

DATE



PIN 8780.49
KINGSTON CONNECTIVITY

BROADWAY STREETSCAPE PROJECT

CITY OF KINGSTON

NOVEMBER 2019

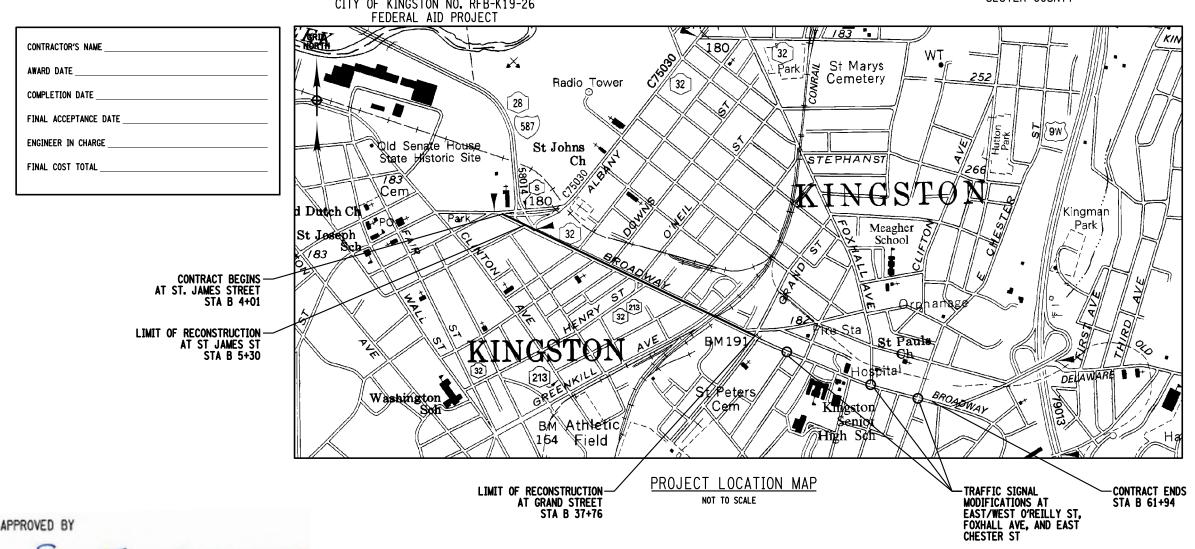
THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE DEPARTMENT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN FEFECT, ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

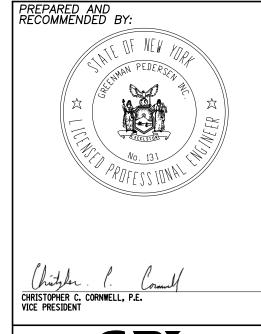
ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN THE CONTRACT PROJECT "PROPOSAL."

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYSDOT POLICIES AND GUIDELINES AND THE FINAL DESIGN REPORT APPROVED ON 12/14/2018

DL NO. 034880
GIGP NO. 1378
CITY OF KINGSTON NO. RFB-K19-26

ULSTER COUNTY





GREENMAN-PEDERSEN, INC.
CONSULTING ENGINEERS
80 WOLF ROAD, SUITE 300
ALBANY, NY 12205

	KINGSTON CONNE	CTIVITY	
	BROADWAY STREE	ETSCAPE PROJECT	
S	CITY OF KINGST	ON	
	ULSTER COUNTY		
	FED. ROAD REG. NO.	STATE	SHEET NO.
	8	N.Y.	1
	CAPITAL PROJECT IDENTIFICATION NO. 878	0.49	

INDEX ON SHEET NO. 2

ESIGN SUPERVISOR C. CORNWELL

STEVEN T. NOBLE MAYOR, CITY OF KINGSTON

E = ...\CADD\878Ø49_CPH_IND-1.4 E = 11/21/2019 ? = tvuce*

TB

TC

VCP

BOTTOM OF CURB BOTTOM OF OPENING BO CAP CORRUGATED ALUMINUM PIPE СВ CATCH BASIN CAST IRON PIPE € STRM CENTERLINE OF STREAM CMP CORRUGATED METAL PIPE CP CONCRETE PIPE CSP CORRUGATED STEEL PIPE CULV CULVERT DIA DIAMETER DMH DRAINAGE MANHOLE DRAINAGE STRUCTURE PIPE D'XING DITCH CROSSING EHW EXTREME HIGH WATER EL ELEVATION ELEV ELEVATION ELW EXTREME LOW WATER ES END SECTION HW HEADWALL INV INVERT MH MANHOLE MEAN HIGH WATER MHW ORDINARY HIGH WATER OHW ORDINARY LOW WATER REINFORCED CONCRETE PIPE RCP SICPP SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE

TOP OF BANK (STREAM)

VITRIFIED CLAY PIPE

TOP OF CURB TOP OF GRATE

ALIGNMENT

SUPERELEVATION RATE (CROSS SLOPE)

CENTER CORRECTION OF VERTICAL CURVE

DESCRIPTION

AHFAD

BACK BASELINE

AZIMUTH

BEARING

CENTERLIN

EQUALITY

EXTERNAL

MAIN LINE

RADIUS

STATION

CURVE TO SPIRAL

LENGTH OF SPIRAL

POINT OF CURVATURE

POINT ON LINE

POINT OF TANGENT

SPIRAL TO CURVE

TANGENT LENGTH

VERTICAL CURVE

DESCRIPTION

TANGENT TO SPIRAL

SPIRAL TO TANGENT

POINT OF INTERSECTION

PASSING SIGHT DISTANCE

POINT OF VERTICAL CURVE

STOPPING SIGHT DISTANCE

THEORETICAL GRADE LINE

BOTTOM OF BANK (STREAM)

POINT OF VERTICAL TANGENT

POINT OF VERTICAL INTERSECTION

TOPOGRAPHY (DRAINAGE)

HORIZONTAL CONTROL LINE

HEADLIGHT SIGHT DISTANCE

LENGTH OF CIRCULAR CURVE

LENGTH OF VERTICAL CURVE

ABBR.

ΔН

ΑZ

BK

BRG

CS

EQ

EXT

HCL

HSD

LS

PC

ΡI

P0L

PSD

PT

PVC

PVI

PVT

R

SSD

ST

STA

TGL

TS

ABBR.

BB

VC

LVC

STANDARD SHEETS STANDARD SHEETS
203-01, 209-03, 402-01, 502-01, 502-02, 502-03, 502-04, 502-05, 502-06, 502-07, 502-08, 502-09, 502-10, 502-11, 502-12, 502-13, 502-14, 502-15, 603-01, 604-02, 605-01, 608-01, 608-02, 608-03, 608-04, 608-05, 608-06, 609-01, 609-02, 609-03, 611-01, 619-02, 619-10, 619-11, 619-12, 619-20, 619-21, 619-50, 619-51, 619-60, 619-61, 625-01, 645-01, 645-02, 645-03, 645-05, 645-06, 645-07, 645-03, 645-10, 645-11, 645-12, 645-14, 646-12, 646-13, 646-16, 649-01, 655-01, 655-02, 655-03, 655-04, 655-05, 655-06, 655-07, 655-08, 663-07, 663-02, 663-03, 663-04, 663-05, 663-05, 663-07, 664-01, 670-01, 670-02, 670-03, 680-01, 680-02, 664-01, 670-01, 670-02, 670-03, 680-01, 680-02, 680-03, 680-04, 680-05, 680-06, 680-07, 680-08, 680-11, 680-12, 680-13, 680-14, 680-15, 680-16, 680-17, 685-01

TOPOGRAPHY (MISCELLANEOUS)

ABBR.

ABUT

AOBE

ВМ

DESCRIPTION

AS ORDERED BY ENGINEER

ABUTMENT

BENCH MARK

CC CENTER TO CENTER

CMU CONCRETE MASONRY UNIT

DM DIRECT MEASUREMENT

EP EDGE OF PAVEMENT

ES EDGE OF SHOULDER

FEE WO/A FEE ACQUISITION WITHOUT ACCESS

IRON PIN OR IRON PIPE

NAIL AND WASHER

PERMANENT EASEMENT

ORIGINAL GROUND

FEE | FEE ACQUISITION

FENCE LINE

FP FENCE POST

FD FOUNDATION

GRAVEL

HIGHWAY

MAILBOX

O/H OVERHEAD

P PARCEL

PAV'T PAVEMENT

PED POLE PEDESTRIAN POLE

PORCH

ROW RIGHT OF WAY

SH STATE HIGHWAY

RETAINING WALL

TE TEMPORARY EASEMENT

TO TEMPORARY OCCUPANCY

RR RAILROAD

RTE ROUTE

SHLDR SHOULDER

ST STREET

STK STAKE

STY STORY

SW SIDEWALK

U/G UNDERGROUND WW WING WALL

SPK SPIKE

P PROPERTY LINE

MONUMENT

GAR GARAGE

HOUSE

FL

GR

HWY

MON

N&W

OG |

PE

POR I

RW

IP I

ASPH ASPHALT

BDY BOUNDARY

BLDG BUILDING

CONC CONCRETE

CONST CONSTRUCTION

DWY DRIVEWAY

CR COUNTY ROAD

D DEED DISTANCE

STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
II .	-	INCHES
,	LF	LINEAR FEET
mi	MI	MILES
f†²	SF	SQUARE FEET
YD ²	SY	SQUARE YARD
AC	AC	ACRES
YD3	CY	CUBIC YARD
GAL	GAL	GALLON
lb	LB	POUND
TON	TON	TON

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

BROADWAY STREETSCAPE PROJECT

PIN 8780.49

RTE 32

REGION: 8

BRIDGES

CULVERTS

	X	TO BE USED IF ONE OF THE ABO BE DEFINED AT THE TIME THE E: IS MADE	VE CANNOT KPLORATION
	STANDARD Symbol (Plans)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
	н	-	INCHES
	,	LF	LINEAR FEET
	mi	MI	MILES
	f†²	SF	SQUARE FEET
	YD ²	SY	SQUARE YARD
	AC	AC	ACRES
	YD3	CY	CUBIC YARD
	GAL	GAL	GALLON
	lb	LB	POUND
- 1	TAN	TON	TON

UTILITIES

DESCRIPTION

EMH | ELECTRIC MANHOLE

GSB GAS SERVICE BOX (HOUSE LINE)

GV GAS VALVE (MAIN LINE)

E ELECTRIC

GP GUY POLE

HYD HYDRANT

LP LIGHT POLE LPG | LOW PRESSURE GAS

PP POWER POLE

T TELEPHONE

TEL P | TELEPHONE POLE

W WATER

ABBR.

CP

FH

SA SANITARY SEWER

SMH SANITARY MANHOLE ST STORM SEWER

TCB TRAFFIC CONTROL BOX TELBOX TELEPHONE BOX

TMH | TELEPHONE MANHOLE

WSB WATER SERVICE BOX (HOUSE LINE)

REPLACE ABBREVIATION "AB" WITH:

SUBSURFACE EXPLORATION

1 INCH SAMPLER (RETRACTABLE PLUG)

TO BE DEFINED AT THE TIME OF EXPLORATION

WV WATER VALVE (MAIN LINE)

CONE PENTROMETER

DA 21/4 INCHES CASED DRILL HOLE

DN 4 INCHES CASED DRILL HOLE

HOLLOW FLIGHT AUGER

PT PERCOLATION TEST HOLE

ABBREVIATION "C" IN CATEGORIES: DA, DM, DN, AND FH WITH:

DESCRIPTION

AH HAND AUGER

DM DRILLING MUD

PA POWER AUGER

SP SEISMIC POINT

TP TEST PIT

B BRIDGE

C CUT

F FILL

W WALL

K CULVERT

D DAM

PH PROBE

CTV CABLE TELEVISION

G GAS

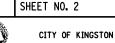
ABBR.

DRAWING INDE)	DRAWING INDEX					
DESCRIPTION	DWG NO	SHEET QUANTITY	SHEET			
TITLE SHEET	CVR	1	3			
DRAWING INDEX	IND	1	2			
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GENERAL NOTES	GNN	1	7			
WZTC NOTES	MPN	2	8			
WZTC PLANS	MPP	2	10			
BASELINE TIES	BLT	3	12			
MAINTENANCE JURISDICTION PLANS	MJP	2	15			
MISCELLANEOUS TABLES	MST	10	17			
MISCELLANEOUS DETAILS - GENERAL	MSD	5	27			
MISCELLANEOUS DETAILS - DRAINAGE	MSD	1	32			
MISCELLANEOUS DETAILS - BIO-SWALE	MSD	4	33			
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ESTIMATE OF QUANTITIES	EOQ	4	103			
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ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

INDEX & ABBREVIATIONS



DRAWING NO. IND-1

CONTRACT NUMBER

ALIGNMENT LANDSCAPE ROADWAY TRAFFIC WORK ZONE STYLE NAME STYLE NAME DESCRIPTION STYLE NAME DESCRIPTION BARRIER, TEMPORARY DESCRIPTION BARRIER, TEMPORARY, W/ WARNING TW7BTWI CONTROL (CENTERLINE) LABL AREA, BRUSH LINE CZ RCZ_P CLEAR ZONE ~~~~~ TWZCD_P CHANNELIZING DEVICE AD_P LAHR AREA, HEDGE ROW -0-GUIDE RAIL, MISCELLANEOUS DETOUR PAVEMENT MARKING REMOVAL OR TWZPMRC_P ,,,,,,,,,, AT P TRANSITION CONTROL I APR AREA, PLANTING BED ___ __ RGB GUIDE RAIL, BOX BEAM **UTILITIES BRIDGE** LAWA AREA, WOODED AREA OUTLINE RGBM GUIDE RAIL, BOX BEAM, MEDIAN ___ ——— ___ LAWE AREA, WATERS EDGE RGC GUIDE RAIL, CABLE STYLE NAME DESCRIPTION RAIL -[0]-UC CONDUIT, UNDERGROUND **BSHT** SHEET PILING LCUT_P CUT LIMIT RGCB GUIDE RAIL, CONCRETE BARRIER -]c[CONDUIT, HANGING RGP_F CONTROL LFILL_F FILL LIMIT 0 GUIDE POST 0 OC UC0 CONDUIT, OVERHEAD GUIDE RAIL, W BEAM **-**⊠-RGW BASELINE LFNC **FENCE** UE ELECTRIC LINE, UNDERGROUND *********** LTRC TREE ROW, CONIFEROUS **RGWM** GUIDE RAIL, W BEAM, MEDIAN CBPR BASELINE, PROJECTION M UEH ELECTRIC LINE, HANGING LTRD TREE ROW, DECIDUOUS PARKING BUMPER DRAINAGE OF UE0 ELECTRIC LINE, OVERHEAD RRC LWH WALL, H PILE RAIL ROAD, CATENARY -ST-DCP CULVERT PIPE UETO ELECTRIC TRANSMISSION, OVERHEAD -3RRRER RAIL ROAD, 3RD RAIL WALL, RETAINING DCP_F CULVERT PIPE (DIR) $\times \times \times \times \times$ UESS **ELECTRIC. SUBSTATIONS** LWS WALL, STONE RRPLS_P RAIL, PHOTO, LARGE SCALE - F0 FIBER OPTIC, UNDERGROUND DDG_P DITCH, GRASS LINED **ROW MAPPING** –]F0[-FIBER OPTIC, HANGING RRPSS RAIL, PHOTO, SMALL SCALE DDP_P DITCH, PAVED INVERT DEED LINE UF00 FIBER OPTIC, OVERHEAD - OF O -RRS RUMBLE STRIP PΕ EASEMENT, EXISTING HG GAS, UNDERGROUND DDS_P DITCH, STONE LINED RRSLS_P RAIL, SURVEY, LARGE SCALE MEP_P EASEMENT, PERMANENT -]G[GAS, HANGING DFL_F FLOW LINE RRSSS EASEMENT, PERMANENT, APPROX. RAIL, SURVEY, SMALL SCALE APE UGO GAS, OVERHEAD OGDSSD SLOTTED DRAIN MET_P EASEMENT, TEMPORARY SIGNS · IC -UIC INFORM CABLE, UNDERGROUND DUD_P UNDERDRAIN - ATE -META_F EASEMENT. TEMPORARY, APPROX. **SBLB BILLBOARDS** -]*IC*[-INFORM CABLE, HANGING **ENVIRONMENTAL** MULTIPLE POST FEE ACQUISITION, W/ ACCESS FEE U0 OIL LINE, UNDERGROUND S **EBLHS** BALE, STRAW SS0 STRUCTURE, OVERHEAD AFEE MFA_P FEE ACQUISITION, APPROXIMATE ====0]0[UOH OIL LINE, HANGING CURTAIN, TURBIDITY MFS_P FEE ACQUISITION, SHAPE SSOC STRUCTURE, OVHD, CANTILEVER POLE, BRACE, PUSH BRACE 000000 EDMC DAM, COFFER MEWOA FEE ACQUISITION, W/O ACCESS **STRIPING** -FEE W/OA **>**------LIPGW POLE, GUY WIRE EDMEC_P DAM, EARTHEN CHECK MHA HISTORICAL, ACQUISITION STB* BROKEN LINE USA SANITARY SEWER, UNDERGROUND - SA MHB STDB* DOUBLE BROKEN LINE HIGHWAY BOUNDARY HR – ISA[-USAH SANITARY SEWER, HANGING EDMGSC_P DAM, GRAVEL BAG/SAND BAG CHECK STDL* MHBA HIGHWAY BOUNDARY, APPROX. - AHB -DOTTED LINE LONG - SAF-USAF SANITARY SEWER, FORCE MAIN, UGND EDMPC_P DAM, PREFABRICATED CHECK MHBW HWY BOUNDARY, FACE OF WALL STDS* DOTTED LINE SHORT –]S*AF*[– SANITARY SEWER, FORCE MAIN, HANG FULL BARRIER LINE MHBWOA HIGHWAY BOUNDARY, W/O ACCESS STFB* HR W/OA UT TELEPHONE. UNDERGROUND EDMSC_P DAM, STONE CHECK MJC JURISDICTION, CITY STH* HATCH LINE]7[-UTH TELEPHONE, HANGING MJCY JURISDICTION, COUNTY PARTIAL BARRIER LINE UT0 TELEPHONE, OVERHEAD **EFNSV** FENCE, SILT & VEGETATION MJHD JURISDICTION, HISTORIC DISTRICT STRCT ROUNDABOUT, CAT TRACKS IITV CABLE TV, UNDERGROUND - CTV **EFNV** FENCE, VEGETATION ************ MJLL JURIS., (GREAT, MILITARY) LOT LINE STRYL ROUNDABOUT, YIELD LINE –]CTV[− UTVH CABLE TV, HANGING AA EWAA_F WETLAND, ADJACENT AREA MJN JURISDICTION, NATION STSB STOP BAR UTVO CABLE TV, OVERHEAD -OCTV-WETLAND, FEDERAL **MJPB** JURISDICTION, PUBLIC LANDS STSE* SOLID, EDGE 11/1 UNKNOWN, UNDERGROUND **EWES** WETLAND, FEDERAL AND STATE MJS JURISDICTION, STATE - *]UU[* -STXL UNKNOWN, HANGING X WALK, LADDER LINE **EWM** WETLAND, MITIGATION AREA JURISDICTION, TOWN OUL-UUO UNKNOWN, OVERHEAD SW EWS WETLAND, STATE STXLB X WALK, LADDER BAR LINE M.IV JURISDICTION, VILLAGE UW WATER LINE, UNDERGROUND • = W (WHITE) OR Y (YELLOW) MPL PROPERTY LOT LINE WATER LINE, HANGING TRAFFIC CONTROL 1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED). MPLA PROPERTY LOT LINE, APPROXIMATE UWO WATER LINE, OVERHEAD OW 0 TCSW SIGNAL, SPAN WIRE 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.). MSL SUB LOT LINE 3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS. PIN 8780.49 BRIDGES **CULVERTS** KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON 5. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE LEGEND, LINE AND POINT SYMBOLOGY DRAWING NO. LEG-1 CORRESPONDING EXISTING FEATURES.

