQUAL FOUR (4) 5/8-11 X 18" ZINC-PLATED ANCHOR BOLTS, SPACING PER MANUFACTURER'S SPECIFICATIONS FINISHED GRADE CONTINUOUS *4 SOFT DRAWN BARE COPPER GROUND WIRE CONNECTING TWO (2) GROUND RODS TO GROUND LUG IN PEDESTAL 2" PLASTIC CONDUIT (TYP.) LIGHTING SYSTEM WIRING PER PLANS CONCRETE PEDESTAL BASE
	GROUND ROD PER N.E.C.
 •	6'-0" MIN.

- ALL COSTS ASSOCIATED WITH THE POWER SERVICE, TO INCLUDING CABINET, FOUNDATION, GROUNDING SYSTEM AND ANCILLARY EQUIPMENT SHALL BE PAID FOR UNDER ITEM 670.5307XX04, EXCEPT FOR THE ELECTRIC METER SOCKET (IF REQUIRED), WHICH WILL BE PAID FOR SEPARATELY UNDER ITEM 680.90920008.
- 2. EACH POWER SERVICE SHALL INCLUDE AN 8 CIRCUIT LOADCENTER WITH CIRCUIT BREAKERS AS SHOWN
- INSTALLATION SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS LISTED IN THE CONTRACT DOCUMENTS, THE NEC, AND ALL APPLICABLE CODES AND CENTRAL HUDSON GAS & ELECTRIC CORP. REQUIREMENTS.
- 4. INSTALLATION SHALL BE INSPECTED BY CENTRAL HUDSON GAS & ELECTRIC CORP. PRIOR TO SERVICE CONNECTION.

PANEL SCHEDULE*												
DESCRIPTION	BREAKER (AMPS/POLES)	NO.	PHASE	NO.	BREAKER (AMPS/POLES)	DESCRIPTION						
LIGHT POLES	S 40/2		A	2	20/1	RECEPTACLES **						
LIGHT POLES	4072	3	В	4	20/1	RECEPTACLES **						
	SPACE	5	A	6	SPACE							
	SPACE	7	В	8	SPACE							

POLE NO. A-1 A-2 A-3 A-4	STATION B 06+32 B 06+91 B 07+81 B 08+45	0FFSET 29.2' 21.5'	SIDE RT	POLE TYPE	CI LIGHT	RCUITS RECEPTACLE
A-1 A-2 A-3	B 06+32 B 06+91 B 07+81 B 08+45	29.2′ 21.5′		TYPE	LIGHT	RECEPTACLE
A-2 A-3	B 06+91 B 07+81 B 08+45	21.5′	RT			
A-3	B 07+81 B 08+45			A	A1	A4
	B 08+45	70.47	LT	A	A1	A2
A-4		30.1	RT	A	A1	A2
** *	- aaa	23.2′	LT	A	A1	A4
A-5	B 09+31	31.1′	RT	A	A1	A4
A-6	B 09+93	26.9′	LT	A	A1	A2
A-7	B 10+83	34.4′	RT	A	A1	A2
A-8	B 11+36	30.4′	LT	A	A1	A4
A-9	B 12+40	33.2′	RT	A	A1	A4
A-10	B 12+70	32.4′	LT	A	A1	A2
A-11	B 13+77	33.6′	RT	A	A1	A2
A-12	B 14+17	32.8′	LT	A	A1	A4
A-13	B 15+09	33.1′	RT	A	A1	A4
A-14	B 15+72	32.4′	LT	A	A1	A2
A-15	B 16+59	32.5′	RT	A	A1	A2
A-16	B 17+11	32.4′	LT	A	A1	A4
A-17	B 18+14	32.4′	RT	A	A1	A4
A-18	B 18+20	32.2′	LT	A	A1	A2
A-19	B 19+11	33.5′	RT	A	A1	A2
A-20	B 19+42	32.4′	LT	A	A1	A4
B-1	B 20+51	32.7′	RT	A	B1	B2
B-2	B 20+39	32.6′	LT	A	B1	B4
B-3	B 21+69	33.0′	RT	A	B1	B4
B-4	B 21+88	32.2′	LT	A	B1	B2
B-5	B 22+98	31.4'	RT	A	B1	B2
B-6	B 22+97	32.0′	LŤ	A	B1	B4
B-7	B 24+06	31.5′	RT	A	B1	B4
B-8	B 24+06	31.9′	LT	A	B1	B2
B-9	B 25+35	32.4′	RT	A	B1	B2
B-10	B 25+41	32.3′	LT	A	B1	B4
B-11	B 26+65	32.2′	RT	A	B1	B4
B-12	B 26+72	31.8′	LT	A	B1	B2
B-13	B 28+11	28.1′	RT	A	B1	B2
B-14	B 28+07	28.9′	LT	A	B1	B4
B-15	B 29+45	28.6′	RT	A	B1	B4
B-16	B 29+25	29.4′	LT	A	B1	B2
B-17	B 30+83	27.8′	RT	A	B1	B2
B-18	B 30+73	29.1′	LT	A	B1	B4
C-1	B 33+09	27.8′	RT	A	C1	C4
C-2	B 32+94	29.1′	LT	A	C1	C2
C-3	B 34+61	27.9′	RT	A	C1	C2
C-4	B 34+18	29.5′	LT	A	C1	C4
C-5	B 36+15	27.7′	RT	A	C1	C4
C-6	B 35+75	28.7′	LT	A	C1	C2
C-7	B 37+68	27.8′	RT	A	C1	C2

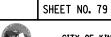
GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