COUNTY: ULSTER

REGION:

...\CADD\87884* 11/21/2019 tviic-1

GREENMAN-PEDERSEN, INC CONSULTING ENGINEERS

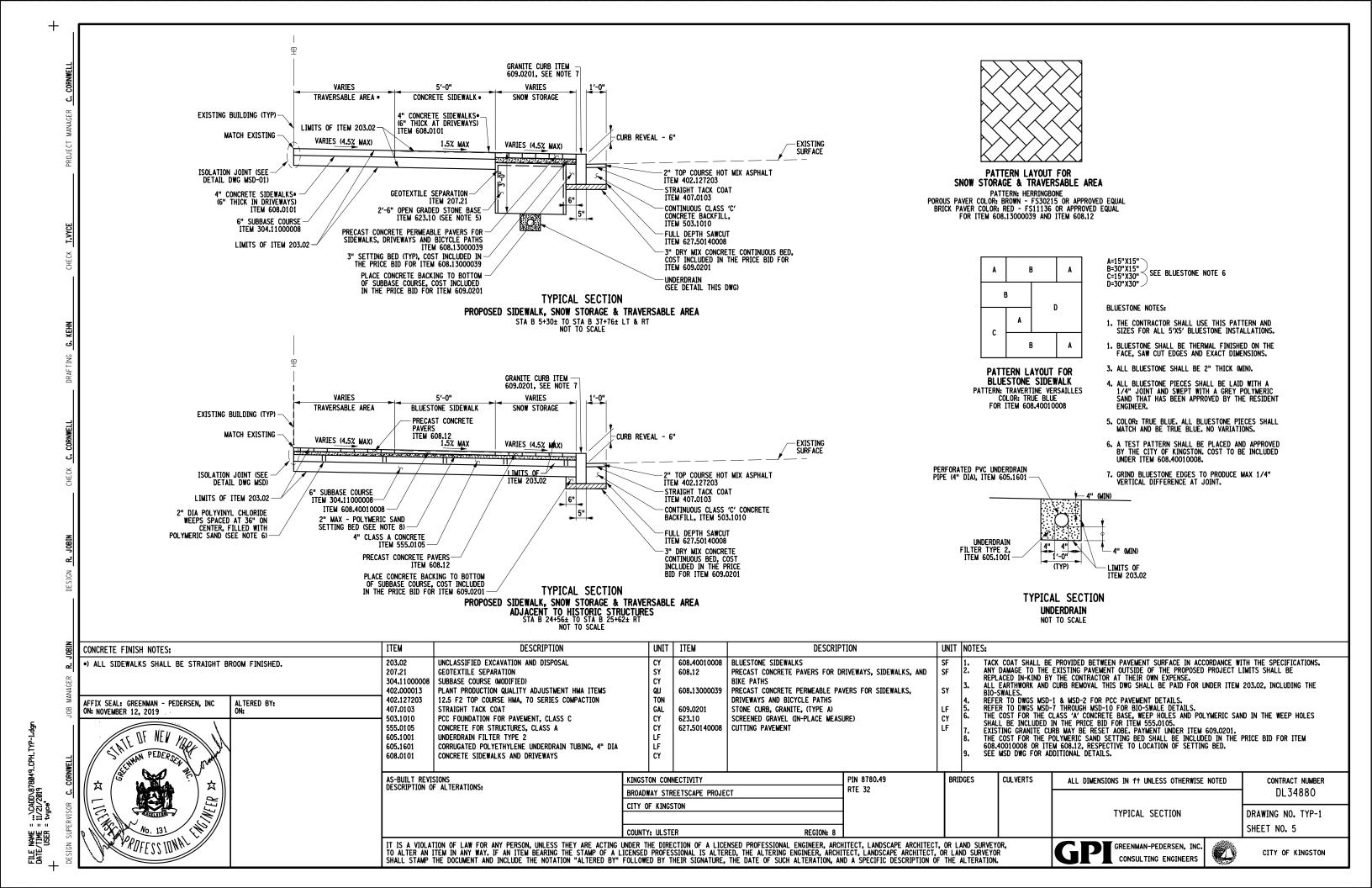
CITY OF KINGSTON

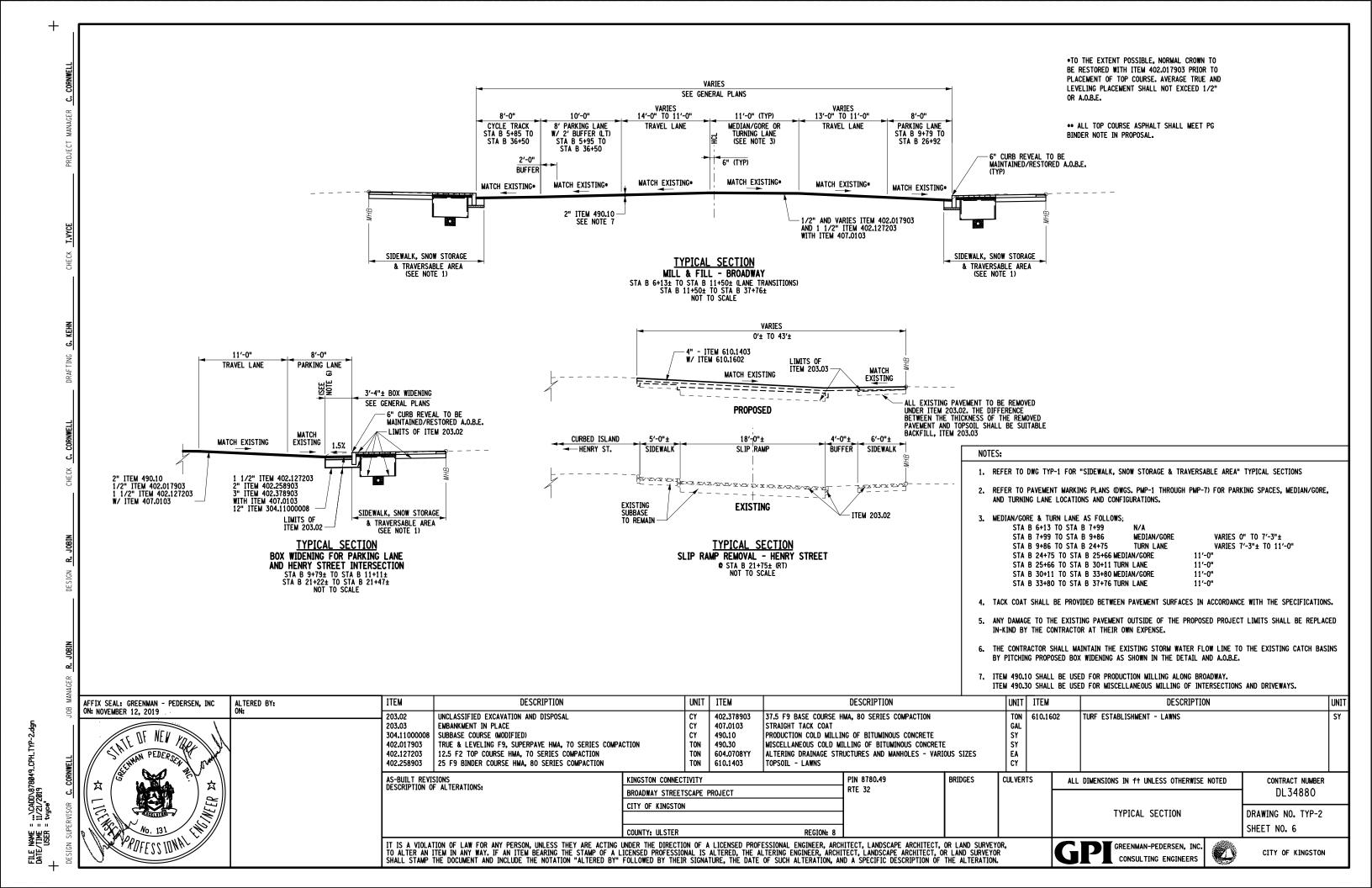
SHEET NO. 3

ITS **ROW MAPPING ALIGNMENT** DRAINAGE SIGNS UTILITIES **CELL** NAME DESCRIPTION CELL CELL NAME DESCRIPTION **CELL CELL** CELL NAME DESCRIPTION NAME DESCRIPTION NAME DESCRIPTION NAME DESCRIPTION \oplus CENTER OF CURVATURE DINV ₩ IANT_P 0 MDL1P DEED LINE, TYPE 1 \mathbb{E} UEB ELECTRIC, BOX **ANTENNAS** SINGLE POST **ACOGO** 0 S_P SINGLE POST, PROPOSED ELECTRIC, METER IASCTS ACCOU, SPEED/COUNT SNSR.S MDL2P DEED LINE. TYPE 2 UEM DS STRUCTURE. RECTANGULAR (<u>©</u>) ACS CURVE TO SPIRAL (3) SB F BACK TO BACK, PROPOSED Œ) UEMH ELECTRIC. MANHOLE TCARPAD CABINET & PAD MDL 3F DEED LINE. TYPE 3 DSI STRUCTURE, INVERT 4 Δ DETOUR, POINT OF INTERSECT \oplus ADPT_F ICCTV CCTV SITE MDL4P DEED LINE, TYPE 4 UEPT ELECTRIC, POLE, TRANS. DSM STRUCTURE, MANHOLE)CDPD(ADPL_P DETOUR, POINT ON LINE ICDPD **5** \bigoplus UGM GAS, METER CDPD TRANSCEIVER MDL5P DEED LINE, TYPE 5 SPM PARKING METER STRUCTURE, MANHOLE, DSMTXX_F **AEQN** EQUATION 0 **ICELL** CELL PHONE TOWER MEEP EASEMENT, EXISTING RFMSRM REFERENCE MARKERS **UGMH** GAS. MANHOLE "XX" = 48, 60, 72, 96 (A) **AEQNAHD EQUATION AHEAD** SRSC3 SHLD, CTY, 123 DIG **-⟨Ĝ>**-GAS, LINE MARKER **ICJB** CONDUIT JACK OR BORING (A) EASEMENT, PERM., APPROX. DSR STRUCTURE, ROUND (B) **AEQNBK EQUATION BACK** 0 SRSC4 SHLD, CTY, 4 DIG. FP UGP GAS/FUEL PUMP \boxtimes TONTI CAR MEPP F CONTROLLER CABINET EASEMENT, PERM., BACK LINE STRUCTURE, RECT., WITH CURB TYPE "X"
"X" = F, G, N, O, P, R \odot **AEVT EVENT STATION** \bigcirc 0 Ω **ICPB** COMMUNICATION PULL BOX MEPSP_ EASEMENT, PERM., SHAPE SRSCT2 SHLD, CTY TOUR, 1-2 DIG. UGV GAS, VALVE (o) APC POINT OF CURVATURE $-\otimes$ ICTD CONDUIT TURNING DOWN ♦ MFAP_F FEE ACQUISITION, APPROX. SRSCT4 SHLD, CTY TOUR, 3-4 DIG. (XIO) STRUCTURE, RECT., TYPE "X" POINT OF COMPOUND CURVATURE "X" = I, K, L, M, O, P, U APCC ULP \odot —⊙ ICTU CONDUIT TURNING UP **۞** MFP_P FEE ACQUISITION, BACK LINE SRSI SHLD. INTERSTATE LIGHTING. POLE API POINT OF INTERSECTION)Ó(**ICVTR1** COMM. VEH. ROAD TRANSCEIVER MFSP_ FEE ACQUISITION, SHAPE SRSN2 SHLD, NATIONAL, 2 DIG. **ULPM** LIGHTING, POLE, MEDIAN **ENVIRONMENTAL** APOB POINT OF BEGINNING IDEFAUL1 SHLD, NATIONAL, 3 DIG. ULPP LIGHTING, POLE, PED. MHBA HIGHWAY BNDRY., APPROX CULV EI0P_P STR., INLET, OUTLET PROT. \odot APOC POINT OF CURVATURE ΕZ • \bigcirc UMFC MISC. FILLER CAP SRSS2 SHLD. STATE, 2 DIG. **IEZR** E-ZPASS READER MHBCP HISTORICAL, BLDG. CORNERS AP0E POINT OF END Δ **IEZTR** TRANSMITTAL READER HIGHWAY BNDRY, PT SRSS3 SHLD, STATE, 3 DIG **-**∅> UOLM OIL, LINE MARKER (B) EIPGB_P STR., INLET PROT., GRAVEL BAG **APOL** POINT ON LINE \odot **IFOXCAB** 0 SRSS4 SHLD, STATE, 4 DIG. POLE, WITH UTILITY FIBER OPTIC X-CONNECT CABINET MJCF PT., JURIS, CITY H/S) EIPHS_P STR., INLET PROT., HAY/STRAW \odot APOS POINT ON SPIRAL **(** MPBC PT., BUILDING CORNER \odot LIPD POLE, DEAD (NO UTILITY) IFUSSPL FUSION SPLICE TRAFFIC CONTROL 0 AP01 POINT ON TANGENT **THARAD**\ HAR ADVISORY SIGN PT., CROSS CUT UPL POLE, WITH LIGHT (PRFB) EIPP_P STR., INLET PROT., PREFAB. **TCBJ** BOX. JUNCTION POINT ON VERTICAL CURVE 一位 **APOVO** IHARS1 HAR SITE MPDH PT., DRILL HOLE (S) USME SANITARY SEWER MANHOLE TCBP BOX. PULL BOX (SF) EIPSF_F STR., INLET PROT., SILT FENCE POINT ON VERTICAL TANGENT APOVT * P HTR Δ ILC LOAD CENTER MPF PT., FENCE LOCATION TELEPHONE, BOOTH TCBS BOX, SPLICE **APORC** POINT ON REVERSE CURVE 0 UTLM TELEPHONE, LINE MARKER IMECSPI MECHANICAL SPLICE MPIF PT., IRON PIPE **ERCB** RISER, CONCRETE BOX TCMC MICROCOMPUTER CABINET 0 APT POINT OF TANGENCY PM] \odot \overline{T} HTMH TELEPHONE, MANHOLE **TMSCS** PORT, SPEED & COUNT SENSOR MPTR PT., IRON ROD ETRS_F TRAP, SEDIMENT PED POLE POINT OF VERTICAL CURVATURE **(** APVC | M |] **IMSCTS** MICRO SPEED & COUNT SENSOR MPM PT., MONUMENT **-**♦> UTVLM CABLE TV, LINE MARKER WETLAND FLAG TCSH SIGNAL HEADS APVCC POINT OF VERT. CMPND CURVE \blacksquare CABLE TV, PULL BOX Δ (M): IMT MICROWAVE TRANSCEIVER MPMM PT., MONUMENT, MISC. UTVPB \odot **TCSP** SIGNAL POLE **GEOTECHNICAL** POINT OF VERT. INTERSECTION VMS Ø PT., NATI \Box TOVHVMS PERM. OVERHEAD VMS MPN UUB UNKNOWN, BOX TRAFFIC WORK ZONE • GDH DRILL HOLE APVRC POINT OF VERT. REVERSE CURVE Δ PA] **IPASCS** PORT. ACCOU. SPD & CNT. SENSOR * MPRS PT., RAILROAD SPIKE \boxtimes UNKNOWN, JUNCTION BOX UUJB APV1 POINT OF VERTICAL TANGENCY PEDESTRIAN SIGNAL HEAD 斑 PT. SPIKE TW7AP P ARROW PANEL **IPEDS** MPSE UNKNOWN, MANHOLE UUMH LANDSCAPE (<u>©</u>) ASC SPIRAL TO CURVE \Diamond **IPSS** PAVEMENT SURFACE SENSOR **MPST** PT., STAKE TW7APC P ARROW PANEL, CAUTION MODE UUPB UNKNOWN, PULL BOX LELS ELEVATION. SPOT SPIRAL POINT OF INTERSECTION ASP1 PVMS ••• TWZAPT_P ARROW PANEL, TRAILER OR SUPPOR **IPVMS** (×) PT., TREE W/ WIRE UUVL UNKNOWN, VALVE LFP FLAG POLE SPIRAL TO SPIRAL ASTS IRM + BARRICADE (TYPE III) PT., WALL LOCATION ∞ RAMP METER UUVT UNKNOWN, VENT MAILBOX \otimes AST SPIRAL TO TANGENT ∕\ RWI **IRWIS** RDWY WEATHER INFO. SENSOR TWZCMS_F CHANGEABLE MESSAGE SIGN (PVMS) 0 UUW UNKNOWN, WELL ROW ACQUISITION PAPER BOX LPB \otimes ATS TANGENT TO SPIRAL × ISF TWZFLG_P \odot WATER, FIRE HYDRANT -LIWE 0 **LPST** POST, SINGLE MFS_P_T FFF ACQUISITION AVEV1 VERTICAL EVENT POINT ISST SPREAD SPECT. TRANSCEIVER TWZFT_P FLAG TREE Δ W UWM WATER, METER (I) LRB ROCK, BOULDER IMPACT ATTENUATOR / AVHIGH VERTICAL HIGH POINT ITDB TELEPHONE DEMARCATION BLK TWZIA_P UWMH WATER, MANHOLE MEPS_P_T CRASH CUSHION (TEMPORARY) EASEMENT, PERMANENT LSHC SHRUB, CONIFEROUS \odot AVLOW VERTICAL LOW POINT ITF SUBSURFACE TEMP, PROBE LUMINAIRE (TEMPORARY) UWV WATER. VALVE () LSHD SHRUB. DECIDUOUS METS_P_T EASEMENT, TEMPORARY ➾ **W** HWW IVTRI TW7SDT P SYMBOL. DIRECTION OF TRAFFIC WATER. WELL VEHICLE TO RDWY TRANSCEIVER BRIDGE 쏬 LTC TREE, CONIFEROUS SYMBOL, DIRECTION OF TEMPORARY IWIMD TWZSDTD_F W/M WEIGHT IN MOTION DETECTOR METS_P. OCCUPANCY. TEMPORARY BSC BRIDGE, SCUPPER LTD TREE, DECIDUOUS)WVR TWZSGN_P SIGN (TEMPORARY TWVR WIRELESS VIDEO REPEATER \bigcirc LTS TREE, STUMP CONTROL FEE ACQUISITION W/O ACCESS SIGNAL, TRAFFIC OR PEDESTRIAN TWZSIG_P (V)**TWVRC** WIRELESS VIDEO RECEIVER (TEMPOŘARY) Ø TREE, WELL OR WALL CBP IWVTT WIRELESS VIDEO TRANSMITTER മ Δ BASELINE, POINT TWZWL_P WARNING LIGHT ROADWAY LUKP UNKNOWN POINT \odot CBPOL BASELINE, POINT ON LINE 113 WORK VEHICLE NOTES: ∅ RES_P ELEVATION, SPOT WORK VEHICLE WITH TRUCK CBSP BASELINE, SPUR POINT TWZWVA_P 1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED). \boxtimes RGA GUIDE RAIL, ANCHOR CBTP BASELINE, TIE POINT 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY \bigcirc GUIDE POST, SINGLE SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLÉ, ETC.). CPBM 3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CPH POINT, HORIZ, PHOTOGRAMMETRY PIN 8780.49 BRIDGES **CULVERTS** CORRESPONDING PROPOSED FEATURES. KINGSTON CONNECTIVITY ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER RTE 32 DL34880 **(** CPSM POINT, SURVEY MARKER, PERM. 4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED ROADWAY STREETSCAPE PROJECT CITY OF KINGSTON CPSV POINT, VERT., PHOTOGRAMMETRY FEATURES IS THICKER (0.015 IN ON B SIZE DRAWINGS). LEGEND, LINE AND POINT SYMBOLOGY DRAWING NO. LEG-2 MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF SHEET NO. 4 COUNTY: ULSTER REGION: TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.

6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES. GREENMAN-PEDERSEN, INC CITY OF KINGSTON CONSULTING ENGINEERS

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AFFIX SEAL: GREENMAN - PEDERSEN, INC

STATE OF NEW MAR

WAN PEDERSEN

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GENERAL NOTES

- MATERIAL AND CONSTRUCTION SPECIFICATIONS: "STANDARD SPECIFICATIONS, CONSTRUCTION AND MATERIALS". NEW YORK STATE DEPARTMENT OF TRANSPORTATION (NYSDOT) OFFICE OF ENGINEERING, WITH CURRENT ADDITIONS AND MODIFICATIONS, SHALL BE IN EFFECT FOR THIS PROJECT.
- CURRENT NATIONAL "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) WITH NEW YORK STATE SUPPLEMENT SHALL BE IN EFFECT FOR THIS PROJECT.
- ADDITIONAL NOTES MAY BE FOUND ON SUBSEQUENT DRAWINGS. SUCH NOTES, WHILE PERTAINING TO THE SPECIFIC DRAWING THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HEREIN.
- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT DUE TO THE NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EXTENT OF THE WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH THE FIELD CONDITIONS OF THE WORK IN ACCORDANCE WITH THE FIELD CONDITIONS AND A.O.B.E.
- THE CONTRACTOR SHALL EXAMINE AND VERIFY IN THE FIELD ALL EXISTING CONDITIONS AND DIMENSIONS WITH THOSE SHOWN ON THE PLANS. THE CONTRACTOR SHALL USE THE FIELD CONDITIONS AND DIMENSIONS, AND MAKE THE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS AS APPROVED BY THE ENGINEER. THE RESULTS OF THIS CHECK OF CONDITIONS AND DIMENSIONS SHALL BE SO NOTED ON THE DRAWINGS SUBMITTED
- THERE SHALL BE NO CLAIM AGAINST THE CITY BY THE CONTRACTOR FOR WORK PERTAINING TO MODIFICATIONS AS MAY BE REQUIRED DUE TO ANY DIFFERENCE BETWEEN ACTUAL FIELD CONDITIONS AND THOSE SHOWN BY THE DETAILS AND DIMENSIONS ON THE CONTRACT PLANS. THE CONTRACTOR WILL BE PAID AT THE UNIT BID PRICE FOR THE ACTUAL QUANTITIES OF MATERIALS USED OR FOR THE WORK PERFORMED, AS INDICATED BY THE VARIOUS ITEMS IN THE CONTRACT AND PER SPECIFICATIONS FOR MAJOR AND MINOR
- 7. AT ALL TIMES, THE CONTRACTOR SHALL TAKE MEASURES TO PROVIDE POSITIVE DRAINAGE OF SURFACE RUNOFF FROM THE TRAVEL LANES AND CONTROL OF THE RUNOFF TO PREVENT EROSION, POLLUTION, SEDIMENTATION OR OTHER DISCHARGES WHICH WOULD AFFECT PROPERTIES ADJACENT TO THE WORK SITE. ALL MEASURES TAKEN TO PROVIDE POSITIVE DRAINAGE SHALL BE APPROVED BY THE ENGINEER PRIOR TO IMPLEMENTATION. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN
- THE CONTRACTOR SHOULD NOTE THAT ADDITIONAL WORK MAY BE REQUIRED AS THE CONTRACT PROGRESSES WHICH IS NOT SHOWN OR NOTED ON THE PLANS. THIS WORK SHALL BE PERFORMED BY THE CONTRACTOR AS ORDERED BY THE ENGINEER AND PAYMENT SHALL BE MADE IN ACCORDANCE WITH SECTION 100 OF THE NYSDOT STANDARD SPECIFICATIONS BOOK.
- THE CLEARING AND GRUBBING ITEM SHALL CONSIST OF SPECIFIC REMOVAL OF TREES AND TREE STUMPS WITHIN THE PROJECT LIMITS WHERE INDICATED ON THE PLANS AND A.O.B.E. THE COST OF THIS WORK SHALL BE INCLUDED UNDER ITEMS 614.060304 AND 614.0701. SEE ADDITIONAL NOTES IN THIS DWG.
- NO PAYMENT SHALL BE MADE FOR WORK CALLED FOR BY NOTES ON THE PLANS, IN THE SPECIFICATIONS, OR UNDER THE HEADING GENERAL NOTES UNLESS PAYMENT IS SPECIFICALLY INDICATED BY ITEM NUMBER. THE COST OF WORK FOR WHICH NO SEPARATE PAYMENT IS INDICATED SHALL BE INCLUDED IN THE UNIT PRICES
- THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY SUPPORTS, BRACING OR OTHER DEVICES THAT MAY BE REQUIRED OR THAT MAY BE DIRECTED BY THE ENGINEER TO PROTECT THE SAFETY OF ADJACENT STRUCTURES, ROADWAYS OR THE VARIOUS ITEMS IN THE CONTRACT. NO SEPARATE PAYMENT SHALL BE MADE.
- 12. PAVED AREAS DISTURBED BY THE CONTRACTOR AS PART OF WORK TO BE PERFORMED UNDER THIS CONTRACT, SHALL BE RESTORED TO AN ACCEPTABLE CONDITION AS SPECIFIED BY AND SATISFACTORY TO THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR GUARDING AND PROTECTING ALL OPEN EXCAVATIONS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 107.05 "SAFETY & HEALTH REQUIREMENTS" OF THE NYSDOT STANDARD SPECIFICATIONS AND ENSURING ACCESS TO ADJACENT STORE FRONTS. IN ADDITION, A MINIMUM OF 1-INCH THICK PLATE WILL BE REQUIRED OVER ALL EXCAVATIONS THAT ARE TO BE COVERED (A.O.B.E.). THE PLATES SHALL BE SECURELY FASTENED DOWN TO THE SATISFACTION OF THE ENGINEER AND SHALL BE STRUCTURALLY CAPABLE OF CARRYING ALL IMPOSED LOADS. THE COST SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT. SEE ADDITIONAL PROPOSAL NOTES.
- 14. INFILTRATION TESTS ARE LOCATED IN THE CONTRACT PROPOSAL DOCUMENTS.

ALTERED BY:

- PROVISIONS TO DE-WATER EXCAVATIONS, DUE TO CONSTRUCTION OPERATIONS ALONG THE PROJECT MAY BE REQUIRED. THERE SHALL BE NO SEPARATE PAYMENT FOR ANY DE-WATERING SYSTEMS. COST SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS IN THE CONTRACT.
- THE CONTRACTOR SHALL KEEP ALL DRAINAGE FACILITIES, WITHIN THE CONTRACT LIMITS, CLEAN AND FULLY OPERATIONAL AT ALL TIMES (A.O.B.E.). THIS WORK SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE
- THE CONTRACTOR SHALL PROVIDE SURVEY AND STAKEOUT AS REQUIRED AND IN ACCORDANCE WITH SECTION 625 OF THE STANDARD SPECIFICATIONS. COST FOR THIS WORK SHALL BE INCLUDED UNDER ITEM
- 18. THE CONTRACTOR IS TO VISIT THE SITE BEFORE BIDDING TO BECOME FAMILIAR WITH THE PRESENT CONDITIONS AND TO JUDGE THE EXTENT AND NATURE OF THE WORK TO BE DONE UNDER THIS CONTRACT. NO EXTRA COMPENSATION WILL BE ALLOWED BECAUSE OF FAILURE TO INCLUDE IN THE BID ALL ITEMS AND MATERIALS WHICH ARE REQUIRED TO BE FURNISHED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.

AS-BUILT REVISIONS

- 19. THE CONTRACTOR IS REQUIRED TO PROTECT WORKERS AT ALL TIMES IN CONFORMANCE WITH APPLICABLE OSHA
- 20. DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT WORK IN SUCH A MANNER AS TO DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR STALL CONDUCT WORK IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM, ANY DAMAGE TO THE VARIOUS WATER BODIES FROM POLLUTION BY DEBRIS, SEDIMENT OR OTHER FOREIGN MATERIALS, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL OR OTHER IMPURITIES.
- THE FIRST WATERING NEEDED FOR VEGETATION AND OTHER LANDSCAPING ITEMS SHALL BE INCLUDED UNDER EACH RESPECTIVE ITEMS IN THE CONTRACT ALL SUBSEQUENT WATERINGS SHALL BE PAID FOR UNDER ITEM
- 22. ITEM 611.19 POST-PLANTING CARE SHALL BE PAID FOR PER BIO-SWALE (1 EA) AND SHALL INCLUDE ALL TREES, SHRUBS, GRASSES, AND PERENNIALS IN EACH RESPECTIVE BIO-SWALE.
- 23. DETAILS ON THE DRAWINGS LABELED AS 'NOT TO SCALE' ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONALLY AND ARE FULLY DIMENSIONED.
- 24. ITEM 209.1703 DRAINAGE STRUCTURE INLET PROTECTION SHALL BE PLACED AT ALL MODIFIED DRAINAGE STRUCTURES PER NYSDOT SS 209-03.

UTILITY NOTES

- LOCATION OF UTILITIES, PUBLIC AND/OR PRIVATE, INDICATED ON THE PLANS AS EXISTING AND/OR TO BE CONSTRUCTED ARE APPROXIMATE ONLY. THEIR EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD. ADDITIONAL UTILITY LINES, WHETHER ABANDONED OR IN SERVICE, MAY EXIST AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONDUCT ALL OPERATIONS AND TAKE NECESSARY PRECAUTIONS SUCH THAT INTERFERENCE WITH OR DAMAGE TO THESE OR OTHER FACILITIES DURING THE COURSE OF CONSTRUCTION IS PREVENTED. PRIOR TO ANY EXCAVATION, THE CONTRACTOR IS TO CALL DIG SAFELY N.Y. TO HAVE
- IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE, CAUSING THE INTERRUPTION IN SAID SERVICE, THE CONTRACTOR SHALL IMMEDIATELY COMMENCE WORK TO RESTORE SERVICE AND MAY NOT CEASE WORK UNTIL SERVICE IS RESTORED, ALL COSTS TO REPAIR OR REPLACE DAMAGED UTILITIES SHALL BE AT THE EXPENSE OF THE CONTRACTOR, IF THE CONTRACTOR DOES NOT MAKE IMMEDIATE NECESSARY REPAIRS, THE RESPECTIVE OWNING COMPANIES OR MUNICIPAL FORCES MAY DO THE WORK, AND THE COST THEREOF
- THE CONTRACTOR SHALL MAKE EXPLORATIONS AS MAY BE NECESSARY TO DETERMINE THE DIMENSIONS AND LOCATIONS OF LINES THAT MAY BE SUBJECT TO DAMAGE, COST TO BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT. ITEM 204.03 WILL BE USED A.O.B.E. AT BIO-SWALES.
- PRIVATE UTILITY COMPANIES ARE ALSO REQUIRED TO ALTER/RELOCATE THEIR FACILITIES WITHIN THE PROJECT LIMITS. IT IS ANTICIPATED THAT THIS WORK SHALL BE COMPLETED PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR MAY BE REQUIRED TO LAYOUT THE PROJECT CENTERLINE AND OTHER FEATURES TO PROVIDE THE UTILITY COMPANIES WITH SUFFICIENT INFORMATION TO ALTER/RELOCATE THEIR FACILITIES. THE COST OF THIS WORK SHALL BE INCLUDED UNDER ITEM 625.01.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR CONTACTING THE UTILITY COMPANY FOR PROVIDING RESTRAINTS TO THE EXISTING UTILITY POLES (IF REQUIRED) WITHIN THE WORK LIMITS, WHILE CONSTRUCTION IS TAKING PLACE. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER TO PERFORM THE WORK. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS ITEMS.
- 6. THE UNDERGROUND UTILITY INFORMATION SHOWN ON THE PLANS IS BASED UPON THE FOLLOWING:

QUALITY LEVEL A IS THE HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED BY THE ACTUAL EXPOSURE (OR VERIFICATION OF PREVIOUSLY EXPOSED AND SURVEYED UTILITY FACILITIES) OF THE SUBSURFACE UTILITIES, USING (TYPICALLY) MINIMALLY INTRUSIVE EXCAVATION FOLIPMENT TO DETERMINE THEIR PRECISE HORIZONTAL AND VERTICAL

QUALITY LEVEL B IS THE SECOND HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED THROUGH THE APPLICATION OF APPROPRIATE SURFACE GEOPHYSICAL METHODS (I.e., UNDERGROUND CAMERAS, RADAR, SONAR, TONE OUTS, ETC.) TO IDENTIFY THE EXISTENCE AND APPROPRIATE HORIZONTAL POSITION OF SUBSURFACE UTILITY FACILITIES, QUALITY LEVEL B DATA ARE REPRODUCIBLE BY SURFACE GEOPHYSICS AT ANY POINT OF THEIR DEPICTION. THE INFORMATION WAS SURVEYED TO APPLICABLE TOLERANCES AND REDUCED ONTO THE PLANS. NO EXCAVATIONS WERE

QUALITY LEVEL C IS THE THIRD HIGHEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS HAS BEEN OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES AND BY USING PROFESSIONAL JUDGEMENT IN CORRELATING THIS INFORMATION TO QUALITY LEVEL D INFORMATION, (SHOWN AS QLC)

QUALITY LEVEL D IS THE LOWEST DEGREE OF ACCURACY. THE INFORMATION SHOWN ON THE PLANS WAS DERIVED SOLELY FROM EXISTING NYSDOT AND/OR UTILITY COMPANY RECORDS OR RECOLLECTIONS.

ALL UTILITIES THIS CONTRACT QL C AND D

- THE CONTRACTOR SHALL PROTECT ALL UNDERGROUND UTILITIES TO REMAIN IN PLACE FROM DAMAGE DURING THE CONSTRUCTION. METHODS OF PROTECTION MAY INCLUDE STEEL PLATES OVER THE UTILITY SO THAT WHEEL LOADINGS FROM CONSTRUCTION VEHICLES DO NOT DAMAGE THE UTILITY. THE COST OF PROVIDING PROTECTION OF UNDERGROUND UTILITIES SHALL BE INCLUDED UNDER VARIOUS ITEMS IN THE CONTRACT AND
- CONTRACTOR SHALL PROVIDE ALTERNATIVE BENCHMARK AND/OR BASELINE TIES IF IMPACTED DURING DNSTRUCTION. COST INCLUDED UNDER ITEM 625.01.

KINGSTON CONNECTIVITY

FIRE HYDRANT REPLACEMENT THROUGHOUT THE PROJECT LIMITS SHALL USE HYDRANTS APPROVED BY THE CITY OF KINGSTON WATER DEPARTMENT, ALL REPLACEMENT WORK SHALL BE COORDINATED WITH THE CITY OF KINGSTON WATER AND FIRE DEPARTMENTS.

DAMAGE TO EXISTING STRUCTURES. VEGETATION/SHRUBS. OR OTHER AMENITIES

NUMEROUS STRUCTURES (INCLUDING BUILDINGS, UTILITIES, ETC.) AND VEGETATION/SHRUBS ARE PRESENT WITHIN THE WORK LIMITS AND ARE TO REMAIN IN PLACE. THE CONTRACTOR SHALL TAKE EXTRA PRECAUTIONS NOT TO DAMAGE THESE ITEMS. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ALL DAMAGED CAUSED BY OPERATIONS TO THE EXISTING STRUCTURES OR MATERIALS WHICH ARE NOT INCLUDED AS PART OF THE INTENDED WORK. ALL DAMAGE TO THE EXISTING STRUCTURES OR MATERIALS WHICH ARE NOT PART OF THE INTENDED WORK SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR WITHOUT COST TO THE CITY AND TO THE SATISFACTION OF THE ENGINEER EXCEPT AS NOTED WITHIN.

EXCAVATION NOTES

- 1. ASSUME ALL EXCAVATED MATERIAL IS NOT SUITABLE FOR EMBANKMENT CONSTRUCTION.
- 2. ASSUME SUBGRADE IMPROVEMENTS WILL NOT BE REQUIRED.

ENVIRONMENTAL PERMITS

THE CONTRACTOR SHALL COMPLY WITH ALL ENVIRONMENTAL PERMIT REQUIREMENTS PROVIDED IN THE CONSTRUCTION DOCUMENTS.

TREE CUTTING RESTRICTIONS

- IN ORDER TO PREVENT ANY DIRECT IMPACT OF INDIANA BAT (MYOTIS SODALIS), AND NORTHERN LONG-EARED BAT (MYOTIS SEPTENTRIONALIS), BOTH FEDERAL AND STATE LISTED ENDANGERED SPECIES, THE CONTRACTOR'S ATTENTION IS HEREBY DIRECTED TO THE FACT THAT TREE CUTTING RESTRICTIONS APPLY TO TREES OR BRANCHES THAT ARE 3 INCHES OR GREATER IN DIAMETER AT BREAST HEIGHT (DBH).
- DUE TO THE POSSIBLE PRESENCE OF PROTECTED BAT SPECIES, THE CONTRACTOR SHALL BE AWARE THAT TREES OR BRANCHES 3" DIAMETER OR LARGER (TREES MEASURED AT BREAST HEIGHT), CAN ONLY BE CUT FROM NOVEMBER 1 TO MARCH 31. NO TREE CUTTING WILL OCCUR FROM APRIL 1 TO OCTOBER 31.

RIGHT OF WAY NOTES

- ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY (ROW) IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR IS TO ASSURE HIMSELF THAT ALL WORK IS BEING PERFORMED WITHIN THE ROW, INCLUDING BUT NOT LIMITED TO VEHICLE ACCESS; STORAGE OF EQUIPMENT, MATERIALS, DEBRIS AND WASTE; LANDSCAPING; VEGETATION REMOVAL AND MANAGEMENT, GRADING, SEEDING AND THE INSTALLATION OF TURF; AND THE INSTALLATION OF ANY FENCES OF DRODECTIVE DARBORS.
- IF CONTRACTOR IS UNABLE TO IDENTIFY THE LIMITS OF THE RIGHT-OF-WAY WHEN THE CONTRACT CALLS FOR WORK IN THOSE VICINITIES, THE CONTRACTOR MUST CONTACT THE PROJECT ENGINEER FOR DEFINITIVE BOUNDARY DETERMINATIONS BEFORE ANY WORK MAY BE INITIATED AT THOSE LOCATIONS (STANDARD SPECIFICATIONS SECTIONS 105-10 AND 625).
- IN ACCORDANCE WITH SECTION 107-13 OF THE STANDARD SPECIFICATIONS, RELEASES FOR ANY NON-ESSENTIAL CONTRACT WORK OUTSIDE OF THE EXISTING RIGHT-OF-WAY, INCLUDING PLANTINGS, LANDSCAPING OR DRIVEWAY ENHANCEMENT, WILL BE PROVIDED BY THE CITY OF KINGSTON AND IN NO INSTANCE ARE TO BE SECURED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT INVADE UPON PRIVATE PROPERTIES, LANDS OR BUILDINGS OUTSIDE OF THE RIGHT-OF-WAY FOR ANY REASON WITHOUT FIRST SECURING WRITTEN PERMISSION FROM THE PROPERTY OWNER (STANDARD SPECIFICATIONS SECTION 105-15.
- THE CONTRACTOR WILL BE HELD LIABLE FOR ANY DAMAGES DONE. ANY SUCH INJURIES OR DAMAGES SHALL BE SATISFACTORILY REPAIRED OR ITEMS REPLACED AT THE CONTRACTOR'S EXPENSE (STANDARD SPECIFICATIONS SECTION 107-08).