LIGHTING DETAILS

BRIDGES

CULVERTS

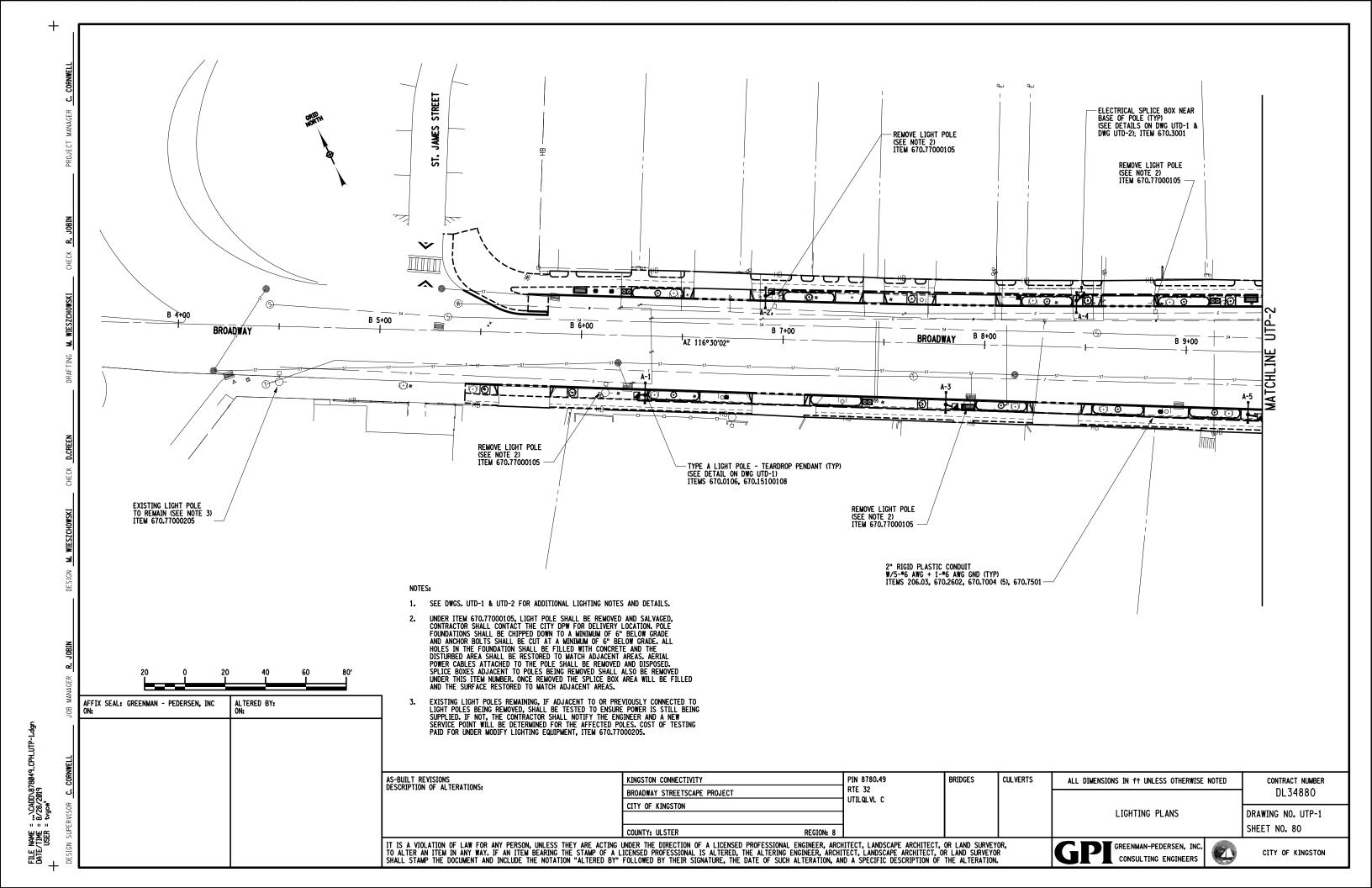


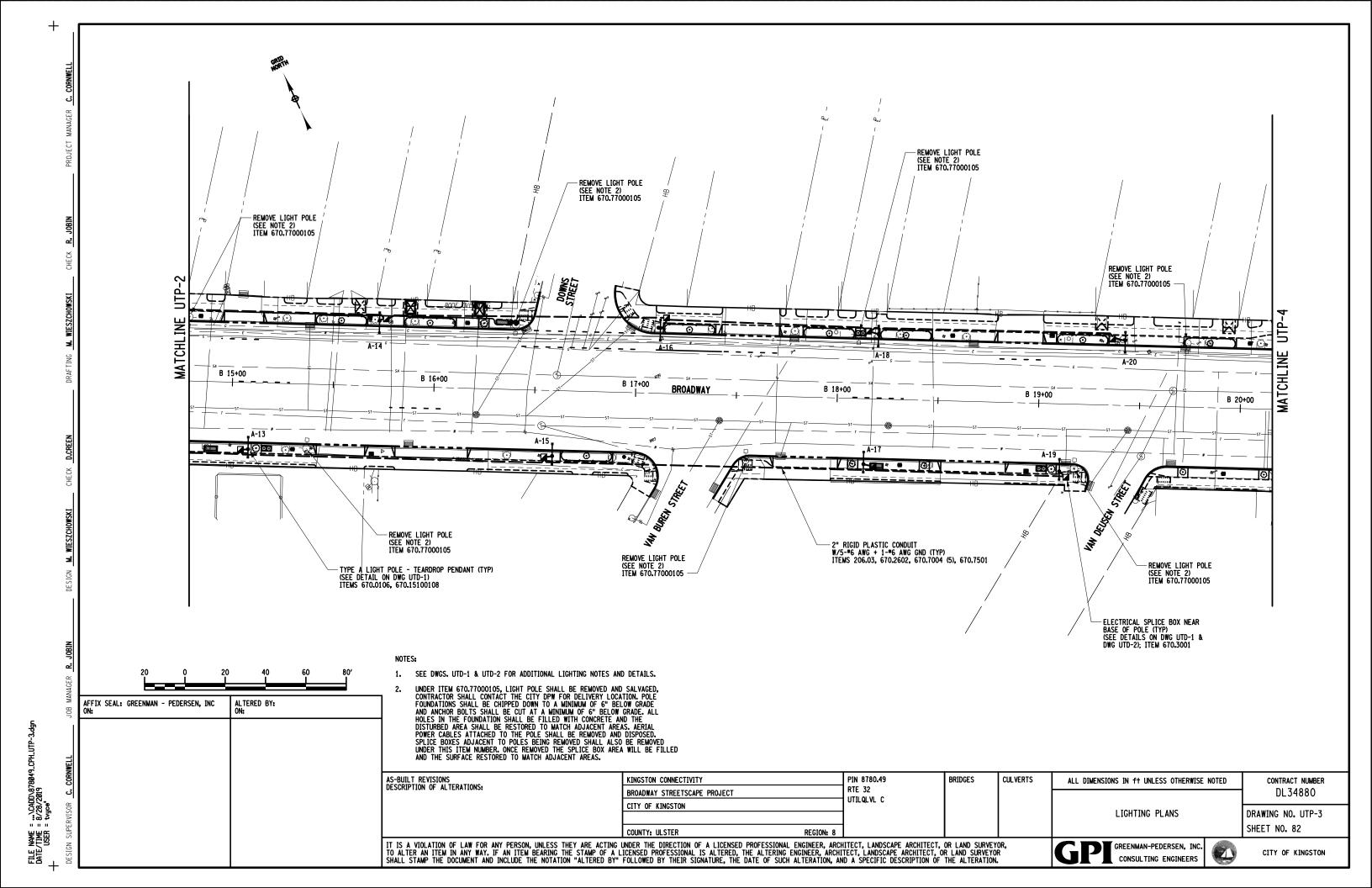
CITY OF KINGSTON

DRAWING NO. UTD-2

CONTRACT NUMBER

DL34880



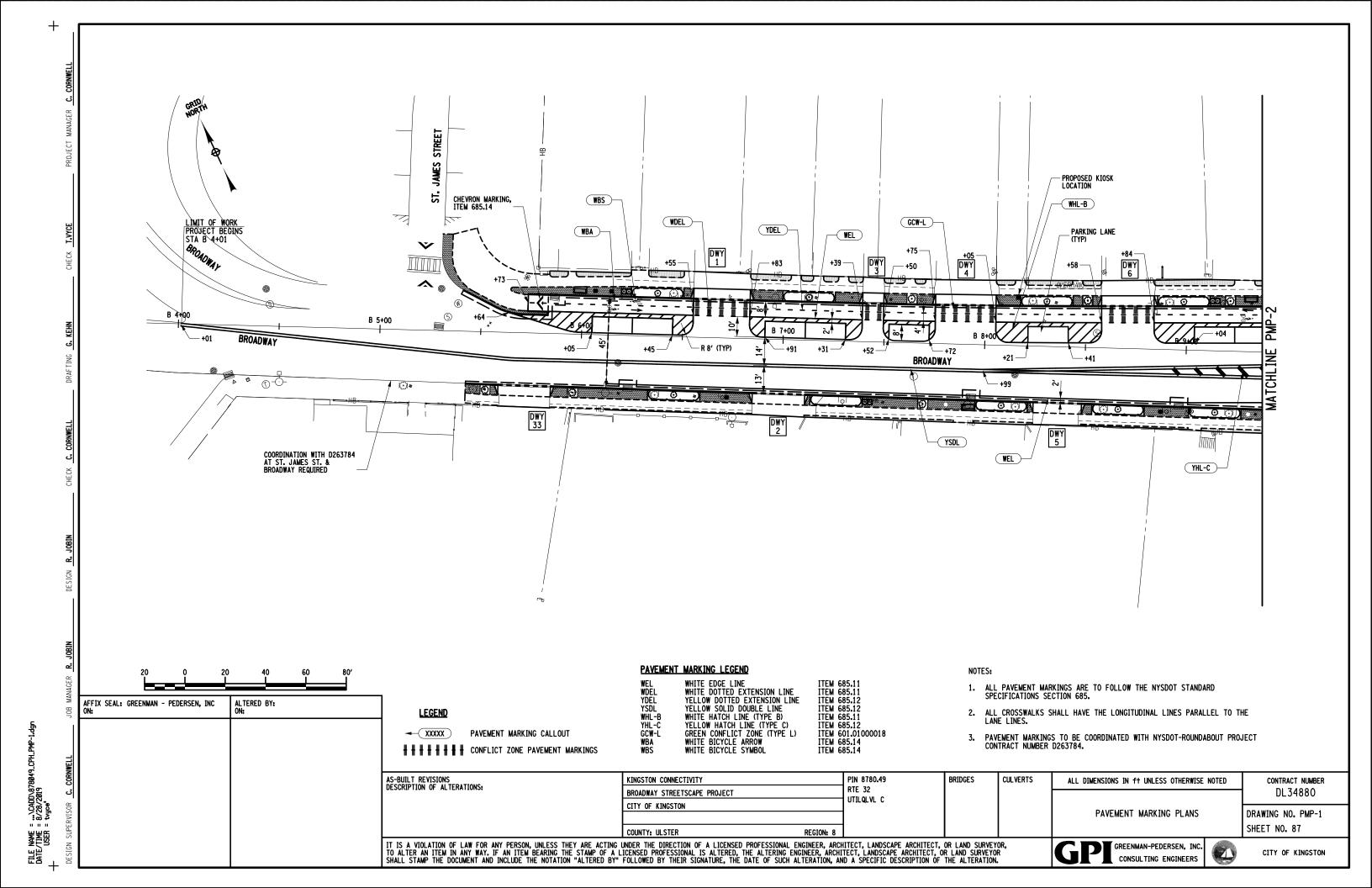


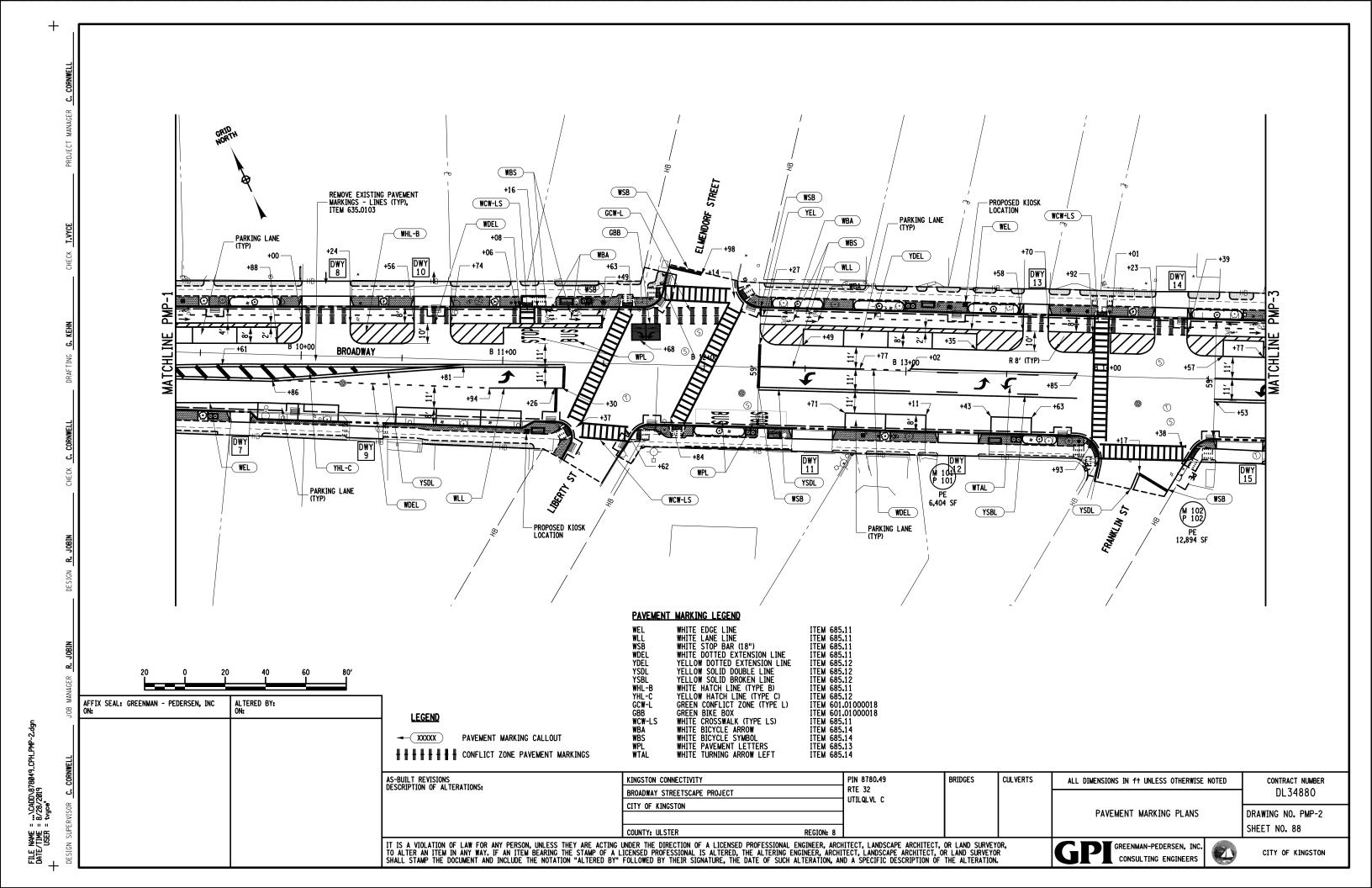
NOTES: SEE DWGS. UTD-1 & UTD-2 FOR ADDITIONAL LIGHTING NOTES AND DETAILS. UNDER ITEM 670.77000105, LIGHT POLE SHALL BE REMOVED AND SALVAGED, CONTRACTOR SHALL CONTACT THE CITY DPW FOR DELIVERY LOCATION, POLE FOUNDATIONS SHALL BE CHIPPED DOWN TO A MINIMUM OF 6" BELOW GRADE AND ANCHOR BOLTS SHALL BE CUT AT A MINIMUM OF 6" BELOW GRADE. ALL HOLES IN THE FOUNDATION SHALL BE FILLED WITH CONCRETE AND THE DISTURBED AREA SHALL BE RESTORED TO MATCH ADJACENT AREAS, AERIAL POWER CABLES ATTACHED TO THE POLE SHALL BE REMOVED AND DISPOSED. SPLICE BOXES ADJACENT TO POLES BEING REMOVED SHALL ALSO BE REMOVED UNDER THIS ITEM NUMBER. ONCE REMOVED THE SPLICE BOX AREA WILL BE FILLED AND THE SURFACE RESTORED TO MATCH ADJACENT AREAS. 26"X18" REINFORCED CONCRETE PULLBOX (TYP)
(SEE NYSDOT STANDARD SHEET 680-02)
LEGEND = "LIGHTING"
| TEM 670.3010 -2" RIGID PLASTIC CONDUIT W/5-96 AWG + 1-96 AWG GND (TYP) ITEMS 206.03, 670.2602, 670.7004 (5), 670.7501 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 - ELECTRICAL SPLICE BOX NEAR BASE OF POLE (TYP) (SEE DETAILS ON DWG UTD-1 & DWG UTD-2); ITEM 670,3001 B-2 B 21+00 MATCHLIN B 22+00 B 23+00 BROADWAY B 25+00 AZ 116°30′02" Xmac Xmac REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 2" RIGID PLASTIC CONDUIT W/5-*6 AWG + 1-*6 AWG GND (TYP) ITEMS 206.03, 670.2602, 670.7004 (5), 670.7501 POWER SERVICE POINT "B" (SEE DETAIL AND CIRCUIT INFORMATION ON DWG UTD-2) ITEMS 670.53070204, 680.90920008 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 - TYPE A LIGHT POLE - TEARDROP PENDANT (TYP) (SEE DETAIL ON DWG UTD-1) ITEMS 670.0106, 670.15100108 ALTERED BY: ON: AFFIX SEAL: GREENMAN - PEDERSEN, INC 2" RIGID PLASTIC CONDUIT W/2-*2 AWG FROM UTILITY POLE TO SERVICE POINT ITEMS 206.03, 670.2602, 670.7002 (2) E = ...\CADD\878849_CPH_UTP-4,dg E = 8/28/2019 R = tvyce' 2" RISER ASSEMBLY ON UTILITY POLE ITEM 680.700606 AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 BRIDGES CULVERTS KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT UTILQLVL C CITY OF KINGSTON LIGHTING PLANS DRAWING NO. UTP-4 SHEET NO. 83 COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC CITY OF KINGSTON CONSULTING ENGINEERS