CURB RAMPS AND SIDEWALK NOTES

- ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS. AT SOME LOCATIONS THE PUBLIC RIGHT-OF-WAY IS AT THE BACK OF THE EXISTING SIDEWALK OR VERY CLOSE TO THE BACK OF THE EXISTING SIDEWALK. AT THESE LOCATIONS THE EXISTING SIDEWALK AND CURB RAMP REMOVAL, AND CURB RAMP INSTALLATIONS WILL REQUIRE NON-TRADITIONAL FORM WORK AND/OR PRE-CAST INSTALLATION TO ENSURE THAT ALL WORK IS PERFORMED WITHIN THE PUBLIC RIGHT-OF-WAY. WHEN THE APPARENT HIGHWAY BOUNDARY IS AT THE BACK OF THE SIDEWALK OR CURB RAMP, ALL WORK MUST BE PERFORMED FROM THE ROAD SIDE.
- THE DESIGN OF THE CURB RAMPS WAS DONE USING OBTAINED SURVEY INFORMATION. HOWEVER, HORIZONTAL DIMENSIONS SHOWN ON THE DETAILS ARE APPROXIMATE AND ARE FOR ESTIMATING PURPOSES ONLY. THE PERCENT SLOPES SHOWN ON THE DETAILS SHALL TAKE PRECEDENT OVER HORIZONTAL DIMENSIONS WHEN

STREET AMENITIES

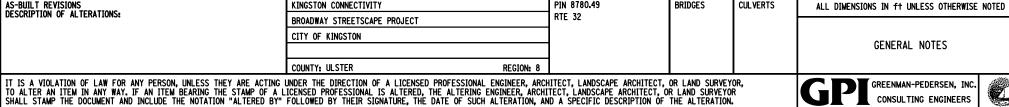
- THE CONTRACTOR SHALL INSTALL ANTI-THEFT/ANCHORAGE FOUNDATIONS, IN ACCORDANCE WITH THE SELECTED MANUFACTURERS' RECOMMENDED INSTALLATION PROCEDURES FOR ALL STREET AMENITIES (I.E. BIKE RACK, TRASH/RECYCLE BINS, BENCH, PAY STATION, ETC.). THE COST SHALL BE INCLUDED UNDER THE VARIOUS STREET AMENITY ITEMS IN THE CONTRACT AND METHODS A.O.B.E.
- THE PARKING METER AT THE CORNER OF BROADWAY AND FIELD COURT SHALL BE REINSTALLED UNDER ITEM 634.06010007. THE CONTRACTOR IS TO REFER TO THE CITY OF KINGSTON'S PARKING METER CODE AND DETAILS FOR INSTALLATION.

RRFB NOTES:

BRIDGES

CULVERTS

- INSTALLATION TO BE PERFORMED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND AS DIRECTED
- THE HEIGHT OF THE ORNAMENTAL RRFB POLES SHALL BE 15 FEET TO ACCOMMODATE SOLAR PANEL AND ALL REQUIRED SIGNS. PAYMENT FOR THIS ITEM SHALL BE MADE UNDER ITEM 680.82254310.
- RRFB POLES ARE TO BE GALVANIZED AND PAINTED BLACK AT THE DISCRETION OF THE CITY IN ACCORDANCE WITH SECTION 657 OF THE STANDARD SPECIFICATIONS (PAINTING GALVANIZED SURFACES). PAINTING SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 680.82254310.



PIN 8780.49





DRAWING NO. GNN-1 SHEET NO. 7

CONTRACT NUMBER

= ...\CADD\878849_CPH_WZN-1.c = 11/21/2819 : tvuce*

WORK ZONE TRAFFIC CONTROL NOTES:

GENERAL

WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), THE CURRENT VERSION OF SECTION 619 OF THE STANDARD SPECIFICATIONS, THE CURRENT WORK ZONE TRAFFIC CONTROL (619 SERIES) STANDARD SHEETS, ANY PROVISIONS CONTAINED IN THE PLANS AND/OR PROPOSAL OF THIS CONTRACT, AND AS DIRECTED BY THE ENGINEER.

WORK ZONE TRAFFIC CONTROL FOR MOBILE OPERATIONS SHALL BE PROVIDED IN ACCORDANCE WITH FIGURES 6H-4, 6H-17 AND 6H-35, AS APPLICABLE, OF THE MUTCD.

AS DEFINED IN SECTION 101-02 OF THE STANDARD SPECIFICATIONS, THE MUTCD CONSISTS OF THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THE NEW YORK STATE SUPPLEMENT TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS.

NIGHTTIME CONSTRUCTION IS NOT ALLOWED UNDER THIS CONTRACT.

CHANGES TO THE WORK ZONE TRAFFIC CONTROL PLAN:

THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE WORK ZONE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE A MINIMUM OF SEVEN (7) CALENDAR DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE BASIC CONCEPT OR SCOPE OF THE WORK ZONE TRAFFIC CONTROL PLAN. SUCH CHANGES TO THE BASIC CONCEPT OR SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE A MINIMUM OF FORTY-FIVE (45) CALENDAR DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.

CONSTRUCTION EQUIPMENT AND VEHICLES

ALL VEHICLES AND EQUIPMENT WITHIN THE CONTRACT LIMITS AND ON TRAVEL LANES AND/OR SHOULDERS SHALL BE EQUIPPED WITH AND SHALL OPERATE A MINIMUM OF ONE AMBER ROTATING OR FLASHING LIGHT EMITTING DIODE (LED) BEACON VISIBLE FROM ALL DIRECTIONS FOR A MINIMUM OF 1000 FEET DURING DAYLIGHT. IF VISIBILITY OF A SINGLE BEACON IS BLOCKED BY A PORTION OF THE VEHICLE OR EQUIPMENT, ADDITIONAL BEACONS SHALL BE PROVIDED. BEACONS SHALL BE MOUNTED IN A MANNER WHICH DOES NOT CAUSE GLARE FOR DRIVERS USING THE ROADWAY OR THE OPERATOR OF THE VEHICLE OR EQUIPMENT.

ALL BARRIER/SHADOW VEHICLES SHALL WEIGH A MINIMUM OF 18,000 LB AND SHALL BE EQUIPPED WITH TRUCK-MOUNTED IMPACT ATTENUATORS AND TRUCK-MOUNTED ARROW PANELS, IMPACT ATTENUATORS SHALL MEET THE REQUIREMENTS OF NCHRP 350 TEST LEVEL 3 AND SECTION 729-12 OF THE STANDARD SPECIFICATIONS, ARROW PANELS SHALL BE TRUCK-MOUNTED SERIES B OR C PANELS MEETING THE REQUIREMENTS OF SECTION 729-15 OF THE STANDARD SPECIFICATIONS AND SECTION 6F.61 OF THE MUTCD. THE PRICE OF TRUCK-MOUNTED IMPACT ATTENUATORS AND TRUCK-MOUNTED ARROW PANELS SHALL BE INCLUDED IN THE PRICE BID FOR THE BASIC WORK ZONE TRAFFIC CONTROL ITEM.

PARKING OF CONSTRUCTION EQUIPMENT & VEHICLES AND STORAGE OF MATERIALS:

CONSTRUCTION EQUIPMENT (INCLUDING LIGHT TOWERS, IF USED), VEHICLES AND MATERIALS SHALL BE PLACED OR STORED DURING NON-WORKING HOURS A MINIMUM OF 30 FEET FROM THE EDGE OF PAYMENT OR BEHIND TEMPORARY CONCRETE BARRIER OR GUIDE RAIL. EQUIPMENT, VEHICLES AND MATERIALS STORED BEHIND TEMPORARY CONCRETE BARRIER OR GUIDE RAIL SHALL NOT BE PLACED OR STORED WITHIN THE APPROPRIATE DEFLECTION DISTANCE SHOWN IN TABLE 619-6, GUIDE RAIL AND TEMPORARY CONCRETE BARRIER STANDARD DEFLECTION DISTANCES, IN THE STANDARD DEFLECTIONS.

PRIVATE VEHICLES OWNED BY THE CONTRACTOR OR THE CONTRACTOR'S WORKERS SHALL BE PARKED, DURING WORKING AND NON-WORKING HOURS, IN ACCORDANCE WITH THE REQUIREMENTS IN THE PRECEDING PARAGRAPH.

CONDITION OF TRAFFIC CONTROL DEVICES:

AT THE START OF CONTRACT WORK, ALL CONES, TEMPORARY TUBULAR MARKERS, DRUMS, CONSTRUCTION BARRICADES, VERTICAL PANELS, WARNING LIGHTS, ARROW PANELS, PORTABLE VARIABLE MESSAGE SIGNS, PAVEMENT MARKING TAPE AND RAISED PAVEMENT MARKERS SHALL APPEAR IN "ACCEPTABLE" CONDITION AS DESCRIBED AND PICTURED IN THE CURRENT EDITION OF THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA) MANUAL QUALITY GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES. THESE DEVICES SHALL NOT BE ALLOWED TO FALL BELOW THE "MARGINAL" CONDITION AT ANY TIME DURING THE LIFE OF THE CONTRACT.

ALL SIGNS SHALL BE IN "ACCEPTABLE" CONDITION AS DESCRIBED IN THE ATSSA MANUAL QUALITY GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES THROUGHOUT THE LIFE OF THE CONTRACT. NON-STANDARD SIGN LEGENDS ARE NOT ACCEPTABLE.

RIGID SIGN PANELS SHALL HAVE A MINIMUM MOUNTING HEIGHT OF 7 FEET, MEASURED FROM THE ROADWAY SURFACE TO THE BOTTOM OF THE LOWEST SIGN PANEL. FOR SIGNS INCORPORATING AN AUXILIARY PANEL BELOW THE PRIMARY PANEL, THE MINIMUM MOUNTING HEIGHT SHALL BE 6 FEET, FLEXIBLE PANEL AND LIGHTWEIGHT RIGID PANELS SHALL BE MOUNTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.

REQUIREMENTS FOR PORTABLE VARIABLE MESSAGE SIGNS:

COORDINATE WITH NYSDOT CONTRACT D263784 I-587 ROUNDABOUT FOR PLACEMENT OF PVMS SIGNS.

PORTABLE VARIABLE MESSAGE SIGNS SHALL BE PLACED AT THE LOCATIONS SHOWN IN THE PLANS, AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT SHALL BE MADE UNDER ITEM 619.110511.

PORTABLE VARIABLE MESSAGE SIGNS WILL BE USED TO NOTIFY MOTORISTS AT LEAST TWO WEEKS IN ADVANCE OF THE ANTICIPATED START OF WORK DATE AT EACH LOCATION AND, WHEN APPLICABLE, THE ANTICIPATED START OF EACH SUBSEQUENT STAGE THAT REQUIRES A NEW WORK ZONE TRAFFIC CONTROL PATTERN. THE PYMS SHALL REMAIN IN PLACE UNTIL ALL WORK IS COMPLETED AT A LOCATION OR IN A STAGE, OR AS DIRECTED BY THE ENGINEER. ASSUME DURATION OF CONSTRUCTION AS NOTED.

WHERE TRAFFIC WILL BE RIDING ON MILLED OR GROOVED PAVEMENT AND THE POSTED SPEED LIMIT IS 45 MPH OR GREATER, THE CONTRACTOR SHALL PLACE A PORTABLE VARIABLE MESSAGE SIGN IN ADVANCE OF THE MILLED OR GROOVED PAVEMENT WARNING MOTORCYCLE RIDERS TO USE CAUTION.

THE FOLLOWING IS A SUMMARY OF THE ANTICIPATED NEED FOR PORTABLE VARIABLE MESSAGE SIGNS:

PVMS 1 LOCATION 1: HENRY ST / CLINTON AVE INTERSECTION, SOUTH OF WORK ZONE, FIRST PARKING SPOT ON HENRY ST. DURATION: ENTIRE DURATION OF CONSTRUCTION

PVMS 2 LOCATION 2: BROADWAY, 200 LF EAST OF E/W O'REILLY ST, PARKING SPOT ON BROADWAY DURATION: ENTIRE DURATION OF CONSTRUCTION

PVMS 3-5 LOCATION 3, 4, AND 5 SHALL BE INCLUDED AND PAID FOR UNDER NYSDOT PROJECT D263784. CONTRACTOR SHALL COORDINATE SIGNING WITH D263784. THE CONTRACTOR IS REMINDED THAT, IN ACCORDANCE WITH SECTION 619-3.10 OF THE STANDARD SPECIFICATIONS, PORTABLE VARIABLE MESSAGE SIGNS WITH A PAY UNIT OF EACH SHALL BE RELOCATED OR REORIENTED, IF NECESSARY, UP TO FOUR (4) TIMES PER YEAR AS CONDITIONS DICTATE AT NO ADDITIONAL COST TO THE CITY.

STANDARD (TYPICAL) MESSAGES FOR PORTABLE VARIABLE MESSAGE SIGNS SHALL BE SUPPLIED TO THE CONTRACTOR BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL SUBMIT ANY UNIQUE MESSAGES TO THE ENGINEER FOR APPROVAL BY THE SURFACE TRANSPORTATION CONTROLLER. REQUESTS FOR MESSAGE APPROVAL SHOULD ACCOMPANY THE ROAD WORK FORM DESCRIBED IN THE TEMPORARY LANE AND SHOULDER CLOSURES NOTE BELOW.

ACTIVITY AREAS

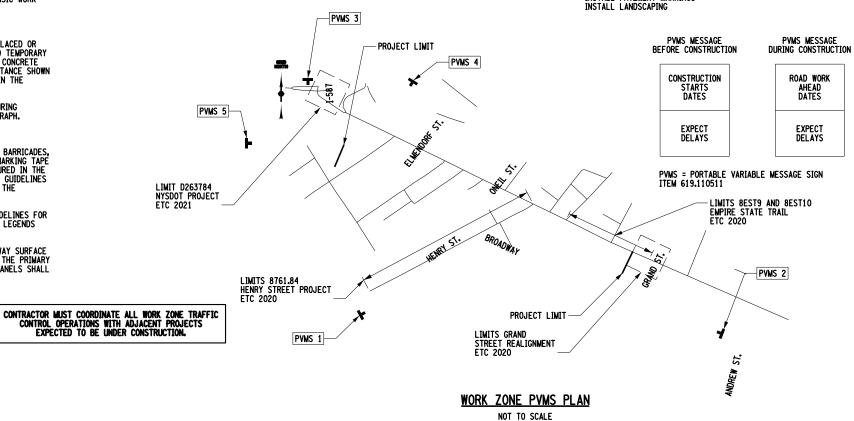
ACTIVITY AREAS SHOULD BE LIMITED TO ONE SIDE OF A ROADWAY AT A TIME UNLESS APPROVED BY THE ENGINEER. UNLESS OTHERWISE APPROVED BY THE ENGINEER, ACTIVITY AREAS ON ALTERNATE SIDES OF A ROADWAY SHALL BE SEPARATED BY A MINIMUM LONGITUDINAL DISTANCE OF 4L, WHERE L IS DEFINED IN TABLE 6H-4 OF THE MUTCD.

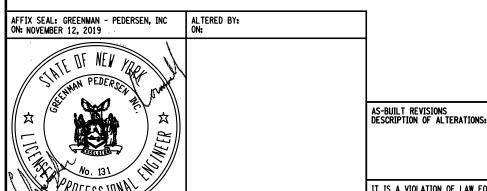
IT MAY BE NECESSARY TO LIMIT THE LENGTH OF A GIVEN ACTIVITY AREA. THE ENGINEER WILL DETERMINE THE MAXIMUM LENGTH OF ACTIVITY AREAS.

WHEN TWO ACTIVITY AREAS ON A ROADWAY ARE SEPARATED BY A SUFFICIENT LONGITUDINAL DISTANCE, ADVANCE WARNING SIGNS SHALL BE PLACED FOR BOTH ACTIVITY AREAS IN ACCORDANCE WITH THE MUTCD OR APPLICABLE WORK ZONE TRAFFIC CONTROL STANDARD SHEET'S). WHEN THE LONGITUDINAL DISTANCE BETWEEN SUCCESSIVE ACTIVITY AREAS IS NOT SUFFICIENT, THE CONTRACTOR SHALL PROVIDE ADEQUATE SIGNAGE TO INFORM ROAD USERS AND REDUCE CONFUSION. THE ENGINEER SHALL DETERMINE THE ADEQUACY OF SIGNAGE IN SUCH CASES.

SUGGESTED CONSTRUCTION SEQUENCE:

INSTALL WORK ZONE TRAFFIC CONTROL
INSTALL EROSION AND SEDIMENT CONTROL MEASURERS
TREE REMOVAL
CLEAN EXISTING DRAINAGE SYSTEMS
PERFORM SIGNAL MODIFICATIONS
PERFORM SIGNAL MODIFICATIONS
PERFORM LIGHTING MODIFICATIONS
INSTALL BIO-SWALES AND NEW CURB
INSTALL CONCRETE SIDEWALKS
INSTALL SIGNS
INSTALL SIGNS
INSTALL PERMEABLE PAVERS
MILL BROADWAY
PAVE BROADWAY
GENERAL CLEANUP
INSTALL PAVEMENT MARKINGS





KINGSTON CONNECTIVITY
BROADWAY STREETSCAPE PROJECT
CITY OF KINGSTON

COUNTY: ULSTER

REGION: 8

CULVERTS

BRIDGES

RTS ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

WORK ZONE TRAFFIC CONTROL

GREENMAN-PEDERSEN, INC

CONSULTING ENGINEERS

CONTRACT NUMBER
DL34880

DRAWING NO. MPN-1 SHEET NO. 8

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



TEMPORARY LANE AND SHOULDER CLOSURES:

IT WILL BE NECESSARY TO TEMPORARILY CLOSE TRAVEL LANES AND/OR SHOULDERS TO PERFORM THE CONTRACT WORK. THE FOLLOWING RESTRICTIONS SHALL APPLY TO LANE AND SHOULDER CLOSURES:

NO KEY CORRIDOR LANE, RAMP AND/OR SHOULDER CLOSURES ARE ANTICIPATED IN THIS CONTRACT.

- THE SAME NUMBER OF TRAVEL LANES (INCLUDING TURNING LANES) AS EXIST PRIOR TO THIS CONTRACT SHALL BE MAINTAINED IN EACH DIRECTION DURING THE HOURS OF 7:00 A.M. TO 9:00 A.M. AND 4:00 P.M. TO 6:00 P.M. MONDAY TO FRIDAY, INCLUSIVE. RAMP AND/OR SHOULDERS SHALL NOT BE CLOSED DURING THESE
- AS DESCRIBED BELOW, THE TIME RESTRICTIONS CAN BE ADJUSTED BY THE ENGINEER AS TRAFFIC CONDITIONS WARRANT.

THE ENGINEER IS AUTHORIZED TO ORDER ADDITIONAL TIME RESTRICTIONS ON ALL ROADS OF UP TO ONE HOUR PER WORK DAY IF TRAFFIC CONDITIONS WARRANT. THERE SHALL BE NO COST TO THE CITY FOR SUCH ADDED RESTRICTIONS. THE CONTRACTOR SHALL NOT HAVE ANY DELAY CLAIMS AGAINST THE CITY IF THE ENGINEER ORDERS UP TO ONE HOUR OF ADDITIONAL TIME RESTRICTIONS PER DAY.

THE ENGINEER IS AUTHORIZED TO REDUCE TIME RESTRICTIONS AS TRAFFIC CONDITIONS WARRANT.

THE FOLLOWING HOLIDAY RESTRICTIONS SUPPLEMENT THE "SPECIAL NOTE - TEMPORARY LANE CLOSURE RESTRICTIONS FOR MAJOR HOLIDAYS (2019 & 2020)". THE CONTRACTOR WILL NOT BE ALLOWED TO IMPLEMENT ANY TEMPORARY LANE, RAMP OR SHOULDER CLOSURES OR OTHERWISE DISRUPT TRAFFIC IN ANY IMPLEMENT ANY TEMPORARY LANE, RAMP OR SHOULDER CLUSURES OR OTHERWISE DISTORT TRAFFIC IN ANY WAY DURING THE FOLLOWING HOLIDAYS IN ADDITION TO THOSE STATED IN TEMPORARY LANE CLOSURES THALL NOT BE PERMITTED UNTIL 10:00 AM, THE DAY FOLLOWING THE HOLIDAY, THIS TIME RESTRICTION SUPERSEDES THOSE TIMES SHOWN IN THE "SPECIAL NOTE - TEMPORARY LANE CLOSURE RESTRICTIONS FOR MAJOR

MOTHERS DAY - SUNDAY, MAY 10. BEGINNING 6:00 AM FRIDAY MAY 8 AND ENDING 10:00 AM MONDAY MAY 11. COLUMBUS DAY - MONDAY, OCTOBER 12. BEGINNING 6:00 AM FRIDAY OCTOBER 9 AND ENDING 10:00 AM

THE CONTRACTOR SHALL BE AWARE OF THE FOLLOWING CITY OF KINGSTON EVENTS WHICH WILL TRAVEL

SAINT PATRICK'S DAY PARADE & SHAMROCK RUN: SUNDAY, MARCH 15. MEMORIAL DAY PARADE: MONDAY, MAY 25. CHILDREN'S DAY PARADE: FRIDAY, JULY 10.

THE CONTRACTOR SHALL SECURE THE WORK ZONE FOR THESE EVENTS AND SHALL HAVE STAFF ON HAND TO RELOCATE EQUIPMENT AS NECESSARY TO ACCOMMODATE THESE EVENTS, PAYMENT SHALL BE INCLUDED IN THE PRICE BID FOR THE BASIC WORK ZONE TRAFFIC CONTROL ITEM.

THE CITY MAY GRANT A WAIVER OF THESE RESTRICTIONS UPON A TIMELY RECEIPT OF A REQUEST FOR SAID WAIVER FROM THE CONTRACTOR. A MINIMUM OF SEVEN (7) CALENDAR DAYS FOR THE REVIEW OF THE CONTRACTOR'S REQUEST

RECOMMENDED PRACTICES FOR CHANNELIZING DEVICES ARE PROVIDED IN THE MUTCD AND SECTION 619-3.02 J OF THE STANDARD SPECIFICATIONS.

TALL CONES, MEETING THE REQUIREMENTS OF SECTION 729-02 OF THE STANDARD SPECIFICATIONS, ARE NORMALLY ADEQUATE FOR WORK ZONES SET UP AND REMOVED ON A DAILY BASIS DURING DAYLIGHT HOURS. DRUMS OR OVERSIZED VERTICAL PANELS ARE PREFERRED FOR ALL OTHER WORK ZONES AND AT ANY LOCATIONS WHERE THE RISK OF INTRUSION IS HIGH, AS DETERMINED BY THE RESIDENT ENGINEER.

THE CONTRACTOR SHALL MAKE FREQUENT CHECKS COMMENSURATE WITH TRAFFIC CONDITIONS TO IDENTIFY AND RESET CHANNELIZING DEVICES DISLODGED BY TRAFFIC.

FLAGGER EQUIPMENT AND STATIONS:

TO INSURE A PROPER LEVEL OF TRAFFIC SAFETY, EACH FLAGGER SHALL BE EQUIPPED WITH A TWO-WAY RADIO DEVICE, THE COST SHALL BE INCLUDED IN THE BASIC WORK ZONE TRAFFIC CONTROL ITEM 619.01.

ALL FLAGGER STATIONS SHALL BE ENHANCED WITH ADDITIONAL CONES AND A FLAG TREE AS SHOWN ON THE WORK ZONE TRAFFIC CONTROL (619 SERIES) STANDARD SHEETS AND AS DIRECTED BY THE ENGINEER. THE FLAG TREE SHALL MEET THE REQUIREMENTS OF SECTION 6F.62 OF THE MUTCD EXCEPT THAT A MINIMUM OF THREE (3) FLAGS ARE REQUIRED. THIS SETUP SHALL BE USED FOR ALL FLAGGER STATIONS EXCEPT THOSE THAT ARE CONSTANTLY ADDITIONAL CONTRACTOR OF THE PROPERTY O MOVING. ALL COSTS ASSOCIATED WITH THESE REQUIREMENTS SHALL BE INCLUDED UNDER THE BASIC WORK ZONE

FLAGGER SIGNS ARE TO BE USED ONLY WHEN A FLAGGER IS ACTUALLY PRESENT AND VISIBLE TO THE MOTORIST. FLAGGER SIGNS SHALL BE COVERED OR REMOVED AT ALL OTHER TIMES.

ENFORCEABLE REDUCED REGULATORY SPEED LIMIT AND ADVISORY SPEED SIGNS (DAILY OPERATION INSTALLATIONS):

ALL REDUCTIONS IN REGULATORY SPEED LIMITS AND ADVISORY SPEEDS ASSOCIATED WITH WORK ZONES MUST BE APPROVED, IN WRITING AND IN ADVANCE, BY THE REGIONAL TRAFFIC ENGINEER OR HIS/HER DESIGNEE.

THE CONTRACTOR MAY REQUEST APPROVAL OF REDUCED REGULATORY SPEED LIMITS AND ADVISORY SPEEDS FOR SHORT-TERM STATIONARY WORK ZONES WHERE SUCH PROVISIONS ARE NOT OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS. REQUESTS MUST BE SUBMITTED THROUGH THE ENGINEER TO THE REGIONAL TRAFFIC ENGINEER A MINIMUM OF FOURTEEN (14) CALENDAR DAYS IN ADVANCE OF THE SCHEDULED IMPLEMENTATION OF ANY WORK ZONE CONTAINING THE REQUESTED REDUCED REGULATORY SPEED LIMITS AND/OR ADVISORY SPEEDS.

GENERALLY, TO QUALIFY FOR A REDUCTION IN REGULATORY SPEED LIMIT AT A SHORT-TERM STATIONARY WORK ZONE, THE WORK ZONE MUST CONTAIN AN ACTIVITY AREA THAT IS GREATER THAN 1/2 MILE ON A HIGHWAY WITH A PRECONSTRUCTION POSTED SPEED LIMIT OF 55 MPH OR GREATER AND BE OF A DURATION EXCEEDING FOUR (4) HOURS WHERE WORKERS ARE ON FOOT IN THE ACTIVITY AREA AND ARE NOT PREDOMINANTLY SEPARATED FROM TRAFFIC BY A POSITIVE BARRIER. IF APPROVED, THE REDUCED REGULATORY SPEED SHALL BE NO MORE THAN 10 MEDIA PELOW THE DRECONSTRUCTION POSTED 1 FOR THE PROPERTY OF THE PROPERTY MPH BELOW THE PRECONSTRUCTION POSTED SPEED LIMIT.

TO QUALIFY FOR AN ADVISORY SPEED AT A SHORT-TERM STATIONARY WORK ZONE, HAZARDOUS WORK ZONE CONDITIONS MUST EXIST THAT WARRANT A LOCALIZED REDUCTION IN SPEED, SUCH CONDITIONS INCLUDE, BUT ARE NOT LIMITED TO, NARROW LANES, BUMPS, GROOVED PAVEMENT, LOW OR NO SHOULDERS, ROADWAY DROP-OFFS, POOR ROADWAY SURFACE, POOR SIGHT DISTANCE, GEOMETRIC CONSTRAINTS AND EXPOSED WORKERS ADJACENT TO ACTIVE

IF A REDUCTION IN REGULATORY SPEED LIMIT AND/OR ADVISORY SPEED IS APPROVED, THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN REGULATORY SPEED LIMIT AND/OR ADVISORY SPEED SIGNS IN ACCORDANCE WITH SECTIONS 619-3.02 H 4 AND 5 OF THE STANDARD SPECIFICATIONS AND THE MUTCD, AND AS DIRECTED BY THE ENGINEER. THESE SIGNS SHALL BE USED IN CONJUNCTION WITH THE LANE CLOSURE OR OTHER WORK ZONE TRAFFIC CONTROL SIGNS AND SHALL BE COVERED OR REMOVED WHEN THE WORK ZONE TRAFFIC CONTROL PATTERN IS REMOVED EACH DAY, ANY EXISTING REGULATORY SPEED LIMIT SIGNS WITHIN THE WORK ZONE SHALL BE COVERED ON THE THAT THE WORK ZONE TRAFFIC CONTROL SIGNS AND SHALL BE COVERED. DURING THE TIME THE WORK ZONE TRAFFIC CONTROL IS IN PLACE AND UNCOVERED WHEN THE WORK ZONE TRAFFIC

REDUCTIONS IN REGULATORY SPEED LIMITS AND ADVISORY SPEEDS SHALL NOT BE PERMITTED FOR MOBILE OR SHORT DURATION WORK ZONES.

THE COST OF UTILIZING APPROVED REGULATORY OR ADVISORY SPEED ZONE SIGNS, AND COVERING EXISTING SIGNS, SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL 619.01.

REPLACEMENT OF GUIDE RAIL, MEDIAN RAIL AND/OR BRIDGE RAIL:

GUIDE RAIL: UNLESS OTHERWISE SHOWN IN THE PLANS OR APPROVED BY THE ENGINEER, THE CONTRACTOR SHALL REMOVE ONLY THE GUIDE RAIL THAT CAN BE REPLACED WITHIN ITHE SAME WORK SHIFT OR X (WHERE X<14) CALENDAR DAYSI. IF GUIDE RAIL IS NOT REPLACED WITHIN THE SPECIFIED TIME THE CONTRACTOR WILL BE ASSESSED LIQUIDATED DAMAGES IN THE AMOUNT OF \$5,000 PER CALENDAR DAY PER LOCATION UNTIL THE GUIDE RAIL IS REPLACED. IN ADDITION, THE ENGINEER MAY DIRECT THE CONTRACTOR TO STOP ALL OTHER WORK UNTIL THE GUIDE RAIL IS REPLACED.

IN ACCORDANCE WITH SECTION 619-3.02 J 6 OF THE STANDARD SPECIFICATIONS, THE CONTRACTOR SHALL USE DRUMS, TALL CONES, EXTRA TALL CONES, TEMPORARY TUBULAR MARKERS, VERTICAL PANELS, OVERSIZED VERTICAL PANELS, TYPE II CONSTRUCTION BARRICADES AND/OR TYPE III CONSTRUCTION BARRICADES TO DELINEATE THE EDGE OF SHOULDER IN LOCATIONS WHERE GUIDE RAIL IS REMOVED. THE CENTER-TO-CENTER SPACING BETWEEN CHANNELIZING DEVICES SHALL NOT EXCEED 80 FEET WHERE THE SHOULDER WIDTH IS 4 FEET OR GREATER, AND SHALL NOT EXCEED 40 FEET WHERE THE SHOULDER WIDTH IS LESS THAN 4 FEET. A MINIMUM OF THREE (3) DEVICES SHALL BE PROVIDED FOR EACH INDIVIDUAL RUN OF GUIDE RAIL THAT IS REMOVED.

TRAFFIC SIGNAL MODIFICATIONS:

REFER TO TRAFFIC SIGNAL NOTES AND TRAFFIC SIGNAL PLANS FOR WZTC WORK INVOLVING THE MODIFICATIONS OF

PUBLIC ACCOMMODATIONS WITHIN CONTRACT LIMITS NOTES:

AT THE END OF EACH WORK DAY, ALL EXCAVATED MATERIAL SHALL BE TAKEN FROM THE SITE TO A LOCATION APPROVED BY THE ENGINEER AND ALL CONSTRUCTION EQUIPMENT, MATERIALS, AND OTHER OBSTRUCTIONS SHALL BE REMOVED FROM DRIVEWAYS, SIDEWALKS OR PEDESTRIAN WAYS THAT ARE OPEN TO TRAFFIC, CONSTRUCTION MATERIALS, VEHICLES, EQUIPMENT, DEBRIS, TEMPORARY SIGN SUPPORTS OR OTHER MATERIALS SHALL NOT BE PLACED OR STORED ON OPEN DRIVEWAYS, SIDEWALKS OR PEDESTRIAN WAYS UNLESS EXPRESSLY SHOWN IN THE CONTRACT DOCUMENTS OR APPROVED BY THE ENGINEER.

UPON COMPLETION OF THE WORK AT EACH LOCATION, THE CONTRACTOR SHALL REMOVE ALL REMAINING MATERIAL AND EQUIPMENT AND SHALL LEAVE THE AFFECTED AREA(S) IN A NEAT CONDITION.

WHERE EXCAVATIONS OR OTHER WORK OCCUR ON OR NEAR SIDEWALKS OR OTHER PEDESTRIAN WAYS, THE CONTRACTOR SHALL PROVIDE A SAFE AND ORDERLY PEDESTRIAN PASSAGE THAT COMPLIES WITH ADA STANDARDS AROUND OR THROUGH THE WORK AREA. THE PEDESTRIAN PASSAGE SHALL NOT SUBJECT PEDESTRIANS TO HAZARDS FROM TRAFFIC OR CONSTRUCTION OPERATIONS NOR CAUSE THE PEDESTRIANS TO WALK UPON UNSUITABLE OR

DRIVEWAY/PUBLIC INGRESS AND EGRESS:

THE CONTRACTOR SHALL PROVIDE BUSINESS OWNERS AND PROPERTY OWNERS WITH PROPER ACCESS TO AND MINIMUM WIDTHS FOR THEIR DRIVEWAYS IN ACCORDANCE WITH THE "POLICY AND STANDARDS FOR ENTRANCES TO STATE HIGHWAYS" AND A.C.L.U. AT ALL TIMES FOR THE DURATION OF THE CONTRACT. DRIVEWAYS SHALL BE MAINTAINED AND DELINEATED BY MEANS OF SIGNS, CONES AND/OR DRUMS SHALL BE INCLUDED IN THE BID PRICE FOR ITEM 619.01 BASIC MAINTENANCE AND PROTECTION OF TRAFFIC

TEMPORARY BUSINESS SIGN NOTE:

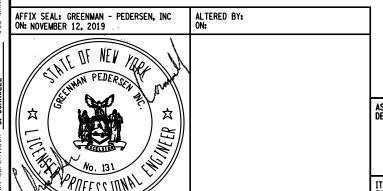
THE CONTRACTOR SHALL PROVIDE AND INSTALL TEMPORARY BUSINESS SIGNS WHILE THE SIDEWALK DETOUR IS IN PLACE AOBE, PAID FOR UNDER ITEM 619.0701. THE RESIDENT ENGINEER SHALL APPROVE THE SIGN LAYOUTS FOR ALL TEMPORARY BUSINESSES PRIOR TO FABRICATION.

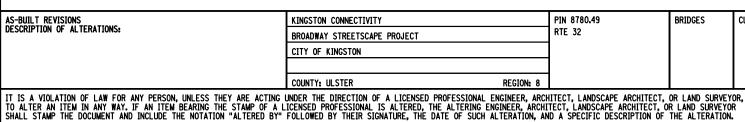
WINTER SHUTDOWN

PRIOR TO WINTER SHUTDOWN. THE CONTRACTOR SHALL BE REQUIRED TO DO THE FOLLOWING:

- A. ANY ROADWAY WHERE PROPOSED DRAINAGE HAS BEEN INSTALLED SHALL BE PAVED TO BINDER COURSE. ASPHALT SHIMS SHALL BE PLACED AROUND EACH DRAINAGE STRUCTURE WHERE BINDER COURSE IS PLACED. COST OF SHIM TO BE INCLUDED IN THE PRICE BID FOR ITEM 619.01.
- B. PROVIDE FULL PAYEMENT DELINEATION USING REFLECTORIZED PAYEMENT MARKING PAINT, CONFORMING TO SECTION 640 OF THE STANDARD SPECIFICATIONS. PAYMENT SHALL BE MADE UNDER ITEM 619.080201 REMOVE PAVEMENT MARKING LETTERS OR SYMBOLS, TRAFFIC PAINT ITEM 699.100101 INTERIM PAVEMENT MARKINGS, STRIPES (TRAFFIC PAINT). ITEM 619.100201 INTERIM PAVEMENT MARKINGS, SYMBOLS (TRAFFIC PAINT), AS APPROVED BY THE E.I.C.
- C. ALL TRAVELWAYS SHALL BE PAVED THROUGH BINDER COURSE, UNDER NO CIRCUMSTANCES WILL TRAFFIC BE MAINTAINED ON SUBBASE OR BASE COURSE, ALL UTILITY TRENCHES SHALL HAVE PERMANENT PAVEMENT PLACED UP TO THE BINDER COURSE.

ALL OTHER MISCELLANEOUS WORK REQUIRED TO PREPARE THE ROADWAY FOR WINTER SHUTDOWN SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.