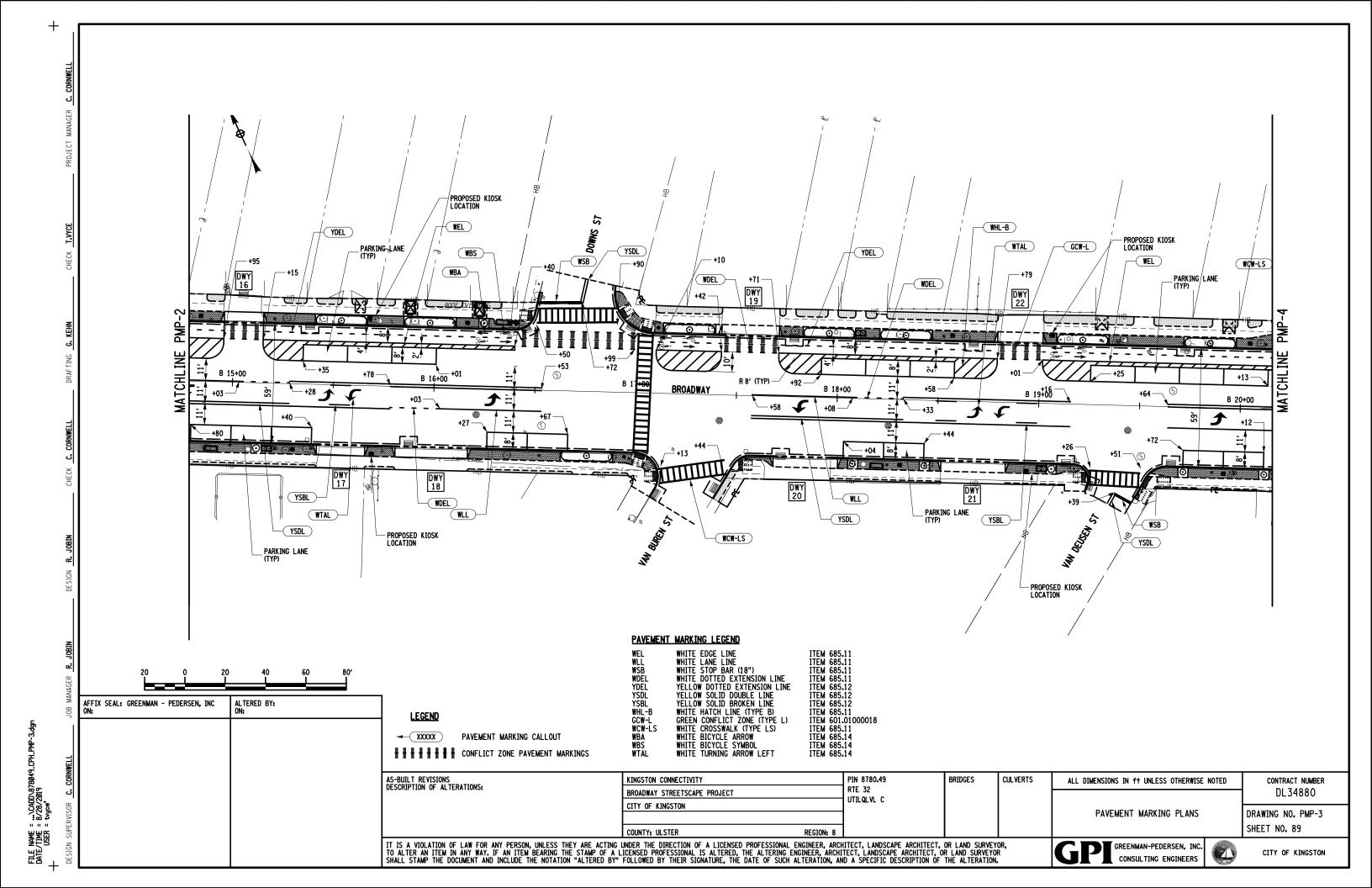
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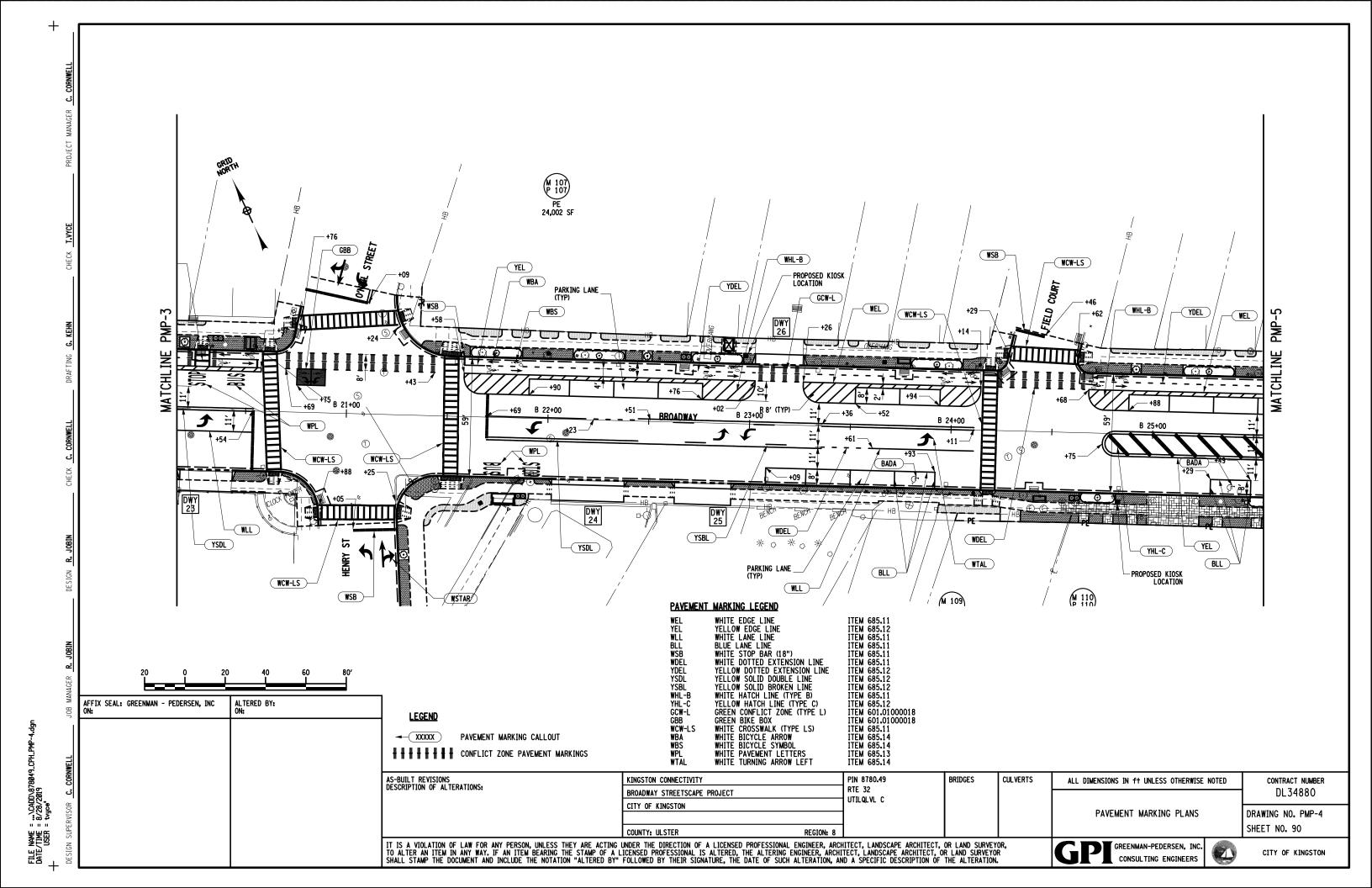
- AERIAL POWER CABLE CONNECTION FROM LIGHT POLE TO BILLBOARD 2-*6 AWG +1-*6 AWG GND WITH INTEGRAL MESSENGER CABLE (SEE NOTE 3) 2" RIGID PLASTIC CONDUIT W/2-*2 AWG FROM UTILITY POLE TO SERVICE POINT ITEMS 206.03, 670.2602, 670.7002 (2) ITEMS 670.7004 (2), 670.7501, 670.77000305 POWER SERVICE POINT "C"
(SEE DETAIL AND CIRCUIT
INFORMATION ON DWG UTD-2)
ITEMS 670.53070304, 680.90920008 — - TYPE A LIGHT POLE - TEARDROP PENDANT (TYP) (SEE DETAIL ON DWG UTD-1) ITEMS 670.0106, 670.15100108 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 26"X18" REINFORCED CONCRETE PULLBOX (TYP)
(SEE NYSDOT STANDARD SHEET 680-02)
LEGEND = "LIGHTING"
ITEM 680.3010 FIXTURES TO REMAIN ON BRIDGE CHLINE MATCHLINE C-2 C-6 B 32+00 B 33+00 MAT B 34+00 B 35+00 B 36+00 AZ 116°30'02' C-5 AERIAL POWER CABLE CONNECTION FROM LIGHT POLE TO BILLBOARD 2-*6 AWG +1-*6 AWG GND WITH INTEGRAL MESSENGER CABLE (SEE NOTE 3) ITEMS 670.7004 (2), 670.7501, 670.77000305 7Ø REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 -2" RIGID PLASTIC CONDUIT W/5-%6 AWG + 1-%6 AWG GND (TYP) ITEMS 206.03, 670.2602, 670.7004 (5), 670.7501 SEE DWGS. UTD-1 & UTD-2 FOR ADDITIONAL LIGHTING NOTES AND DETAILS. UNDER ITEM 670.77000105, LIGHT POLE SHALL BE REMOVED AND SALVAGED, CONTRACTOR SHALL CONTACT THE CITY DPW FOR DELIVERY LOCATION. POLE FOUNDATIONS SHALL BE CHIPPED DOWN TO A MINIMUM OF 6" BELOW GRADE AND ANCHOR BOLTS SHALL BE CUT AT A MINIMUM OF 6" BELOW GRADE. ALL HOLES IN THE FOUNDATION SHALL BE FILLED WITH CONCRETE AND THE DISTURBED AREA SHALL BE RESTORED TO MATCH ADJACENT AREAS, AERIAL POWER CABLES ATTACHED TO THE POLE SHALL BE REMOVED AND DISPOSED. SPLICE BOXES ADJACENT TO POLES BEING REMOVED SHALL ALSO BE REMOVED UNDER THIS ITEM NUMBER, ONCE REMOVED THE SPLICE BOX AREA WILL BE FILLED AND THE SURFACE RESTORED TO MATCH ADJACENT AREAS. - ELECTRICAL SPLICE BOX NEAR BASE OF POLE (TYP) (SEE DETAILS ON DWG UTD-1 & DWG UTD-2) ITEM 670.3001 AFFIX SEAL: GREENMAN - PEDERSEN, INC ALTERED BY: POWER FOR EXISTING BILLBOARD SHALL BE PROVIDED BY THE LIGHTING SYSTEM THROUGH AERIAL POWER FEED FROM NEAREST LIGHT POLE, UNDER ITEM 670,77000305 CONTRACTOR SHALL PROVIDE MESSENGER CABLE BETWEEN LIGHT POLE AND BILLBOARD AND WILL REPLACE THE EXISTING POWER CABLES WITH NEW CABLES TO ENSURE POWER IS PROVIDED TO THE BILLBOARD. COST FOR MESSENGER CABLE TO BE INCLUDED IN THE PRICE FOR ITEM 670,77000305. AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 BRIDGES CULVERTS KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT UTILQLVL C CITY OF KINGSTON LIGHTING PLANS DRAWING NO. UTP-6 SHEET NO. 85 COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC CITY OF KINGSTON CONSULTING ENGINEERS