CULVERTS

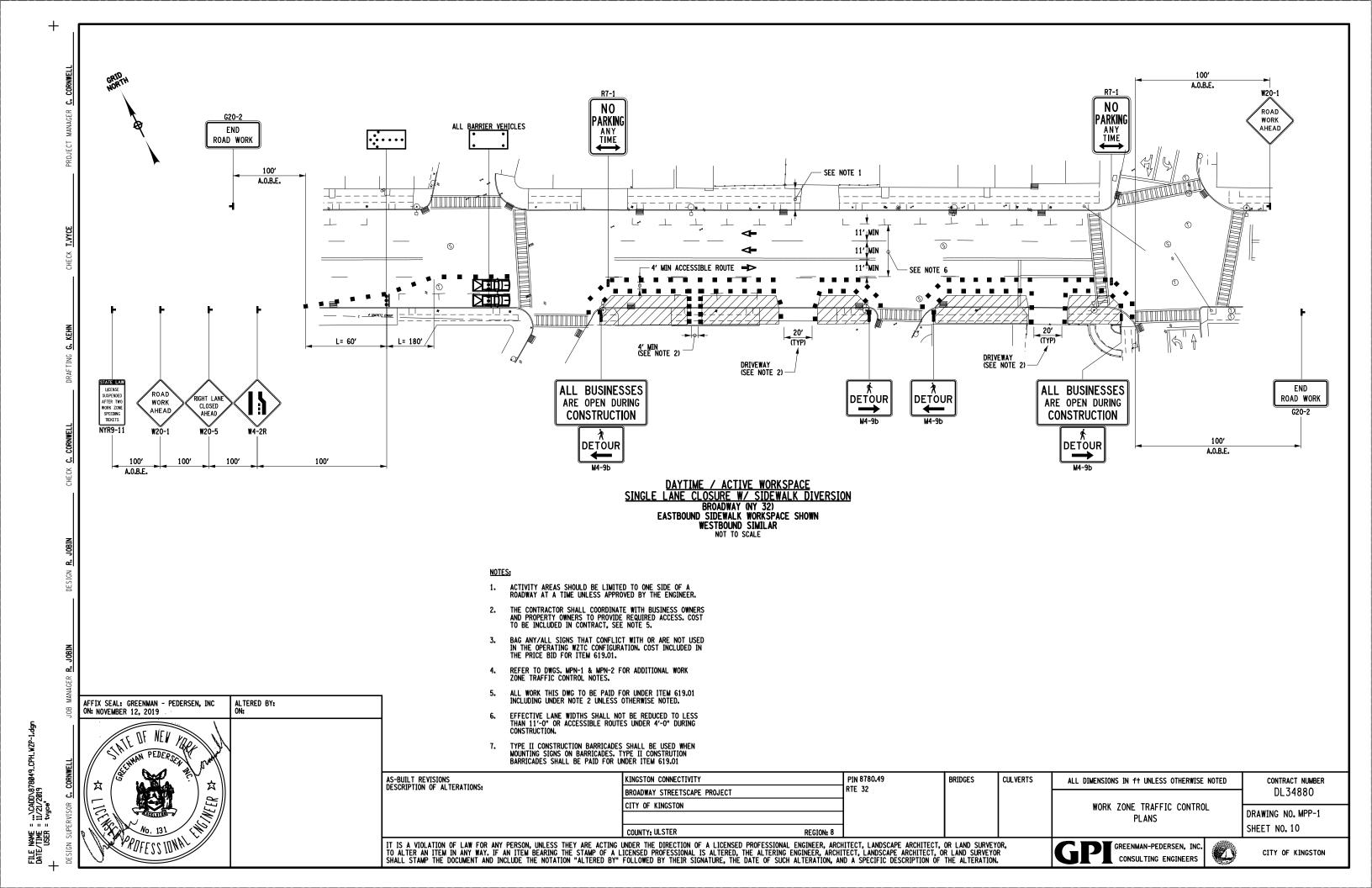
BRIDGES

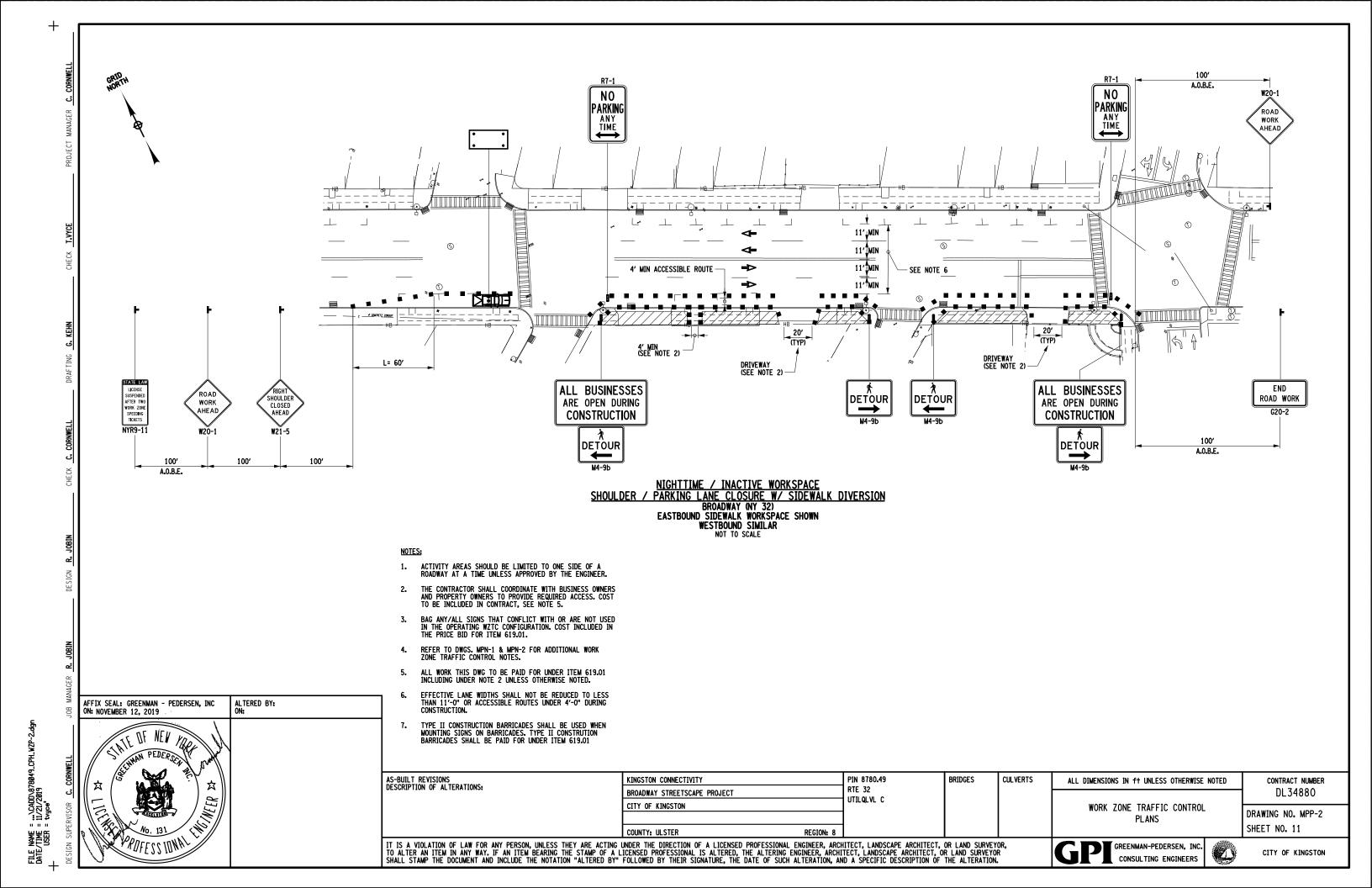


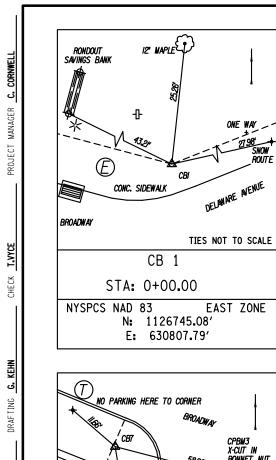
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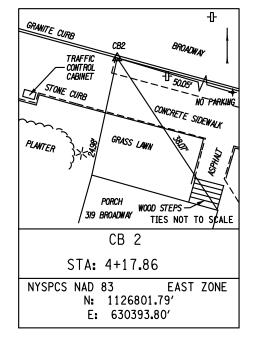


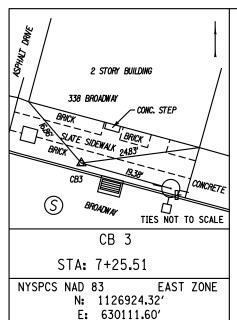
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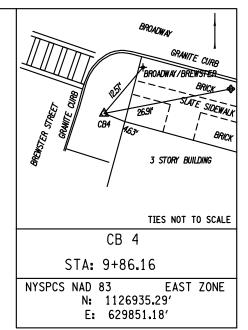


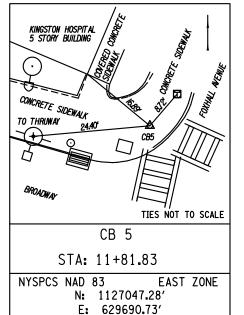


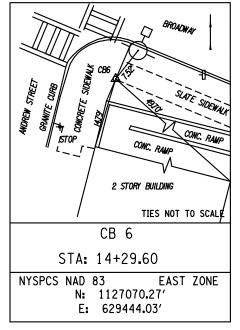


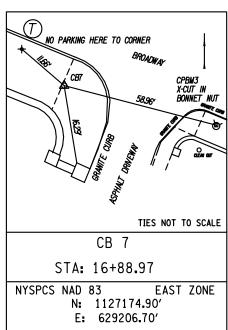


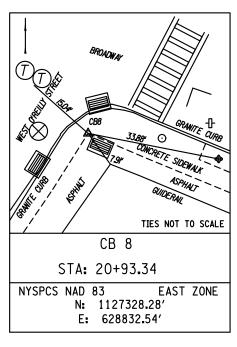


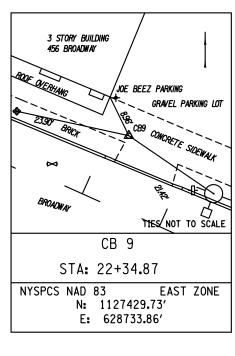


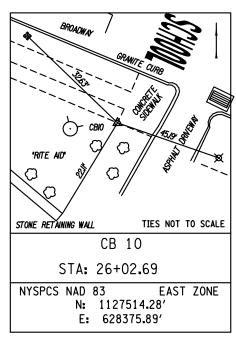


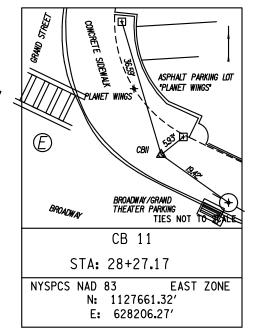


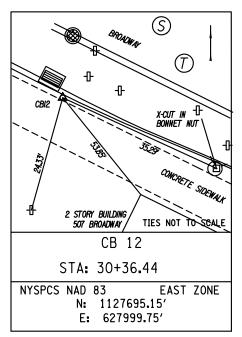


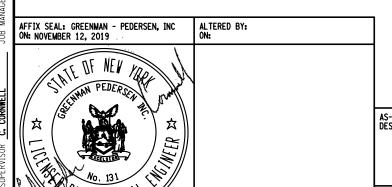






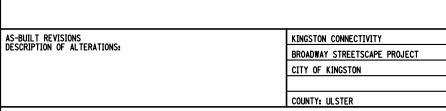






NOTE:

1. SEE DWG CLP-1 THROUGH CLP-6 FOR BASELINE ALIGNMENT.



DWAY STREETSCAPE PROJECT

OF KINGSTON

TY: ULSTER REGION: 8

PIN 8780.49

BRIDGES CULVERTS

VERTS ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

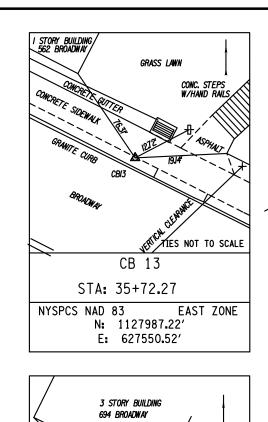
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DL34880

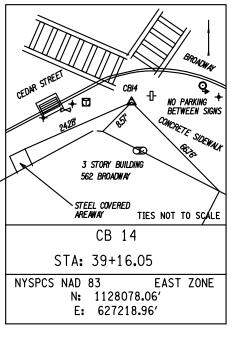
DRAWING NO. BLT-1 SHEET NO. 12

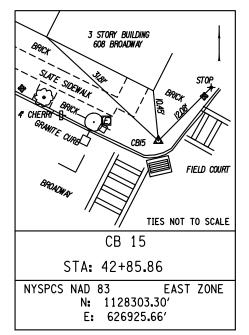
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

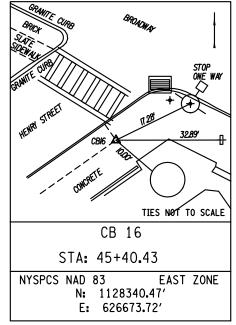
GPI GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

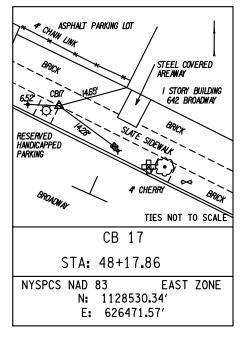
BASELINE TIES

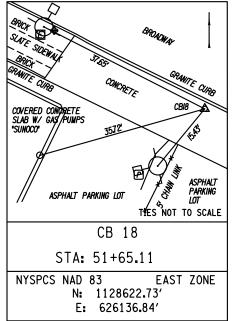


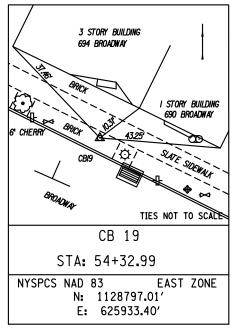


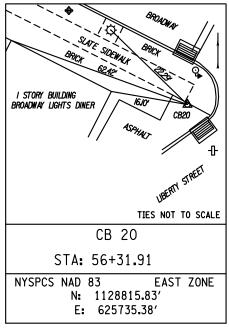


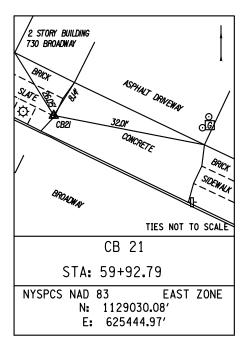


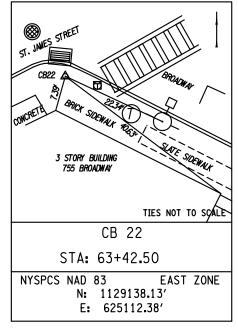


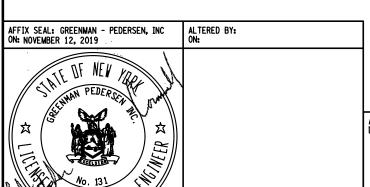












NOTE:

1. SEE DWG CLP-1 THROUGH CLP-6 FOR BASELINE ALIGNMENT.

		AC_DIIT
		AS-BUI Descri
		IT IC

JILT REVISIONS RIPTION OF ALTERATIONS:

KINGSTON CONNECTIVITY BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON

COUNTY: ULSTER

PIN 8780.49 RTE 32

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

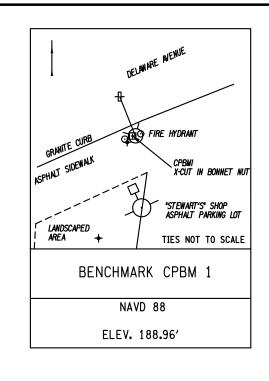
CONTRACT NUMBER DL34880

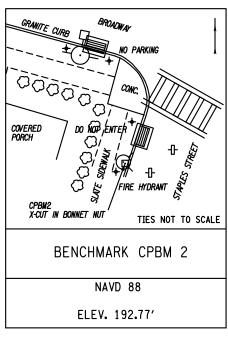
DRAWING NO. BLT-2 SHEET NO. 13

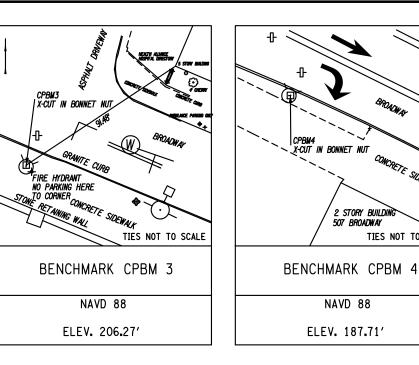
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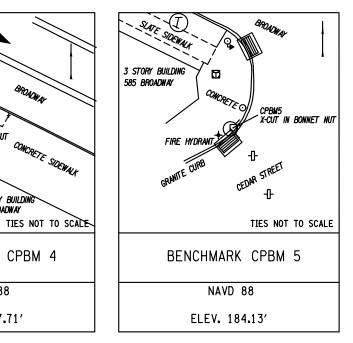


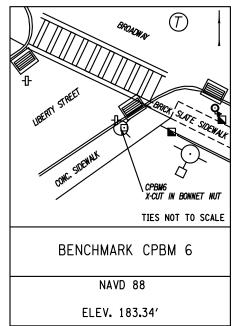
BASELINE TIES

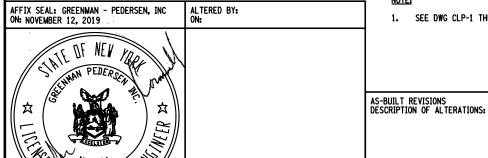












FILE NAME = ...\CADD\878849_CPH_BLT-3.dg DATE/TIME = 11/21/2019 USER = tvyce' NOTE:

1. SEE DWG CLP-1 THROUGH CLP-6 FOR BASELINE ALIGNMENT.

COUNTY: ULSTER REGION: 8

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

KINGSTON CONNECTIVITY

CITY OF KINGSTON

BROADWAY STREETSCAPE PROJECT

PIN 8780.49

RTE 32

BRIDGES

CULVERTS

GP GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

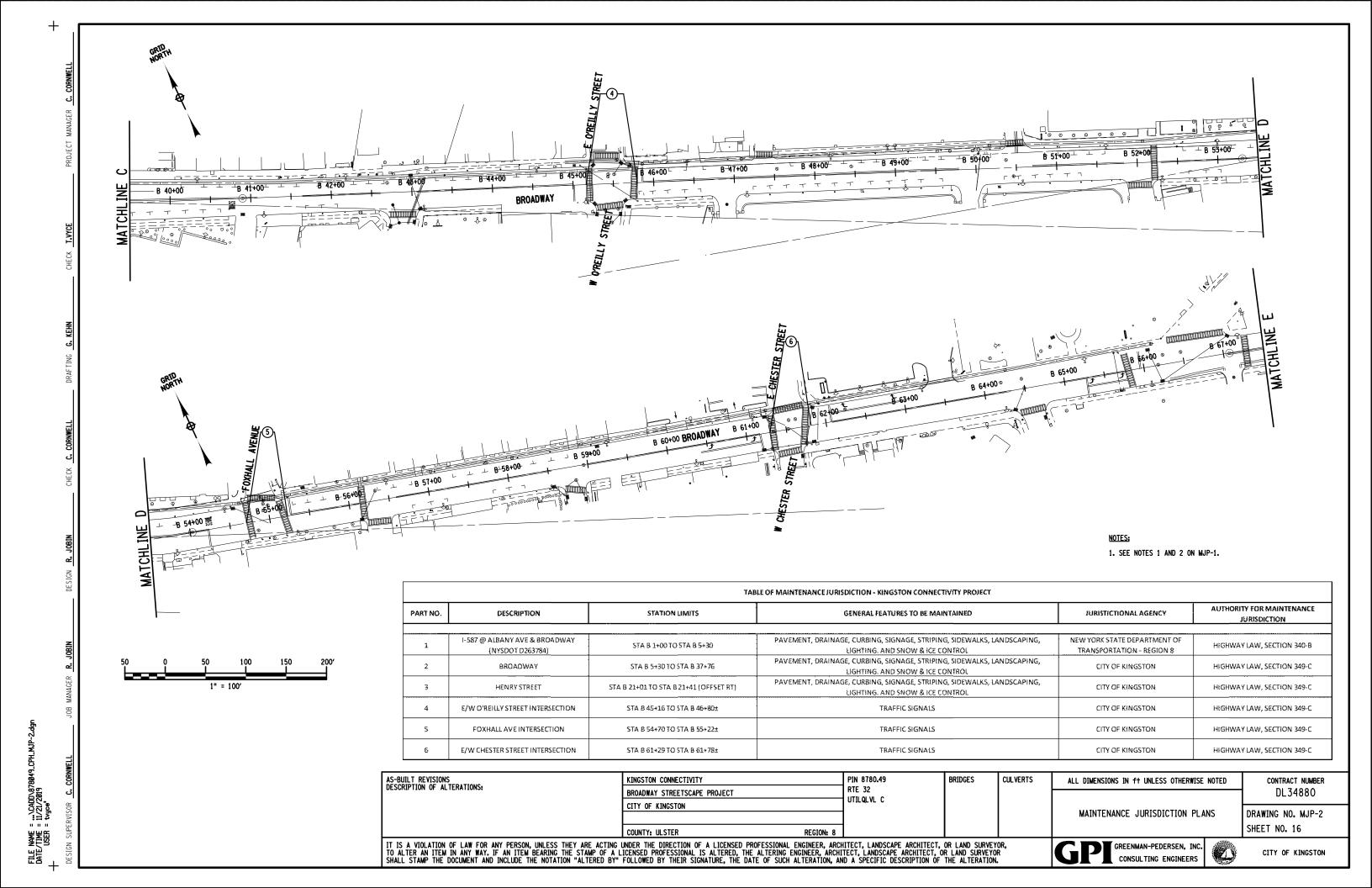
BASELINE TIES

CITY (

CITY OF KINGSTON

DRAWING NO. BLT-3 SHEET NO. 14

CONTRACT NUMBER



E = ...\CADD\878849_CPH_MST-1, E = 11/25/2019 ? = tvuce*

SELWAN PEDERSON

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

UNDERDRAIN

B 6+55 LT B 6+80 RT

B 7+73 LT

B 8+22 RT

B 8+56 LT

B 9+62 RT

B 9+98 LT B 11+57 LT

B 12+41 RT

B 12+73 LT B 13+56 LT

B 13+89 RT

B 14+96 LT

B 16+41 LT B 16+96 RT

B 17+41 LT

B 18+77 LT

B 19+14 RT B 20+25 LT

B 20+23 RT

B 20+56 LT

B 22+60 RT

B 23+01 LT

B 23+13 RT

B 24+96 RT

B 24+15 LT

B 25+82 LT

B 25+80 RT

B 27+03 LT

B 29+27 LT

B 29+50 RT

B 30+66 RT

B 30+67 LT

B 34+17 RT

B 34+19 LT

B 32+22 LT

B 32+59 LT

B 36+49 RT

B 36+33 LT

TOTALS:

- B 32+21 RT

17

11

10

257

15

15

4

B 27+12 RT

- B 22+46 LT

- B 15+84 RT

206.0201

I.D. NO. STATION TO STATION SIDE

UD 1-2 B 5+80

UD 2-4 B 10+27 UD 2-9 B 11+74

UD 2-11 B 12+25

UD 2-16 B 12+75 UD 2-19 B 12+66

UD 2-22 B 13+73

UD 2-26 B 14+51

UD 3-4 | B 15+17

UD 3-10 B 17+00 UD 3-17 B 17+74

UD 3-19 B 18+00

UD 3-22 B 19+05 UD 3-27 B 19+65

UD 4-2 B 20+28

UD 4-6 B 21+31

UD 4-13 B 22+93

UD 4-18 B 23+14

UD 4-15 B 23+30

UD 4-24 B 25+24

UD 5-5 B 25+82

UD 5-6 B 25+82

UD 5-8 B 27+99

UD 5-16 B 29+66

UD 5-17 B 30+02

UD 6-4 B 30+68

UD 6-8 B 31+41

UD 6-9 B 32+22

UD 6-10 B 34+21

UD 6-11 B 34+22

UD 6-12 B 31+13

B 28+00

B 30+70

UD 7-5 B 36+52 - B 37+70 RT

UD 4-8

UD 4-9

UD 5-11

UD 6-5

B 6+10 B 6+85

B 7+12

B 8+08

B 8+50

B 8+86

B 15+87

B 21+55

B 22+53

UD 1-6

UD 1-10

UD 1-18

UD 2-1

ITEM

605.1001

(CY)

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			HEM	ITEM	IIEM	ITEM	HEIVI	IIEM	ITEIN	
I.D. NO.	STATION	SIDE		604.070802	655.0704	655.0705	655.1103	655.1106	655.25010005	DESCRIPTION OF WORK
DN44.4	D F : 07	1.7	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	(EA)	TOD DEDUUD
DM 1-1	B 5+87	LT	1		1					TOP REBUILD
DM 1-3	B 6+24	RT	1				-		1	TOP REBUILD
DM 1-8	B 7+18	LT	1				1		_	TOP REBUILD
DM 1-13	B 7+95	RT		1					1	TOP REBUILD + WALL REPAIR:
DM 1-16	B 8+46	LT	1				1			TOP REBUILD
DM 2-2	B 9+57	LT	1				1			TOP REBUILD
DM 2-32	B 9+97	RT	1						1	TOP REBUILD
DM 2-5	B 10+66	LT	1				1			TOP REBUILD
DM 2-31	B 11+25	RT		1					1	TOP REBUILD + WALL REPAIR:
DM 2-30	B 11+35	RT		1					1	TOP REBUILD + WALL REPAIR:
DM 2-29	B 11+60	RT		1					1	TOP REBUILD + WALL REPAIR:
DM 2-7	B 11+75	RT		1					1	TOP REBUILD + WALL REPAIR:
DM 2-28	B 11+80	LT		1			1			TOP REBUILD + WALL REPAIR:
DM 2-27	B 11+85	LT		1					1	TOP REBUILD + WALL REPAIR
DM 2-10	B 12+25	LT		1			1			TOP REBUILD + WALL REPAIR
DM 2-17	B 13+25	LT		1			1			TOP REBUILD + WALL REPAIR
DM 2-23	B 14+40	LT		1			1			TOP REBUILD + WALL REPAIR:
DM 3-29	B 15+04	LT	1						1	TOP REBUILD
DM 3-2	B 15+40	LT		1			1			TOP REBUILD + WALL REPAIR:
DM 3-5	B 15+90	RT		1					1	TOP REBUILD + WALL REPAIR:
DM 3-32	B 16+45	LT		1			1			TOP REBUILD + WALL REPAIR:
DM 3-9	B 16+95	LT	1				1			TOP REBUILD
DM 3-31	B 17+10	RT	1						1	TOP REBUILD
DM 3-30	B 17+40	RT	1						1	TOP REBUILD
DM 3-13	B 17+75	LT	1				1			TOP REBUILD
DM 3-29	B 19+25	RT		1					1	TOP REBUILD + WALL REPAIR
DM 3-23	B 19+65	RT		1					1	TOP REBUILD + WALL REPAIR:
DM 3-24	B 19+70	LT	1				1			TOP REBUILD
DM 4-28	B 20+20	LT	1						1	TOP REBUILD
DM 4-1	B 20+25	LT	1				1			TOP REBUILD
DM 4-29	B 20+75	LT	_	1					1	TOP REBUILD + WALL REPAIR:
DM 4-30	B 20+90	RT	1	_					1	TOP REBUILD
DM 4-31	B 21+24	RT	1						1	TOP REBUILD
DM 4-3	B 21+30	LT	1						1	TOP REBUILD
DM 4-10	B 22+70	LT	1				1		<u> </u>	TOP REBUILD
DM 4-12	B 23+00	RT	1						1	TOP REBUILD
DM 4-16	B 23+75	LT	1					1	<u> </u>	TOP REBUILD
DM 4-27	B 24+25	LT	1					1		TOP REBUILD
DM 4-25	B 24+70	LT	1					1		TOP REBUILD
DM 5-1		RT	1	1				1	1	
	B 25+80	LT		1			1		1	TOP REBUILD + WALL REPAIR:
DM 5-2 DM 5-23	B 25+80		1	1			1		1	TOP REBUILD + WALL REPAIR: TOP REBUILD
	B 27+25	LT	1							
DM 5-22	B 27+60	LT		1					1	TOP REBUILD + WALL REPAIR:
DM 5-14	B 29+20	RT	1						1	TOP REBUILD
DM 6-16	B 31+07	RT	1						1	TOP REBUILD
DM 6-15	B 31+27	LT	1						1	TOP REBUILD
DM 6-14	B 32+91	LT	1						1	TOP REBUILD
DM 6-7	B 34+20	LT	1			1				TOP REBUILD
DM 7-1	B 36+35	LT	1			1				TOP REBUILD
DM 7-3	B 36+60	LT		1		_		1		TOP REBUILD + WALL REPAIRS
	то	TALS:	30	20	1	2	16	4	27	

DRAINAGE MODIFICATIONS

ITEM

ITEM

ITEM

DRAINAGE MODIFICATION NOTES:

- ITEM 655.0704 FURNISH AND INSTALL NEW CAST FRAME F1, UNMOUNTABLE CURB BOX, RETICULINE GRATE G1.
- ITEM 655.0705 FURNISH AND INSTALL NEW CAST FRAME F2, UNMOUNTABLE CURB BOX, RETICULINE GRATE G2.
- ITEM 655.1103 FURNISH AND INSTALL NEW WELDED FRAME AND RETICULINE GRATE 3.
- ITEM 655.1106 FURNISH AND INSTALL NEW WELDED FRAME AND RETICULINE GRATE 6.
- ITEM 655.25010005 REFURNISH AND REINSTALL EXISTING FRAME AND GRATE ASSEMBLY.

	KINGSTON CONNECTIVITY		PIN 8780.49
	BROADWAY STREETSCAPE PROJECT		RTE 32
	CITY OF KINGSTON		
	COUNTY: ULSTER	REGION: 8	
TING I	INDER THE DIRECTION OF A LICENSED PROFESSIONAL	ENGINEER, AR	CHITECT, LANDSCAL

BRIDGES

CULVERTS ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

18

CONTRACT NUMBER DL34880

DRAWING NO. MST-1 SHEET NO. 17

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



MISCELLANEOUS TABLES

B 32+00	-	В	B 37+72			197	
			TC	TALS:		5397	
	DR	ΔΙΝΙ	AGER	EMOVA	15		
			l l	ITEM		ITEM	_
I.D. NO.	STATI	ON	SIDE			655.1600001	1
1.0.110.	JIAII	OIV	SIDE	(CY)	,,	(EA)	.1
R 1-19	B 08+	00	RT	15		(EA) 1	
R 2-21	B 13+		RT	15		1	
R 2-24	B 14+	-51	RT	15		1	
R 3-12	B 17+	-72	RT	15		1	
R 3-18	B 18+	-68	LT	15		1	
R 3-20	B 19+	-13	RT	15		1	
R 4-5	B 21+	-84	RT	19		1	
R 4-20	B 24+	-41	RT	15		1	
R 5-7	B 27+	-13	RT	15		1	
R 5-15	B 29+	-21	LT	19		1	
R 5-20	B 30+	-65	RT	19		1	
R 5-21	B 30+	-67	LT	15		1	
R 6-2	B 32+	-21	RT	19		1	
R 6-3	B 32+	-25	LT	19		1	
R 6-14A	B 33+	-08	RT	19		1	
R 6-14B	B 33+	-18	RT	19		1	
R 6-6	B 34+	-21	RT	19		1	
R 7-2	B 36+	-52	RT	19		1	

TOTALS: 306

- B 12+00 LT

B 11+50 RT

B 16+61 LT

B 14+22 RT

B 17+50 RT

B 21+00 LT

B 19+50 RT

B 21+00 RT

B 27+50 RT

B 27+50 LT

B 32+00 RT

B 32+00 LT

- B 37+72 RT

B 24+50

687

179

373

533

241

281

260

154

909

111

420

164

704

10

10

B 5+31

B 6+13

B 12+00

B 11+50

B 14+22

B 16+61

B 19+50

B 21+00

B 21+00

B 27+50

B 27+50

B 32+00

H	YDRANT REP	LCAEMENT T	ABLE			
STATION	OFFSET	SIDE	ITEM 663.1301 (EA			
B 11+61	45.9'	RT	1			
B 11+67	31.9'	LT	1			
B 19+49	46.9'	RT	1			
B 20+85	53.9'	RT	1			
B 24+65	43.4'	LT	1			
B 26+07	44.6'	LT	1			
B 27+18	48.1'	RT	1			
B 36+91	30.2'	RT	1			
		TOTAL:	8			
*CONTRA	ACTOR SHALL	COORDINAT	TE HYDRANT			
REPLACEMENTS WITH CITY OF KINGSTON						

ST. JAMES ST. TO ELMENDORF ST.

ST. JAMES ST. TO LIBERTY ST.

ELMENDORF ST. TO DOWNS ST.

LIBERTY ST. TO FRANKLIN ST.

DOWNS ST. TO O'NEIL ST.

VAN BUREN ST. TO VAN DEUSEN ST.

VAN DEUSEN ST. TO HENRY ST.

O'NEIL ST. TO FIELD CT.

HENRY ST. TO CEDAR ST.

FIELD CT. TO CORNELL ST.

CEDAR ST. TO GREENKILL AVE.

CORNELL ST. TO RAILROAD AVE.

GREENKILL AVE. TO PINE GROVE AVE.

RAILROAD AVE. TO DEDERICK ST.

DRAINAGE STRUCTURES									
I.D. NO.	STATION	SIDE	ITEM 203.07	ITEM 206.0201	ITEM 604.300611	ITEM 604.301411	ITEM 655.110		
1.D. NO.	JIAHON	SIDE	(CY)	(CY)	(LF)	(LF)	(EA)		
R 1-19A	B 8+96	RT	11	19.4	6		1		
R 2-21A	B 13+78	RT	11	19.4	6		1		
R 2-24A	B 14+51	RT	11	19.4	6		1		
R 3-12A	B 17+72	RT	11	19.4	6		1		
R 3-18A	B 18+68	LT	11	19.4	6		1		
R 3-20A	B 19+13	RT	11	19.4	6		1		
R 4-5A	B 21+84	RT	13	24.9		6	1		
R 4-20A	B 24+41	RT	11	19.4	6		1		
R 5-7A	B 27+13	RT	11	19.4	6		1		
R 5-15A	B 29+21	LT	13	24.9		6	1		
R 5-20A	B 30+65	RT	13	24.9		6	1		
R 5-21A	B 30+67	LT	11	19.4	6		1		
R 6-2A	B 32+21	RT	13	24.9		6	1		
R 6-3A	B 32+25	LT	13	24.9		6	1		
R 6-14A	B 33+08	RT	13	24.9		6	1		
R6-14B	B 33+18	RT	13	24.9		6	1		
R 6-6A	B 34+21	RT	13	24.9		6	1		
R 7-2A	B 36+52	RT	13	24.9		6	1		
	TO	TALS:	216	399	54	54	18		

I.D. NO.	STATION	SIDE	(C	Y)	(C	Y)	((LF)		LF)	(EA)
R 1-19A	B 8+96	RT	1	1	19	.4		6			1
R 2-21A	B 13+78	RT	1	1	19	.4		6			1
R 2-24A	B 14+51	RT	1	1	19	.4		6			1
R 3-12A	B 17+72	RT	1	1	19	.4		6			1
R 3-18A	B 18+68	LT	1	1	19	.4		6			1
R 3-20A	B 19+13	RT	1	1	19	.4		6			1
R 4-5A	B 21+84	RT	1	3	24	.9				6	1
R 4-20A	B 24+41	RT	1	1	19	.4		6			1
R 5-7A	B 27+13	RT	1	1	19	.4		6			1
R 5-15A	B 29+21	LT	1	3	24	.9				6	1
R 5-20A	B 30+65	RT	1	3	24	.9				6	1
R 5-21A	B 30+67	LT	1	1	19	.4		6			1
R 6-2A	B 32+21	RT	1	3	24	.9				6	1
R 6-3A	B 32+25	LT	1	3	24	.9				6	1
R 6-14A	B 33+08	RT	1	3	24	.9				6	1
R6-14B	B 33+18	RT	1	3	24	.9				6	1
R 6-6A	B 34+21	RT	1	3	24	.9				6	1
R 7-2A	B 36+52	RT	1	3	24	.9				6	1
	то	TALS:	21	.6	39	99		54		54	18
	·				DRAI	NAGEC	LEANIN	IG			
TATION	TO STA	TION	SIDE		EM 03 (LF)	ITE 621.04			DESCR	IPTION OF	WORK

= ...\CADD\878B49_CPH_MST-2.c = 11/25/2019 = tvyce*

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 ALTERED BY:

SIDEWALK TABLE

ΔRFΔ

(SQFT)

1551

376

434

141

188

378

388

335

890

307

236

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876

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286

681

266

716

123

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THICKNESS

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52

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116

24

32

18

106

16

26

100

26

114

24

117

10

48

18

93

85

20

53

20

60

142

12

53

16

56

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135

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23

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36

95

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108

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175

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LOCATION

B 6+55

B 6+83

B 7+39

B 7+49

B 7+75

B 8+05

B 8+57

B 8+83

B 9+99

B 10+23

B 10+55

B 10+73

B 11+79

B 5+59

B 5+85

B 6+85

B 7+11

B 8+25

B 8+49

B 9+66

B 9+76

B 10+24

B 10+42

B 11+35

B 12+44

B 12+64

B 13+17

B 13+37

B 13+97

B 13+58

B 13+70

B 14+23

B 14+39

B 14+95

B 15+15

B 16+50

B 14+55

B 14+78

B 15+42

B 15+66

B 15+81

B 16+17

B 17+12

B 17+42

B 17+71

B 18+79

B 19+01

B 20+76

B 17+66

B 17+98

B 18+52

B 18+84

B 19+25

B 20+18

B 20+44 B 20+87 RT

B 20+44 RT

STATION TO STATION

B 6+55

B 6+83

B 7+39

B 7+75

B 8+57

B 8+83

B 9+99

B 10+23

B 10+55

B 10+73

B 5+59

B 6+85

B 7+11

B 8+25

B 8+49

B 9+66

B 9+76

B 10+42

B 11+59

B 12+44

B 12+64

B 13+17

B 13+37

B 12+16

B 13+58

B 14+23

B 14+95

B 14+39

B 14+55

B 14+78

B 15+42

B 15+66

B 15+81

B 16+17

B 16+89

B 17+71

B 18+79

B 19+01

B 17+40

B 17+66

B 17+98

B 18+52

B 18+84

B 19+49

B 20+18

0	B 30+61
0	B 34+66
0	B 35+00
0	B 35+85
0	B 36+07
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AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

+	-07	В3	6+59	LT		52	344	0.33		5		
								TOTA	LS:	667		ŭ,
						cu	RB TABLE					
	BE	GIN	OFFS			END	OFFSET	SIDE		LENGTH	,	ADIUS
	STA	TION	OFF3		S.	TATION	OFFSEI	SIDE		LLINGIII	K	40103
	B 05	5+41	20.5	3'	Е	3 05+57	12.6'	LT		18'		
	B 05	5+57	12.6	3'	Е	3 05+71	9.4'	LT	T	14'		32°
	B 05	5+71	9.4	'	Е	3 05+83	9.4'	LT	T	12'		
	B 05	5+83	9.4	'	Е	3 05+83	10.5'	LT	T	1'		
	B 05	5+73	19.4	ľ	Е	3 11+06	27.0'	LT	T	533'		
	B 13	L+06	27.0)'	Е	3 11+09	30.0'	LT	T	5'		3°
	B 13	1+09	30.0)'	Е	3 11+08	32.5'	LT		3'		
	B 13	1+08	32.5	7	Е	3 11+21	32.8'	LT		13'		
	B 13	L+07	27.0)'	Е	3 11+69	28.6'	LT		62'		
	B 13	L+69	28.6	5'	Е	3 11+79	36.4'	LT		13'		10°
	B 13	1+79	36.4	!	Е	3 11+80	45.8'	LT		10'		
	B 12	2+15	42.4	!	Е	3 12+27	29.5'	LT		20'		12°
	B 12	2+27	29.5	7	Е	3 16+40	30.0'	LT		413'		
	B 16	6+40	30.0)'	Е	3 16+49	37.6'	LT		14'		10°
	B 16	6+49	37.6	3'	Е	3 16+51	48.6'	LT		11'		
	B 16	6+89	52.7	"	Е	3 17+08	29.5'	LT		34'		20°
	B 17	7+08	30.0)'	Е	3 20+64	30.0'	LT		356'		
	B 20	0+64	30.0)'	Е	3 20+73	37.7'	LT		14'		10°
	B 20)+73	37.7	"	Е	3 20+76	52.6'	LT		15'		
	B 23	L+24	59.3	;'	Е	3 21+51	29.5'	LT		46'		28°
	B 2:	l+51	30.0)'	Е	3 24+21	30.0'	LT		270'		
	B 24	1+21	30.0)'	Е	3 24+27	34.3'	LT		8'		6°
	B 24	1+27	34.3	'	Е	3 24+30	47.5'	LT		14'		
	B 24	1+64	44.1	.'	Е	3 24+62	36.6'	LT		8'		
	B 24	1+62	36.6	5'	Е	3 24+68	30.0'	LT		10'		6°
	B 24	1+68	30.0)'	Е	3 27+02	30.0'	LT		234'		
	B 26	6+49	29.5	3'	Е	3 26+52	32.9'	LT		5'		3°
	B 27	7+02	30.0)'	Е	3 27+21	44.5'	LT	\perp	27'		20°
		7+21	44.5	-		3 27+23	53.5'	LT	4	9'		
		7+61	63.2	-		3 27+60	55.7'	LT	+	8'	_	2E°
		7+60 7+84	55.7 26.4			3 27+84 3 29+23	26.4' 26.4'	LT LT	+	43' 139'	_	25°
		7+84 9+23	26.4			3 29+23	41.4'	LT	+	26'		20°
	D 23	77'23	20.4	•		27742	41.4	L		20		20

SIDEWALK TABLE (CONT.)