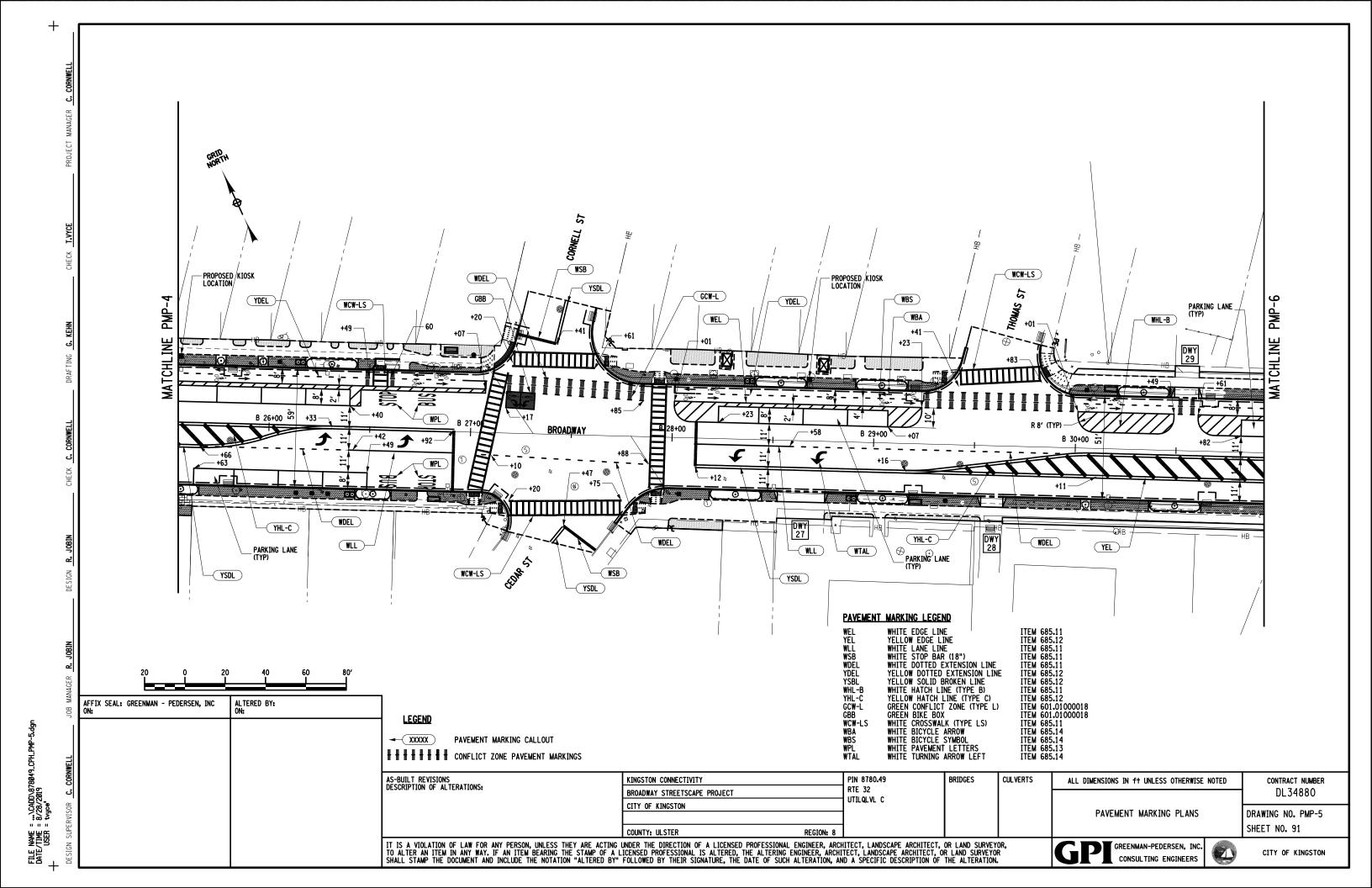
NOTES: SEE DWGS. UTD-1 & UTD-2 FOR ADDITIONAL LIGHTING NOTES AND DETAILS. UNDER ITEM 670.77000105, LIGHT POLE SHALL BE REMOVED AND SALVAGED, CONTRACTOR SHALL CONTACT THE CITY DPW FOR DELIVERY LOCATION. POLE FOUNDATIONS SHALL BE CHIPPED DOWN TO A MINIMUM OF 6" BELOW GRADE AND ANCHOR BOLTS SHALL BE CUT AT A MINIMUM OF 6" BELOW GRADE. ALL HOLES IN THE FOUNDATION SHALL BE FILLED WITH CONCRETE AND THE DISTURBED AREA SHALL BE RESTORED TO MATCH ADJACENT AREAS, AERIAL POWER CABLES ATTACHED TO THE POLE SHALL BE REMOVED AND DISPOSED. SPLICE BOXES ADJACENT TO POLES BEING REMOVED SHALL ALSO BE REMOVED UNDER THIS ITEM NUMBER. ONCE REMOVED THE SPLICE BOX AREA WILL BE FILLED AND THE SURFACE RESTORED TO MATCH ADJACENT AREAS. -2" RISER ASSEMBLY ON UTILITY POLE ITEM 680.700606 -2" RIGID PLASTIC CONDUIT W/2-*2 AWG FROM UTILITY POLE TO SERVICE POINT ITEMS 206.03, 670.2602, 670.7002 (2) EXISTING LIGHT POLES REMAINING, IF ADJACENT TO OR PREVIOUSLY CONNECTED TO LIGHT POLES BEING REMOVED, SHALL BE TESTED TO ENSURE POWER IS STILL BEING SUPPLIED. IF NOT, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND A NEW SERVICE POINT WILL BE DETERMINED FOR THE AFFECTED POLES. COST OF TESTING PAID FOR UNDER MODIFY LIGHTING EQUIPMENT, ITEM 670.770002105. - 26"X18" REINFORCED CONCRETE PULLBOX (SEE NYSDOT STANDARD SHEET 680-02) LEGEND = "LIGHTING" ITEM 680.3010 EXISTING LIGHT POLE TO REMAIN (SEE NOTE 3) ITEM 670.77000205 PROJECT ENDS STA 37+76 COORDINATION WITH GRAND STREET
INTERSECTION PROJECT
AT GRAND STREET &
BROADWAY REQUIRED EXISTING LIGHT POLE TO REMAIN (SEE NOTE 3) ITEM 670.77000205 STREET © ' \odot MATCHL] B 37+00 B 41+00 B 38+00 B 40+00 AZ 112°43'07 AZ 116°30'02" **BROADWAY** 00 A O -O- O oi Oi 0 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 GROVE -2" RIGID PLASTIC CONDUIT W/5-*6 AWG + 1-*6 AWG GND (TYP) ITEMS 206.03, 670.2602, 670.7004 (5), 670.7501 REMOVE LIGHT POLE (SEE NOTE 2) ITEM 670.77000105 EXISTING LIGHT POLE TO REMAIN (SEE NOTE 3) ITEM 670.77000205 TYPE A LIGHT POLE - TEARDROP PENDANT (SEE DETAIL ON DWG UTD-1) ITEMS 670.0106, 670.15100108 EXISTING LIGHT POLE TO REMAIN (SEE NOTE 3) ITEM 670.77000205 -ELECTRICAL SPLICE BOX NEAR BASE OF POLE (SEE DETAILS ON DWG UTD-1 & DWG UTD-2) ITEM 670.3001 ALTERED BY: ON: AFFIX SEAL: GREENMAN - PEDERSEN, INC E = ...\CADD\878849_CPH_UTP-7.dg E = 8/28/2019 R = tvyce* AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 BRIDGES CULVERTS KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT UTILQLVL C CITY OF KINGSTON LIGHTING PLANS DRAWING NO. UTP-7 SHEET NO. 86 COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC CITY OF KINGSTON CONSULTING ENGINEERS

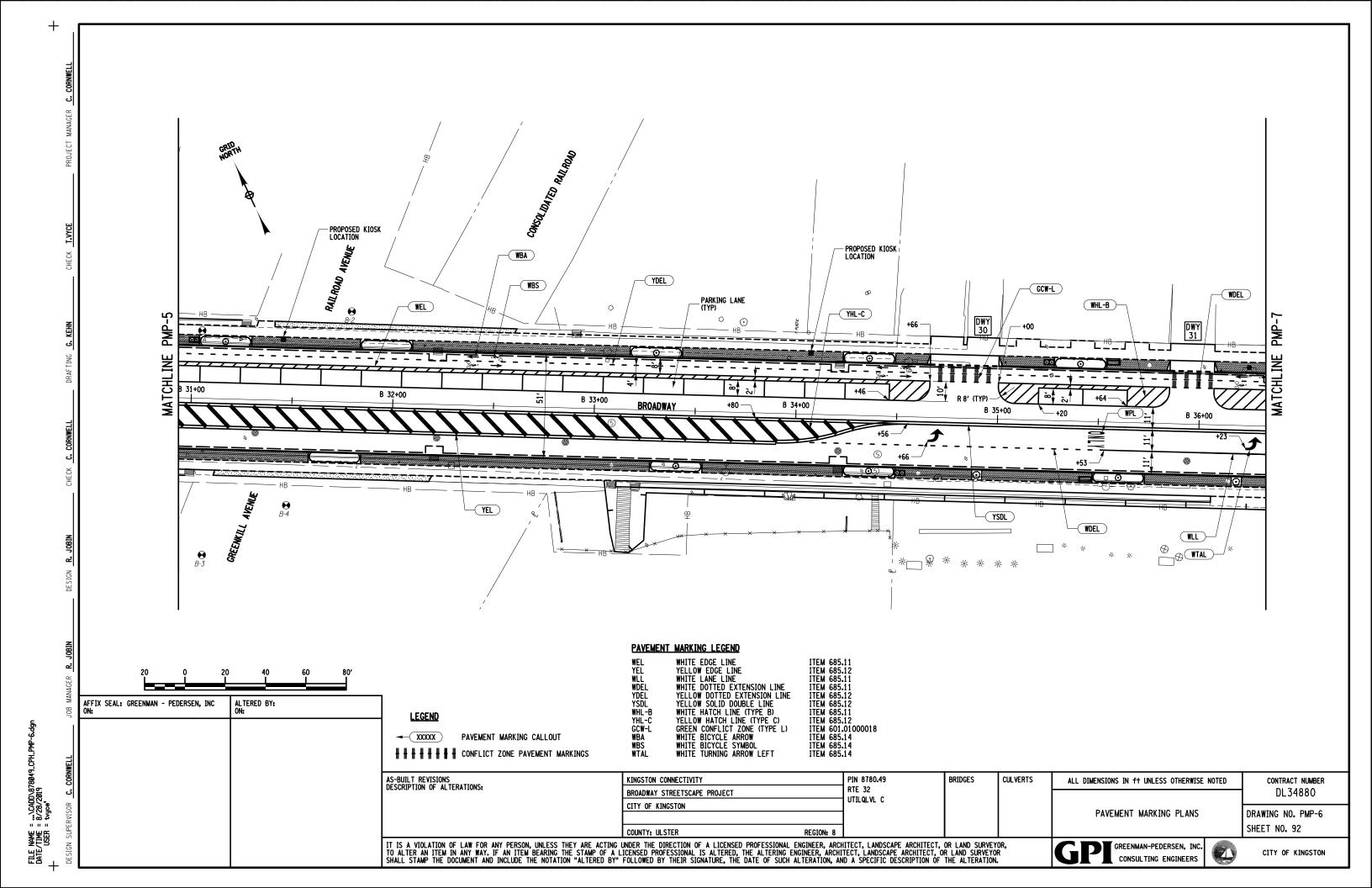


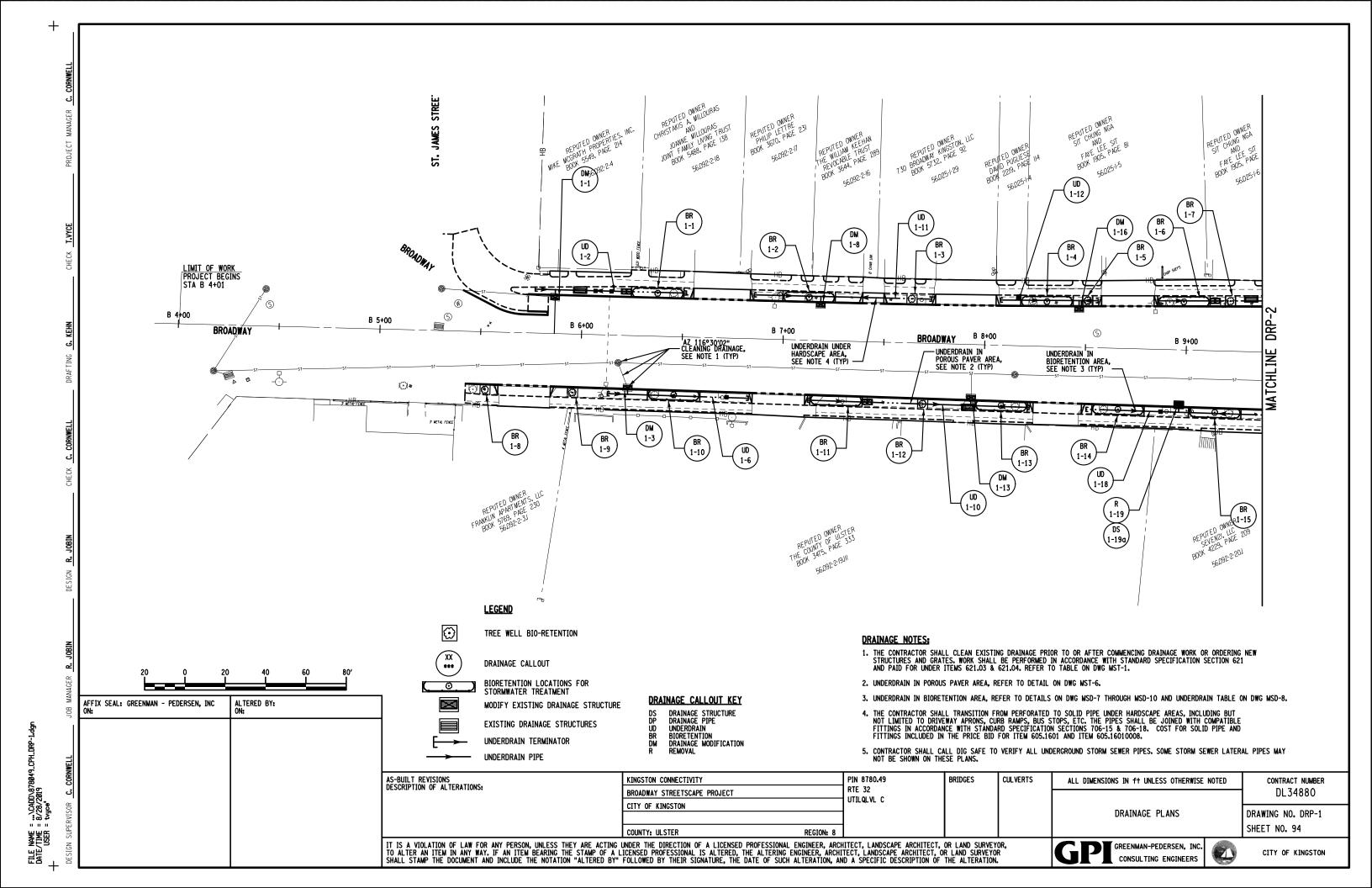


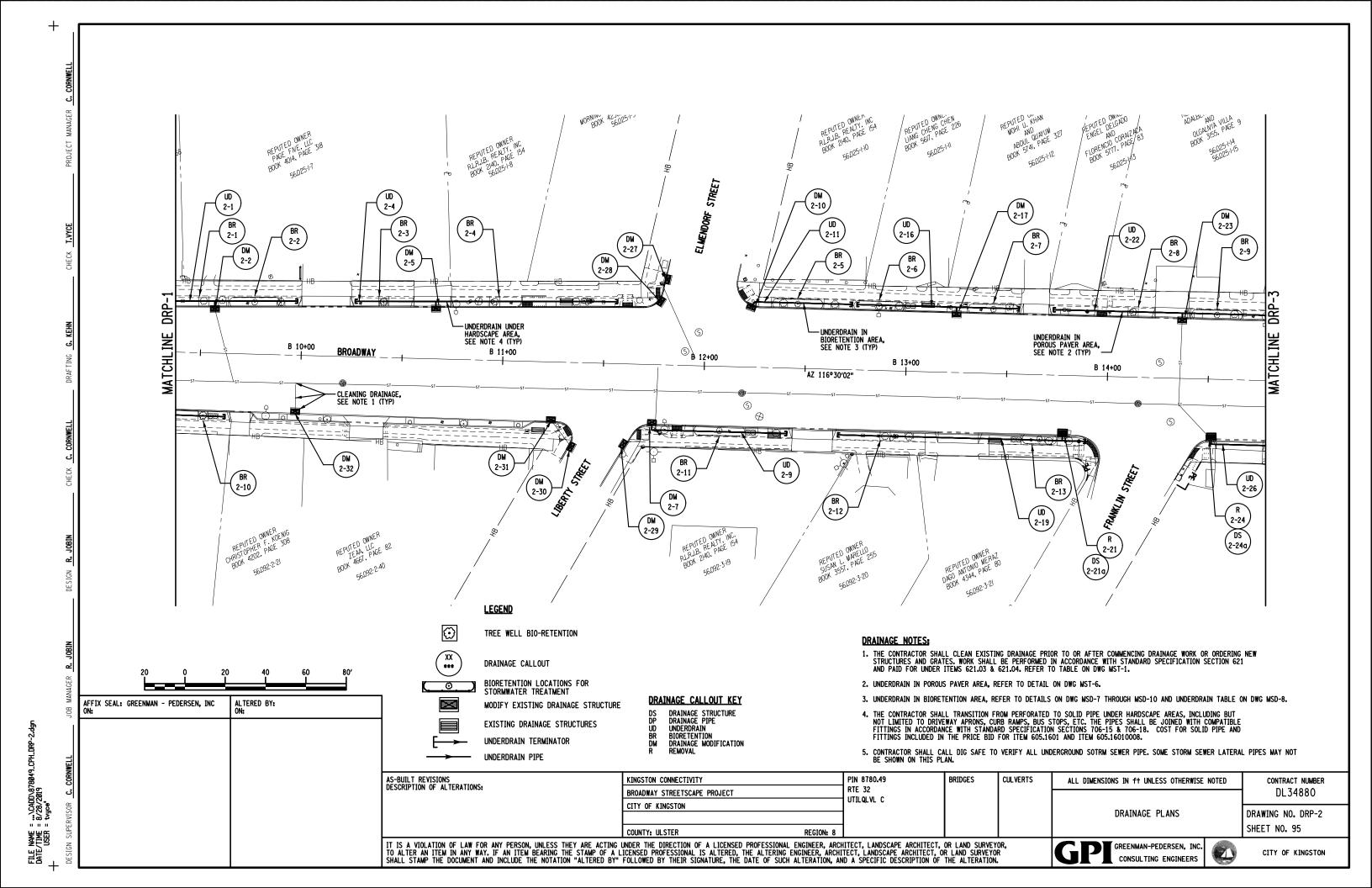


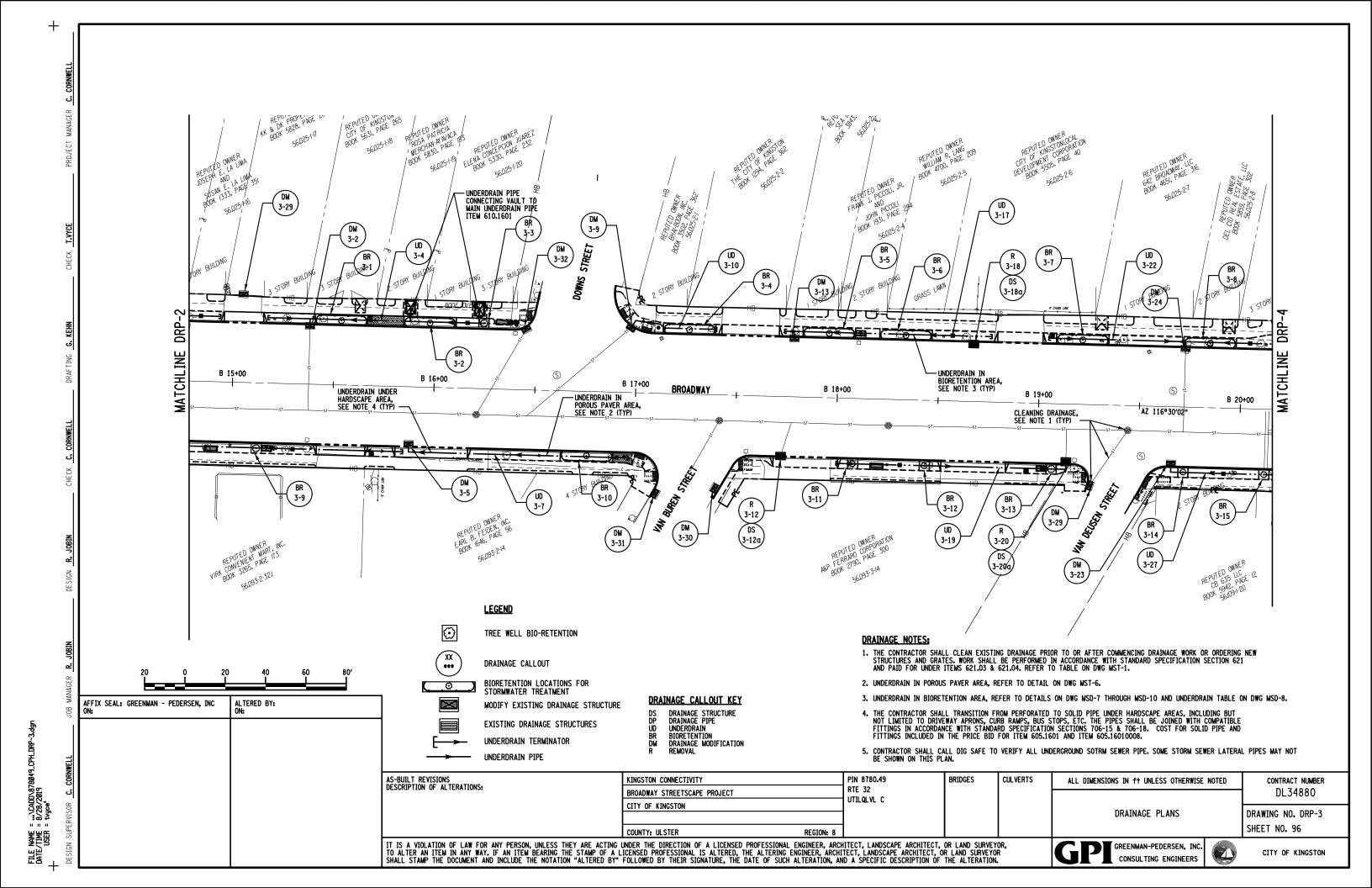


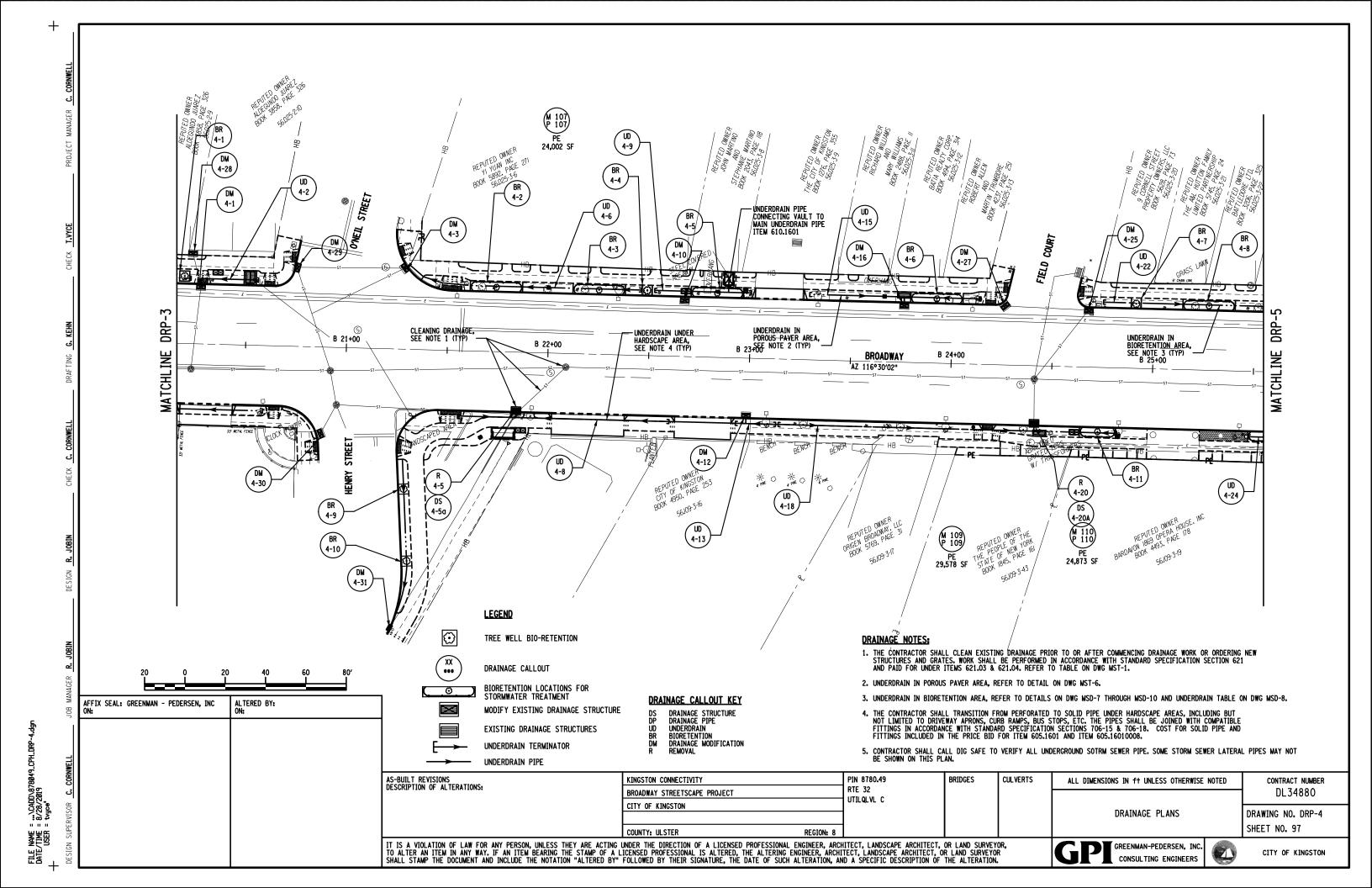


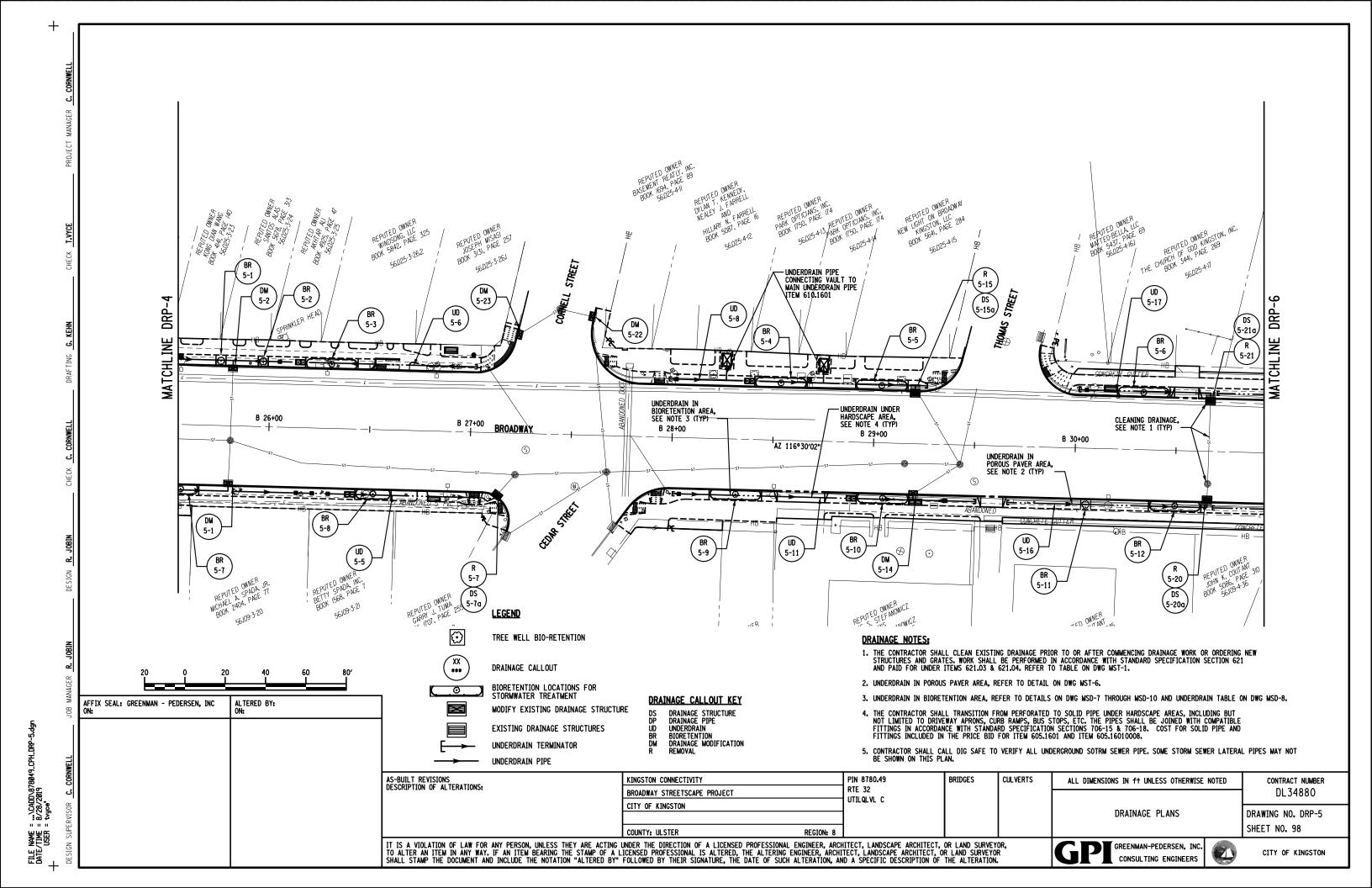


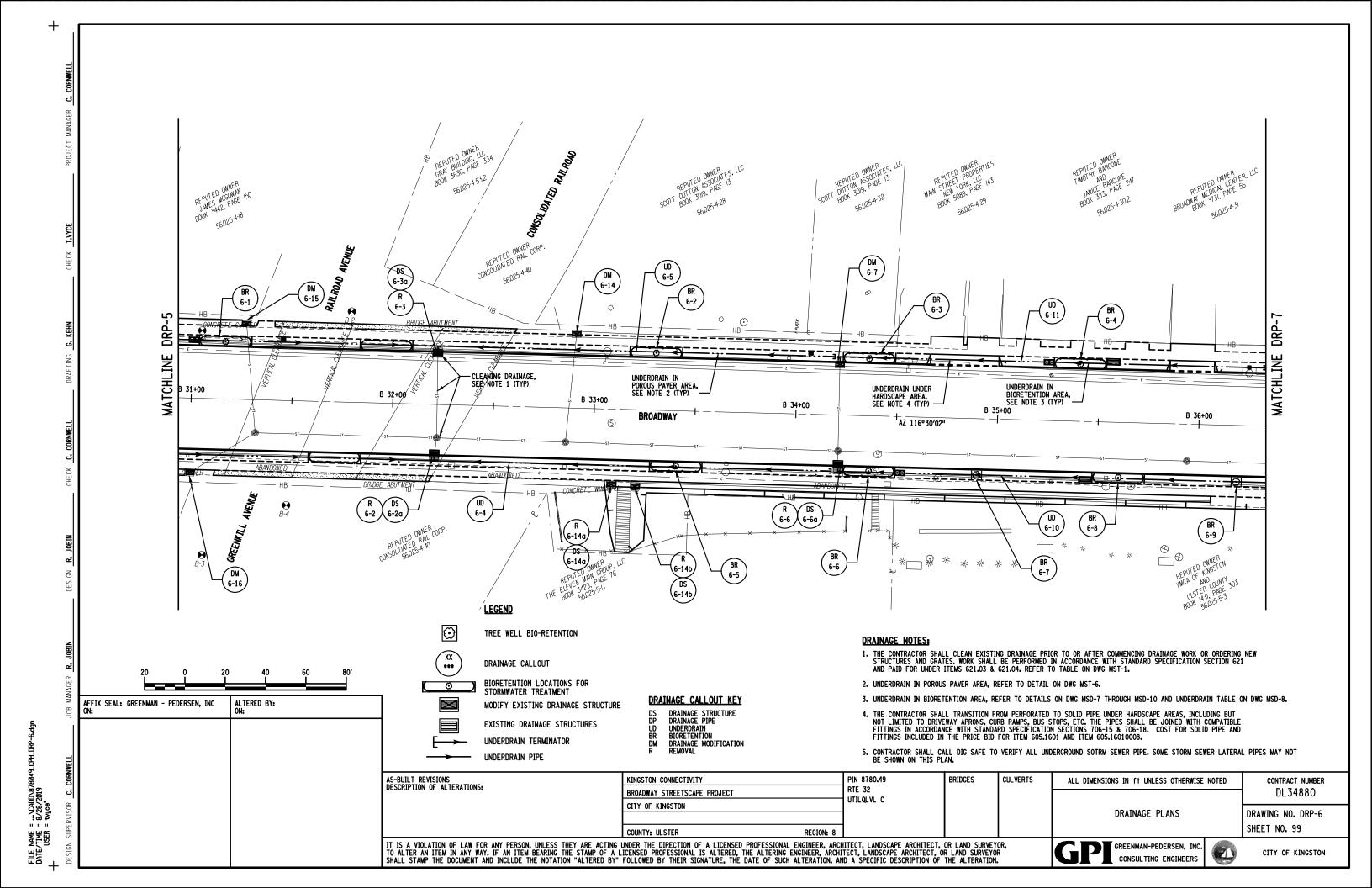


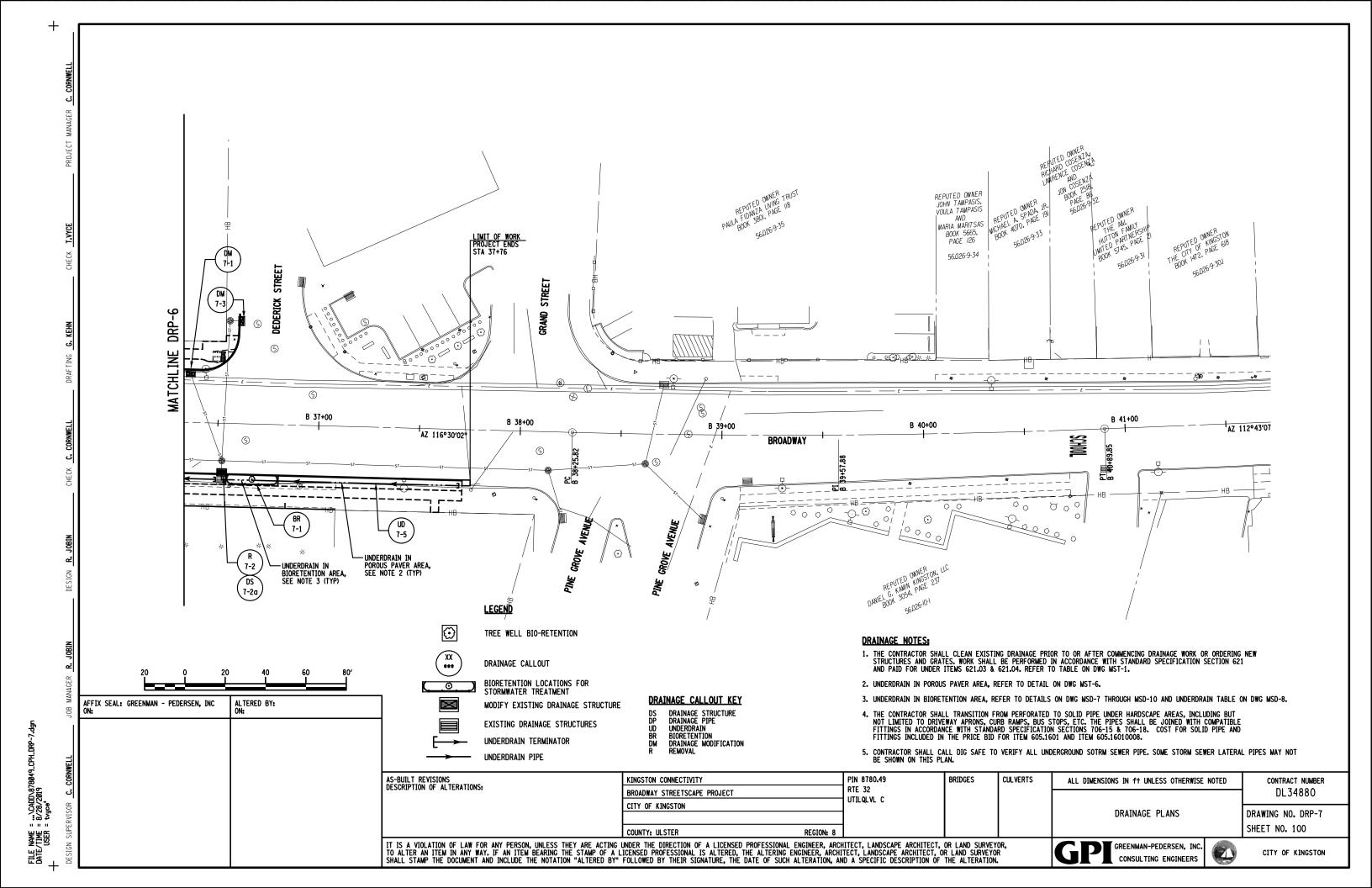












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AS-BUILT AS-BUILT UNIT SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL UNIT SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL ITEM NUMBER ITEM DESCRIPTION ITEM NUMBER ITEM DESCRIPTION OUANTIT DISPOSAL OF BULDINGS BH-578 UNCOATED BAR REINFORCEMENT FOR CONCRETE STURCTURES 202.010001 556.0201 DISPOSAL OF BULDINGS BH-582 SURFACE TREATMENT FOR PAVEMENTS TYPE 2, TRAFFIC GRADE 263 263 202.010002 601.01000018 SY 202.010003 DISPOSAL OF BULDINGS BH-618 LS 603.77 41 202.010004 LS POLYVINYL CHLORIDE (PVC) SEWER PIPE AND FITTINGS, 4' 468 468 DISPOSAL OF BULDINGS BH-640 603.98040007 604.070801 202.010005 DISPOSAL OF BULDINGS BH-642 LS ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES TOP REBUILD 30 30 LS 202.010006 604.070802 ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES TOP REBUILD AND WALL REPAIR 20 20 DISPOSAL OF BULDINGS BH-666 54 LS RECTANGULAR DRAINAGE STRUCTURE TYPE E FOR #11 WELDED FRAME 54 202 010007 DISPOSAL OF BUILDINGS BH-670 604 300611 54 54 LS 604.301411 RECTANGULAR DRAINAGE STRUCTURE TYPE N FOR #11 WELDED FRAME 202.010008 DISPOSAL OF BULDINGS BH-672 202.19 REMOVAL OF SUBSTRUCTURES CY 25 25 605.1001 UNDERDRAIN FILTER, TYPE 2 CY 183 183 203.02 UNCLASSIFIED EXCAVATION AND DISPOSAL CY 6.989 6,989 605.1601 PERFORATED POLYVINYL CHLORIDE UNDERDRAIN PIPE, 4 INCHDIAMETER 6.043 6.043 203.03 EMBANKMENT IN PLACE CY 23 23 605.16010008 NON-PERFORATED PVC PIPE, 4" DAIMETER LF 539 203.07 SELECT GRANULAR FILL CY 216 216 608.0101 CONCRETE SIDEWALKS AND DRIVEWAYS 721 721 203.21 SELECT STRUCTURAL FILL CY 37 37 608.020102 HOT MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS, AND BICYCLE PATHS TON 67 67 LIGHTWEIGHT CONCRETE FILL (TYPE A) 21 21 608.12010011 PRECAST CONCRETE PAVERS (MORTAR SET BED) 603 206.0201 TRENCH AND CULVERT EXCAVATION CY 608.13000039 PRECAST PERMEABLE PAVERS FOR SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS SY 1,947 206.03 CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION LF 7,600 CAST IRON EMBEDDED DETECTABLE WARNING UNITS 51 51 608.21000003 TEST PIT EXCAVATION EΑ 50 BLUESTONE SIDEWALKS (STONE DUST SETTING BED) 963 963 206.05 608.40010008 NON-SECURITY BOLLARD-FIXED IN GROUND, WITHOUT LIGHTING, WIDTH OVER 3 INCHES TO 5 INCHES, I 207.21 **GEOTEXTILE SEPARATION** SY 5,461 608.72126108 192 192 5.461 208.01030022 BIORETENTION AND DRY SWALE SOIL CY 536 609.0201 STONE CURB, GRANITE, (TYPE A) 6,433 6.433 1,072 209.1703 DRAINAGE STRUCTURE INLET PROTECTION, PREFABRICATED-TEMPORARY LF 1,072 609.05 CAST-IN-PLACE CONCRETE CURB & GUTTER (AS DETAILED) 1 F 1.798 1,798 209.1803 ROLLED EROSION CONTROL PRODUCT, CLASS I TYPE C, SHORT TERM SY 221 221 609.15 RESETTING EXISTING CURB 363 363 SUBBASE COURSE (MODIFIED) CY 1,826 1,826 609.15100011 STEEL EDGING LF 1,203 1,203 304.11000008 TRUE & LEVELING F9, SUPERPAVE HMA, 80 SERIES COMPACTION TON 587 610.1101 MULCH FOR PLANTING TYPE A, B & D - WOOD CHIPS AND SHREDDED BARK CY 402.017903 402.127203 12.5 F2 TOP COURSE HMA, 80 SERIES COMPACTION TON 1.761 1,761 610.1403 TOPSOIL - LAWNS CY 50 50 402.258903 25 F9 BINDER COURSE HMA, 80 SERIES COMPACTION TON 610.1602 TURF ESTABLISHMENT - LAWNS 444 444 402.378903 37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION TON 610.19 73 407.0103 GAL 1.990 1.990 611.0171 PLANTING - MAJOR DECIDUOUS TREE - 3 INCH CALIPER BALL & BURLAP, FIELD POTTED OR FIELD BOXED 76 TACK COAT (STRAIGHT) 1.309 PLANTING - DECIDUOUS SHRUBS - AS SPECIFIED CONTAINER OR BOX GROWN - 2FT 37 420.98010104 POROUS PAVING SURFACE, TYPE NN 1.309 611.0442 490 10 SY 18.314 18.314 611 0452 PLANTING - DECIDUOUS SHRUBS - AS SPECIFIED CONTAINER OR BOX GROWN - 3FT FΑ 36 PRODUCTION COLD MILLING BITUMINOUS CONCRETE 490.30 MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE SY 1.559 1.559 611.0461 PLANTING - DECIDUOUS SHRUBS - AS SPECIFIED CONTAINER OR BOX GROWN - 4FT 110 503.1010 PCC FOUNDATION FOR PAVEMENT, CLASS C CY 101 101 611.0462 PLANTING - DECIDUOUS SHRUBS - AS SPECIFIED CONTAINER OR BOX GROWN - 4FT EΑ 52 52 552.17 SHIELDS AND SHORING 1,456 1,456 611.0751 PLANTING - HERBACEOUS PLANTS - NUMBER 1 CONTAINER - CONTAINER GROWN 1,470 1,470 CONCRETE FOR STRUCTURES, CLASS HP CY 23 611.17 PORTABLE DRIP IRRIGATION SYSTEM 555.09 AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 BRIDGES **CULVERTS** KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON ENGINEERING SHARE 1 - ROADWAY AND SIDEWALK IMPROVEMENTS (NYSDOT AND NYSDEC)
ENGINEERING SHARE 2 - GREEN INFRASTRUCTURE (NYSEFC)
ENGINEERING SHARE 3 - BROADWAY TRAFFIC SIGNALIZATION WITHIN PROJECT LIMITS
ENGINEERING SHARE 4 - BROADWAY TRAFFIC SIGNALIZATION (NYSERDA FUNDING) ESTIMATE QUANTITIES DRAWING NO. EOQ-1 SHEET NO. 101 COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. GREENMAN-PEDERSEN, INC CITY OF KINGSTON

ESTIMATE OF QUANTITIES

CONSULTING ENGINEERS

ESTIMATE OF QUANTITIES

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ITEM NUMBER ITEM DESCRIPTION		UNIT	SHARE 1	SHARE 2 SHARE 3	SHARE 4	TOTAL	AS-BUILT QUANTITY	ITEM NUMBE	R	ITEM DES	CRIPTION			UNIT	SHARE 1	SHARE 2	SHARE 3 SHARE	E 4 TOTAL QUANTI
611.18	REMOVAL OF PORTABLE DRIP IRRIGATION SYSTEM	EA		76		76		645.5102	GROUND MOUNTED S	IGN PANELS WITHOUT Z BARS	- HIGH VIS SHEET	ΠNG		SF	138			138
611.19	POST-PLANTING CARE	EA		76		76		645.5202	GROUND MOUNTED SIGN PANELS WITH Z BARS - HIGH VIS SHEETING					SF	81			81
614.060304	TREE REMOVAL OVER 12 INCHES TO 18 INCHES DIAMETER BREAST HEIGHT - STUMPS GRUBBED	EA		72		72		645.81	TYPE A SIGN POSTS					EA	162			162
614.0701	PRE-EXISTING STUMP REMOVAL UP TO 24 INCH DIAMETER AT 6 INCHES ABOVE GRADE	EA		14		14		645.81020003	RETROREFLECTIVE SIG	N POST STRIP				EA	20			20
615.0101011	LITTER (TRASH) RECEPTACLE TYPE 1	EA	23			23		647.31	RELOCATE SIGN PANE	L, SIGN PANEL ASSEMBLY SIZE	(UNDER 30 SQU	JARE FEET)		EA	31			31
615.0101021	LITTER (RECYCLE) RECEPTACLE TYPE 2	EA	23			23		647.41	REMOVE AND STORE S	SIGN PANEL, SIGN PANEL ASSE	MBLY SIZE I(UND	ER 30 SQUARE FEET)	EA	18			18
615.2702001	BICYCLE RACK	EA	28			28		647.51	REMOVE AND DISPOS	E SIGN PANEL, SIGN PANEL AS	SEMBLY SIZE I(UN	NDER 30 SQUARE FE	ET)	EA	25			25
615.08010005	BENCH TYPE 01	EA	22			22		647.61	REMOVE AND DISPOS	E GROUND MOUNTED TYPE A S	IGN SUPPORT(S) FDNS AND ANY AT	TACHED SIGNS -	EA	2			2
619.01	BASIC WORK ZONE TRAFFIC CONTROL	LS	1			1		655.16000011	REMOVE AND DISPOS	E OF FRAMES AND GRATES				EA	18			18
619.04	TYPE III CONSTRUCTION BARRICADES	EA	45			45		655.0704	CAST FRAME F1, UNM	OUNTABLE CURB BOX AND RET	TCULINE GRATE	G1		EA	1			1
619.0701	TEMPORARY BUSINESS SIGNS	SF	200			200		655.0705	CAST FRAME F2, UNM	OUNTABLE CURB BOX AND RET	ICULINE GRATE	G2		EA	2			2
619.080201	REMOVE PAVEMENT MARKING LETTERS OR SYMBOLS, TRAFFIC PAINT	EA	2			2		655.1103	WELDED FRAME AND	RETICULINE GRATE 3				EA	20			22
619.100101	INTERIM PAVEMENT MARKINGS, STRIPES (TRAFFIC PAINT)	LF	10,000			10,000		655.1106	WELDED FRAME AND	RETICULINE GRATE 6				EA	4			5
619.100201	INTERIM PAVEMENT MARKINGS, SYMBOLS (TRAFFIC PAINT)	EA	40			40		655.25010005	FURNISH AND/OR INS	TALL INLET ASSEMBLY, REINST	ALL EXISTING			EA	15			15
619.110511	(PVMS) STANDARD SIZE - FULL MATRIX (LED) NO OPTIONAL EQUIPMENT SPECIFIED, NO CELLULAR COMI	EA	5			5		655.25020005	FURNISH AND/OR INS	TALL INLET ASSEMBLY, FURNIS	H NEW			EA	34			34
620.10	GALVANIZED GABIONS	CY		134		134		655.25030005	FURNISH AND/OR INS	TALL INLET ASSEMBLY, DRIVEW	/AYS			EA	4			4
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF	5,397			5,397		660.65000101	ALTERING UTILITY MANHOLES					EA	53			53
621.04	CLEANING DRAINAGE STRUCTURES	EA	96			96		660.65000201	ALTERING UTILITY MAI	NHOLES AND VAULTS BH-578				EA	1			1
623.10	CRUSHED STONE (IN PLACE MEASURE) MODIFIED	CY	511			511		660.65000301	ALTERING UTILITY MAI	NHOLES AND VAULTS BH-582				EA	1			1
624.01	CONVENTIONALLY FORMED OR MACHINE FORMED GUTTERS	SF	1,662			1,662		660.65000601	ALTERING UTILITY MANHOLES AND VAULTS BH-670					EA	1			1
625.01	SURVEY OPERATIONS	LS	1			1		663.1301	HYDRANT					EA	8			8
627.50140008	CUTTING PAVEMENT	LF	7,256			7,256		663.33	ADJUST EXISTING VAL	VE BOX ELEVATION				EA	92			92
633.14	REMOVAL AND REPAIR OF DETERIORATED HMA PAVEMENT	SY	312			312		665.20040004	ALUMINUM ACCESS DOORS					EA	5			5
634.06010007	RELOCATE PARKING METERS	EA	1			1		670.0106	FOUNDATION FOR LIGHT STANDARDS, 6 FEET LONG					EA	45			45
634.06030007	REMOVE AND STORE PARKING METERS	EA	63			63		670.15100108	FURNISH AND INSTALL DECORATIVE STREET LIGHT POLE AND LUMINAIRE ASSEMBLIES TYPE-1					EA	45			45
634.10010039	UNIT STAIR ASSEMBLY SYSTEM	EA	5			5		670.2602	RIGID PLASTIC CONDU	JIT, 2"				LF	6,100			6,100
634.91030004	ABANDON EXISTING SIDEWALK VAULT	EA	1			1		670.3001	PULL BOXES LESS THAI	N 5 CUBIC FEET, INSIDE VOLUM	E(LIGHTING)			EA	40			40
634.99010017	BUILDING CONDITION SURVEY (8 BUILDINGS)	LS	1			1		670.3010	PULL BOXES 7.6 CUBIC	FEET TO 10 CUBIC FEET, INSIDE	VOLUME(LIGHT	īNG)		EA	9			9
635.0103	CLEANING AND PREPERATION OF PAVEMENT SURFACES - LINES	LF	23,000			23,000		670.53070104	POWER POINT DISCON	NNECT ASSEMBLY - LOCATION	Α			EA	1			1
635.0303	CLEANING AND PREPERATION OF PAVEMENT SURFACES - SYMBOLS	EA	23			23		670.53070204	POWER POINT DISCONNECT ASSEMBLY - LOCATION B				EA	1			1	
645.48010001	HIGH PRESSURE LAMINATE GRAPHIC SIGN	EA	2			2		670.53070304	POWER POINT DISCONNECT ASSEMBLY - LOCATION C					EA	1			1
645.48020001 STEEL EXHIBIT PEDESTAL		EA	2			2		670.7002	SINGLE CONDUCTOR C	CABLE #2 AWG				LF	800			800
645.5101	GROUND MOUNTED SIGN PANELS WITHOUT Z BARS	SF	141			141		670.7004	SINGLE CONDUCTOR CABLE #6 AWG					LF	33,800			33800
AS-BUILT REVISIONS DESCRIPTION OF ALTERATION				TONS:			KINGSTON CONNECTIVITY			PIN 8780.49 BRIDGES CULVERTS ALL I				DIMENSIONS IN ft UNLESS OTHERWISE NO			NOTED	CONTRACT NUMBER
SEESTE TOO OF RETENENTS						Y OF KIN	TREETSCAPE GSTON	PROJECT					F.6	T11.1.4.T1	CUANT	ITICC	20	DL34880
					COUNTY III CTED								ES	IIAMII	E QUANT	111F2		AWING NO. EOQ-2 EET NO. 102
	IT IS A VIOLATION OF LAW TO ALTER AN ITEM IN ANY	FOR ANY	PERSON, UN	NLESS THEY ARE AC	COUNTY: ULSTER REGION: 8 ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCH OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCH ED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, A					ITECT, LANDSCAPE ARCHITECT,	OR LAND SURVE	YOR,	GDI			RSEN, INC.	A STATE OF THE PARTY OF THE PAR	CITY OF KINGSTON
	SHALL STAMP THE DOCUMEN	T AND INC	CLUDE THE	NOTATION "ALTERED	BY" FOLI	OWED BY	THEIR SIGN	TURE, THE DATE O	F SUCH ALTERATION, AND	D A SPECIFIC DESCRIPTION OF	THE ALTERATIO	Ň.	JII	CONSL	ILTING EN	IGINEERS		511. 51 KM0510N

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ENGINEERING SHARE 1 - ROADWAY AND SIDEWALK IMPROVEMENTS (NYSDOT AND NYSDEC)
ENGINEERING SHARE 2 - GREEN INFRASTRUCTURE (NYSEFC)
ENGINEERING SHARE 3 - BROADWAY TRAFFIC SIGNALIZATION WITHIN PROJECT LIMITS
ENGINEERING SHARE 4 - BROADWAY TRAFFIC SIGNALIZATION (NYSERDA FUNDING)

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

PIN 8780.49 RTE 32 KINGSTON CONNECTIVITY BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON COUNTY: ULSTER

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

ESTIMATE QUANTITIES

CONTRACT NUMBER DL34880

DRAWING NO. EOQ-3 SHEET NO. 103

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

GPERMAN-PEDERSEN, INC. CONSULTING ENGINEERS

CITY OF KINGSTON

ESTIMATE OF QUANTITIES											
ITEM NUMBER	ITEM DESCRIPTION	UNIT	SHARE 1	SHARE 2	SHARE 3	SHARE 4	TOTAL	AS-BUILT			
670.7006	SINGLE CONDUCTOR CABLE #10 AWG	LF	4,600				4600				
670.7501	GROUND WIRE #6 AWG	LF	6,800				6800				
670.77000105	MODIFY LIGHTING INSTALLATION, TYPE 1 (REMOVE LIGHT POLE)	EA	44				44				
670.77000205	MODIFY LIGHTING INSTALLATION, TYPE 2 (CIRCUIT TESTING)	EA	5				5				
670.77000305	MODIFY LIGHTING INSTALLATION, TYPE 3 (BILLBOARD CONNECTION)	EA	2				2				
680.06010006	DECORATIVE WRAP FOR TRAFFIC SIGNAL CABINETS	SF	143				143				
680.5001	POLE EXCAVATION AND CONCRETE FOUNDATION	CY			26		26				
680.5002	CONCRETE BASE FOR CONTROLLER CABINET	EA			3		3				
680.510501	PULL BOX-RECTANGULAR, 26 X 18 INCH, REINFORCED CONCRETE	EA			12		12				
680.520103	CONDUIT, METAL STEEL, ZINC COATED, 1"	LF			350		350				
680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2"	LF			220		220				
680.520108	CONDUIT, METAL STEEL, ZINC COATED, 3"	LF			960		960				
680.58010009	MICROWAVE VEHICLE DETECTOR WITH 12 VOLT AC POWER SUPPLY DETECTOR CARD	EA				1	1				
680.58803008	WIRELESS VEHICLE DETECTION SYSTEM INTERSECTION SENSOR	EA			67		67				
680.58804008	WIRELESS VEHICLE DETECTION SYSTEM CONTACT CLOSURE INTERFACE (APCC)	EA			3		3				
680.58805008	WIRELESS VEHICLE DETECTION SYSTEM EXTENSION CONTACT CLOSURE CARD	EA			3		3				
680.58806008	WIRELESS VEHICLE DETECTION SYSTEM ISOLATOR MODULE (ISO)	EA			3		3				
680.58807008	WIRELESS VEHICLE DETECTION SYSTEM SERIAL PORT PROTOCOL DIGITAL RADIO (SPP)	EA			5		5				
680.62184609	FLUTED TRAFFIC SIGNAL POLE, MAST ARM, 18 FEET MOUNTING HEIGHT,46 FEET MAST ARM LENGTH	EA			1		1				
680.62184809	FLUTED TRAFFIC SIGNAL POLE, MAST ARM, 18 FEET MOUNTING HEIGHT,48 FEET MAST ARM LENGTH	EA			1		1				
680.62185009	FLUTED TRAFFIC SIGNAL POLE, MAST ARM, 18 FEET MOUNTING HEIGHT,50 FEET MAST ARM LENGTH	EA			3		3				
680.65080409	FLUTED TRAFFIC SIGNAL POLE - TOP MOUNT - PAINTED, 8 FEET LENGTH	EA			7		7				
680.65100409	FLUTED TRAFFIC SIGNAL POLE - TOP MOUNT - PAINTED, 10 FEET LENGTH	EA			8		8				
680.700606	RISER ASSEMBLY, 2 INCH DIAMETER	EA			3		3				
680.730514	SIGNAL CABLE 5 CONDUCTORS, 14 AWG	LF			6450		6450				
680.731014	SIGNAL CABLE 10 CONDUCTORS, 14 AWG	LF			1700	800	2500				
680.78100108	MODIFY AND REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 1	LS				1	1				
680.78100208	MODIFY AND REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 2	LS				1	1				
680.78100308	MODIFY AND REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 3	LS				1	1				
680.79010008	REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 1	LS			1		1				
680.79020008	REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 2	LS			1		1				
680.79030008	REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 3	LS			1		1				
680.80149305	NEMA TS 2 SOLID STATE MENU DRIVEN ACTUATED TRAFFIC SIGNAL CONTROLLER AND CABINET	EA			3		3				

ITEM NUMBER	ITEM DESCRIPTION	UNIT	SHARE 1	SHARE 2	SHARE 3	SHARE 4	TOTAL	AS-BUIL
680.80324708	MICROCOMPUTER CABINET BASE (ALUMINUM)	EA			3		3	
680.810101	TRAFFIC SIGNAL MODULE - 12 INCH, RED BALL, LED	EA			22		22	
680.810102	TRAFFIC SIGNAL MODULE - 12 INCH, RED ARROW, LED	EA			3		3	
680.810103	TRAFFIC SIGNAL MODULE - 12 INCH, YELLOW BALL, LED	EA			22		22	
680.810104	TRAFFIC SIGNAL MODULE - 12 INCH, YELLOW ARROW, LED	EA			3		3	
680.810105	TRAFFIC SIGNAL MODULE - 12 INCH, GREEN BALL, LED	EA			22		22	
680.810106	TRAFFIC SIGNAL MODULE - 12 INCH GREEN ARROW, LED	EA			3		3	
680.810301	TRAFFIC SIGNAL MODULE - 8 INCH, RED BICYCLE, LED	EA			10		10	
680.810303	TRAFFIC SIGNAL MODULE - 8 INCH, YELLOW BICYCLE, LED	EA			10		10	
680.810305	TRAFFIC SIGNAL MODULE - 8 INCH, GREEN BICYCLE, LED	EA			10		10	
680.810601	TRAFFIC SIGNAL SECTION - POLYCARBONATE, TYPE I, 12 INCH	EA			75		75	
680.810701	TRAFFIC SIGNAL SECTION - POLYCARBONATE, TYPE I, 8 INCH	EA			30		30	
680.8111	TRAFFIC SIGNAL BRACKET ASSEMBLY - 1 WAY	EA			25		25	
680.8112	TRAFFIC SIGNAL BRACKET ASSEMBLY - 2 WAY	EA			5		5	
680.813105	PEDESTRIAN SIGNAL MODULE - 12 INCH BI-MODAL, HAND/MAN SYMBOLS LED	EA			24	22	46	
680.813106	PEDESTRIAN SIGNAL SECTION - POLYCARBONATE, TYPE I, 12 INCH	EA			48	44	92	
680.8133001	AUDIBLE PEDESTRIAN SIGNAL	EA			24	22	46	
680.8141	PEDESTRIAN SIGNAL BRACKET MOUNT ASSEMBLY	EA			15	10	25	
680.8142	PEDESTRIAN SIGNAL POST TOP MOUNT ASSEMBLY	EA			9	12	21	
680.8150001	PEDESTRIAN COUNT-DOWN TIMER MODULE	EA			24	22	46	
680.81990003	TRAFFIC SIGNAL BACKPLATES WITH YELLOW REFLECTIVE TAPE	EA			27		27	
680.8207	OVERHEAD SIGN ASSEMBLY, TYPE G	EA			8		8	
680.8225431	RECTANGULAR RAPID FLASHING BEACON (RRFB) ASSEMBLY - FOUR BEACONS, SOLAR POWERED	EA			6		6	
680.90920008	ELECTRIC METER SOCKET, 200 AMP, SINGLE PHASE, 240/120 VOLT W/BYPASS SWITCH FOR SIGNAL INSTA	EA			6		6	
680.93200101	NEMA TS2 TYPE A2 CONTROLLER - BUS PRIORITY CAPABILITY	EA				3	3	
680.93500101	SPREAD SPECTRUM WIRELESS TRAFFIC CONTROL	EA				6	6	
680.93500201	DIRECTIONAL YAGI ANTENNA FOR SPREAD SPECTRUM WIRELESS TRAFFIC CONTROL	EA				6	6	
680.93500301	REMOTE ANTENNA CABLE FOR SPREAD SPECTRUM WIRELESS TRAFFIC CONTROL	LF				300	300	
680.94000008	TRAFFIC SIGNAL SERVICE ENTRANCE	EA			3		3	
680.94000015	RAINTIGHT DISCONNECT BOX	EA			3		3	
680.95010615	SERVICE CABLE 1 CONDUCTOR, NO. 06 AWG	LF			800		800	
685.11	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF	27,933				27933	
685.12	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS	LF	13,993				13993	

ESTIMATE OF QUANTITIES

FILE NAME = ...\CADD\878849_CPH_E00-4.dgn
DATE/TIME = 8/28/2019
+ USER = tygos
DESIGN SUPERVISOR C. CORNWELL

ITEM NUMBER

685.14

697.03

698.04

698.05

699.040001

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

ESTIMATE OF QUANTITIES

ITEM DESCRIPTION

WHITE EPOXY REFLECTORIZED PAVEMENT LETTERS - 20 MILS

WHITE EPOXY REFLECTORIZED PAVEMENT SYMBOLS - 20 MILS

ENGINEERING SHARE 1 - ROADWAY AND SIDEWALK IMPROVEMENTS (NYSDOT AND NYSDEC)
ENGINEERING SHARE 2 - GREEN INFRASTRUCTURE (NYSEFC)
ENGINEERING SHARE 3 - BROADWAY TRAFFIC SIGNALIZATION WITHIN PROJECT LIMITS
ENGINEERING SHARE 4 - BROADWAY TRAFFIC SIGNALIZATION (NYSERDA FUNDING)

ASPHALT PRICE ADJUSTMENT (LUMP SUM) [CALCULATED IN ESTIMATOR]

FIELD CHANGE PAYMENT (FCP)

FUEL PRICE ADJUSTMENT

MOBILIZATION 4%

SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL

QUANTITY

290,000

2,430

1,694

1

UNIT

EA

DC

DC

DC

LS

126

290,000

2,430

1,694

1

RTE 32 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON

PIN 8780.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

ESTIMATE QUANTITIES

GPERENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

DL34880 DRAWING NO. EOQ-4 SHEET NO. 104

CONTRACT NUMBER

CITY OF KINGSTON

CITY OF KINGSTON			i
COUNTY: ULSTER	REGION: 8		
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KINGSTON CONNECTIVITY

			1
	COUNTY: ULSTER REGION: 8		
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