ARFA

(SQFT)

359

1397

274

125

214

992

917

977

1626

313

882

2235

2241

993

320

533

141

4351

452

164

2343

348

554

228

THICKNESS

(FT)

0.33

0.50

0.33

0.50

0.33

0.50

0.33

0.00

0.33

0.33

0.50

0.33

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0.50

608.0101

18

13

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12

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11

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28

13

54

29

KINGSTON CONNECTIVITY

BROADWAY STREETSCAPE PROJECT

608.40010008

(SF)

0

0

0

0

Ω

917

0

0

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0

0

0

Ω

Ω

0

0

0

LENGTH

(FT)

10

81

36

25

28

163

108

157

178

24

104

261

184

79

23

78

11

807

12

405

34

85

22

SIDE

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LOCATION

B 21+22

B 22+03

B 22+39

B 22+64

B 22+92

B 25+63

B 27+20

B 23+02

B 23+26

B 24+30

B 27+23

B 29+43

B 28+53

B 28+76

B 29+54

B 29+65

B 37+72

B 30+61

B 34+66

B 35+00

B 35+85

B 36+07

STATION TO STATION

B 21+18

B 22+03

B 22+64

B 22+92

B 24+55

B 25+63

B 21+24

B 23+02

B 23+26

B 24+62

B 27+74

B 28+76

B 29+65

B 29+81

B 30+49

0, 03	0.55	1 32	311	0.55			
				TOTALS	667	!	917
							1
		CU	RB TABLE				1
BEGIN STATION	OFFSET	END STATION	OFFSET	SIDE	LENGTH	RADIUS	
B 05+41	20.5'	B 05+57	12.6'	LT	18'		
B 05+57	12.6'	B 05+71	9.4'	LT	14'	32°	
B 05+71	9.4'	B 05+83	9.4'	LT	12'		
B 05+83	9.4'	B 05+83	10.5'	LT	1'		1
B 05+73	19.4'	B 11+06	27.0'	LT	533'		1
B 11+06	27.0'	B 11+09	30.0'	LT	5'	3°	1
B 11+09	30.0'	B 11+08	32.5'	LT	3'		1
B 11+08	32.5'	B 11+21	32.8'	LT	13'		1
B 11+07	27.0'	B 11+69	28.6'	LT	62'		1
B 11+69	28.6'	B 11+79	36.4'	LT	13'	10°	1
B 11+79	36.4'	B 11+80	45.8'	LT	10'		1
B 12+15	42.4'	B 12+27	29.5'	LT	20'	12°	1
B 12+27	29.5'	B 16+40	30.0'	LT	413'		1
B 16+40	30.0'	B 16+49	37.6'	LT	14'	10°	1
B 16+49	37.6'	B 16+51	48.6'	LT	11'		1
B 16+89	52.7'	B 17+08	29.5'	LT	34'	20°	1
B 17+08	30.0'	B 20+64	30.0'	LT	356'		1
B 20+64	30.0'	B 20+73	37.7'	LT	14'	10°	1
B 20+73	37.7'	B 20+76	52.6'	LT	15'		1
B 21+24	59.3'	B 21+51	29.5'	LT	46'	28°	1
B 21+51	30.0'	B 24+21	30.0'	LT	270'		1
B 24+21	30.0'	B 24+27	34.3'	LT	8'	6°	1
B 24+27	34.3'	B 24+30	47.5'	LT	14'		1
B 24+64	44.1'	B 24+62	36.6'	LT	8'		1
B 24+62	36.6'	B 24+68	30.0'	LT	10'	6°	1
B 24+68	30.0'	B 27+02	30.0'	LT	234'		1
B 26+49	29.5'	B 26+52	32.9'	LT	5'	3°	1
B 27+02	30.0'	B 27+21	44.5'	LT	27'	20°	1
B 27+21	44.5'	B 27+23	53.5'	LT	9'		1
B 27+61	63.2'	B 27+60	55.7'	LT	8'		
B 27+60	55.7'	B 27+84	26.4'	LT	43'	25°	l
B 27+84	26.4'	B 29+23	26.4'	LT	139'		l
D 20 . 22	26 41	D 20 - 42	41 41	I 17 I	261	20°	

B 29+42	41.4'	B 29+44	48.4'	LT	7'	
B 29+82	50.5'	B 30+01	26.4'	LT	35'	20°
B 30+01	26.4'	B 36+42	26.4'	LT	641'	
B 36+42	26.4'	B 36+59	43.6'	LT	27'	18°
B 36+59	43.6'	B 36+59	54.3'	LT	11'	
B 05+43	26.2'	B 09+76	29.0'	RT	433'	
B 09+76	29.0'	B 09+79	32.4'	RT	5'	
B 09+79	32.4'	B 11+11	33.2'	RT	132'	
B 11+11	33.2'	B 11+17	29.8'	RT	7'	
B 11+17	29.8'	B 11+28	29.5'	RT	11'	
B 11+28	29.5'	B 11+34	40.3'	RT	15'	8°
B 11+34	40.3'	B 11+30	47.9'	RT	9'	
B 11+59	46.1'	B 11+66	34.6'	RT	13'	
B 11+66	34.6'	B 11+74	30.0'	RT	10'	10°
B 11+74	30.0'	B 13+85	30.0'	RT	211'	
B 13+85	30.0'	B 13+96	43.7'	RT	20'	12°
B 13+96	43.7'	B 13+95	52.0'	RT	8'	
B 14+39	42.8'	B 14+43	35.8'	RT	8'	
B 14+43	35.8'	B 14+53	30.0'	RT	12'	12°
B 14+53	30.0'	B 17+00	30.0'	RT	247'	
B 17+00	30.0'	B 17+10	47.2'	RT	24'	12°
B 17+10	47.2'	B 17+08	51.4'	RT	5'	
B 17+40	51.5'	B 17+49	34.8'	RT	19'	
B 17+49	34.8'	B 17+58	30.0'	RT	10'	10°
B 17+58	30.0'	B 19+18	30.0'	RT	160'	
B 19+18	30.0'	B 19+25	39.8'	RT	14'	8°
B 19+25	39.8'	B 19+24	43.1'	RT	3'	
B 19+51	41.4'	B 19+54	36.2'	RT	6'	
B 19+54	36.2'	B 19+64	30.0'	RT	13'	12°
B 19+64	30.0'	B 20+67	30.0'	RT	103'	
B 20+67	30.0'	B 20+87	48.3'	RT	30'	20°
B 20+87	48.3'	B 20+87	56.0'	RT	8'	
B 21+01	184.3'	B 21+21	146.7'	RT	43'	
B 21+21	146.7'	B 21+30	105.4'	RT	43'	70°
B 21+30	105.4'	B 21+26	50.8'	RT	55'	
B 21+26	50.8'	B 21+46	30.0'	RT	32'	20°
B 21+46	30.0'	B 27+06	30.0'	RT	560'	
B 27+06	30.0'	B 27+18	50.3'	RT	29'	14°
B 27+74	43.6'	B 27+79	35.7'	RT	10'	
B 27+79	35.7'	B 27+99	25.4'	RT	23'	25°
B 27+99	25.4'	B 37+72	25.0'	RT	973'	

RESETTING CURB TABLE

B 08+85

B 09+12

B 09+99

B 10+55

B 12+44

B 13+03

B 15+38

B 18+52

B 20+60

PIN 8780.49

RTE 32

OFFSET

39.4'

35.7'

37.2'

38.7'

41.1'

41.7'

41.2'

41.5'

41.0'

CURB TABLE (CONT.)

OFFSET

SIDE

LENGTH RADIUS

END

STATION

OFFSET

STATION

LOCATIO	114	COVP	DWS	KAIVIE	IIEIVI	
STATION		RAMP		SIDE/BACK	608.21000003	
(+/-)	SIDE	TYPE	PLACEMENT	CONFIG.	(SY)	
B 11+12 ±	LT	9	6	В	1.1	
B 11+34 ±	RT	8	1	А	1.1	
B 11+61 ±	LT	11	1	Α	1.1	
B 11+68 ±	RT	1	2	Α	1.1	
B 11+77 ±	LT	1	2	Α	1.1	
B 11+87 ±	RT	11	1	Α	1.1	
B 12+18 ±	LT	8	1	Α	1.1	
B 13+95 ±	RT	3	3	Α	2.3	
B 13+96 ±	LT	10	1	Α	1.1	
B 14+43 ±	RT	3	6	Α	1.3	
B 16+48 ±	LT	1	2	Α	1.1	
B 16+92 ±	LT	7	1	А	1.1	
B 17+03 ±	RT	7	1	Α	1.1	
B 17+05 ±	LT	7	1	Α	1.1	
B 17+11 ±	RT	7	1	Α	1.1	
B 17+51 ±	RT	1	2	Α	1.1	
B 19+24 ±	RT	1	2	Α	1.1	
B 19+55 ±	RT	1	2	Α	1.1	
B 20+19 ±	LT	11	1	Α	1.1	
B 20+61 ±	LT	7	1	Α	1.1	
B 20+65 ±	RT	1	1	Α	1.1	
B 20+74 ±	LT	7	2	Α	1.1	
B 20+87 ±	RT	7	1	Α	1.1	
B 21+26 ±	LT	1	1	Α	1.1	
B 21+27 ±	RT	7	1	Α	1.1	
B 21+49 ±	LT	1	1	Α	1.1	
B 21+51 ±	RT	7	1	Α	1.1	
B 24+19 ±	RT	11	1	Α	1.1	
B 24+19 ±	LT	7	1	Α	1.1	
B 24+24 ±	LT	7	2	Α	1.1	
B 24+63 ±	LT	1	2	Α	1.1	
B 26+55 ±	LT	9	6	В	1.1	
B 27+03 ±	RT	7	1	Α	1.1	
B 27+17 ±	LT	4	6	Α	3.7	
B 27+17 ±	RT	1	2	Α	1.1	
B 27+66 ±	LT	1	2	Α	1.1	
B 27+82 ±	RT	7	2	Α	1.1	
B 27+91 ±	RT	7	2	Α	1.1	
B 27+93 ±	LT	11	1	Α	1.1	
B 29+35 ±	LT	1	2	Α	1.1	
B 29+84 ±	LT	8	6	Α	1.1	

CURB RAMP

& DETECTABLE WARNING TABLE

CURB

ITEM

1.1

50.6

TOTAL: *SEE NYSDOT STANDARD SHEET 609 FOR CURB TYPES (DWS) - DETECTABLE WARNING SURFACE

B 36+53 ± LT

ITEM

609.15 (LF

34'

28'

59'

31'

72'

22'

60'

54'

3'

363'

CUL VERTS

SIDE

LT

LT

LT

RT

RT

RT

RT

TOTAL:

BRIDGES

RT

	CITY OF KINGSTON				l
					l
	COUNTY: ULSTER	REGION: 8			
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A L SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	ICENSED PROFESSIONAL IS ALTERED, 1	THE ALTERING ENGINEER, ARCHI	TECT, LANDSCAPE ARCHITECT, OF	R LAND SURVEYOR	₹

BEGIN

STATION

B 08+51

B 08+84

B 09+40

B 10+24

B 11+72

B 14+78

B 17+98

B 20+57

OFFSET

39.1'

35.1'

35.9'

38.0'

41.0'

41.7'

41.3'

41.8'

41.0'

GREENMAN-PEDERSEN, INC CONSULTING ENGINEERS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

CITY OF KINGSTON

DRAWING NO. MST-2 SHEET NO. 18

CONTRACT NUMBER

FILE NAME = ...\CADD\878849_CPH_MST-3.dgr DATE/TIME = 11/25/2019 USER = tvyce'

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 ALTERED BY: ON:

TABLE OF PAVEMENT CUTTING

RT

RT

RT

LT

RT

RT

RT

LT

RT

LT

RT

LT

LT

LT/RT

TOTAL:

FROM STATION

B 5+41

B 5+43

B 6+13

B 11+32

B 11+58

B 11+81

B 12+15

B 13+96 B 14+39

B 16+54 B 16+89

B 17+08 B 19+28

B 19+51

B 20+79

B 20+89

B 21+00

B 21+24

B 24+31

B 24+62

B 27+20 B 27+27

B 27+56

B 27+73

B 29+47

B 29+81

B 36+64

TO STATION

B 11+81

B 11+31

B 6+13

B 11+51

B 13+95

B 12+15

B 16+52

B 14+30

B 17+13

B 16+88

B 20+76

B 17+32

B 19+44

B 20+88

B 21+23

B 21+25

B 27+21

B 24+30

B 24+63

B 27+23

B 27+63

B 27+61

B 29+45

B 37+72

B 29+81

B 36+60

B 37+72

HANDICAP PARKING LOT

ITEM 627.50140008

LENGTH (FT)

657

44

22

290

499

35

322

467

26

18

188

44

37

793

381

34

317

44

241

35

754

179

86

7,357

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IOTES		
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	ŀ	B B
	ŀ	В
	ŀ	<u>в</u> В
	l.	D

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

	SNOW STORAGE TABLE						
L		LENGTH	ITEM 608.13000039	ITEM 608.12			
STATION T	O STATION	SIDE	(FT)	(SY)	(SY)		
B 5+30	B 5+83	LT	53	22	0		
B 5+43	B 5+50	RT	7	4	0		
B 5+55	B 5+59	RT	4	2	0		
B 5+64	B 6+24	LT	60	25	0		
B 5+99	B 6+33	RT	34	20	0		
B 6+50	B 6+55	LT	5	3	0		
B 6+59	B 6+85	RT	26	14	0		
B 6+83	B 6+99	LT	16	9	0		
B 7+11	B 7+14	RT	3	1	0		
B 7+25	B 7+39	LT	14	7	0		
B 7+40 B 7+49	B 7+67	RT	27	16	0		
В 7+49	B 7+61 B 7+75	LT LT	12 9	<u>6</u> 5	0		
B 7+73	B 7+73	RT	23	13	0		
B 8+05	B 8+17	LT	12	6	0		
B 8+22	B 8+25	RT	3	2	0		
B 8+43	B 8+48	LT	5	3	0		
B 8+49	B 8+54	RT	5	3	0		
B 8+53	B 8+57	LT	4	2	0		
B 8+80	B 9+02	RT	22	13	0		
B 8+83	B 8+85	LT	2	1	0		
B 9+11	B 9+19	LT	8	5	0		
B 9+24	B 9+49	LT	25	14	0		
B 9+28	B 9+50	RT	22	12	0		
B 9+54	B 9+63	LT	9	6	0		
B 9+55	B 9+66	RT	11	6	0		
B 9+76	B 10+24	RT	48	9	0		
B 9+89	B 10+00	LT	11	6	0		
B 10+24	B 10+38	LT	14	8	0		
B 10+42	B 11+37	RT	95	27	0		
B 10+43	B 10+55	LT	12	7	0		
B 10+73	B 10+93	LT	20	11	0		
B 10+98	B 11+08	LT	10	6	0		
B 11+26	B 11+59	LT	33	15	0		
B 11+26	B 11+35	RT	9	6	0		
B 11+64	B 11+76	LT	12	5	0		
B 11+69	B 11+85	RT	16	6	0		
B 11+90	B 11+95	RT	5	3	0		
B 12+20	B 12+32	LT	12	7	0		
B 12+21	B 12+27	RT	6	3	0		
B 12+32	B 12+44	RT LT	12 14	5 9	0		
B 12+59 B 12+64	B 12+73 B 12+88	RT	24	13	0		
	B 13+17		24	13	0		
B 12+93 B 12+99	B 13+30	LT	31	13	0		
B 13+37	B 13+58	RT	21	12	0		
B 13+56	B 13+58	LT	2	1	0		
B 13+70	B 13+94	LT	24	12	0		
B 13+76	B 13+89	RT	13	7	0		
B 13+99	B 14+09	LT	10	6	0		
B 14+14	B 14+23	LT	9	5	0		
B 14+39	B 14+41	LT	2	1	0		
B 14+45	B 14+55	RT	10	3	0		
B 14+67	B 14+95	LT	28	16	0		
B 14+78	B 15+10	RT	32	18	0		
B 15+15	B 15+40	LT	25	14	0		
B 15+15	B 15+42	RT	27	15	0		
B 15+66	B 15+84	LT	18	10	0		
B 15+66	B 15+81	RT	15	7	0		
B 16+10	B 16+21	LT	11	7	0		
B 16+17	B 16+63	RT	46	26	0		
B 16+26	B 16+47	LT	21	6	0		

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

BROADWAY STREETSCAPE PROJECT

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

JIAHONT	USTATION	SIDE	(F1)	(SY)	(SY)
B 16+89	B 16+96	LT	7	4	0
B 16+89	B 16+99	RT	10	6	0
B 16+95	B 17+03	LT	8	4	0
B 17+04	B 17+11	RT	7	3	0
B 17+08	B 17+13	LT	5	3	0
B 17+39	B 17+42	LT	3	2	0
B 17+54	B 17+69	RT	15	4	0
B 17+71	B 17+82	LT	11	7	0
B 18+01	B 18+06	RT	5	3	0
B 18+08	B 18+20	LT	12	7	0
B 18+11	B 18+42	RT	31	17	0
B 18+46	B 18+79	LT	33	18	0
B 18+47	B 18+52	RT	5	3	0
		_			
B 18+84	B 19+04	RT	20	11	0
B 19+01	B 19+08	LT	7	3	0
B 19+10	B 19+23	RT	13	6	0
B 19+34	B 19+71	LT	37	21	0
B 19+56	B 19+70	RT	14	6	0
B 19+75	B 20+10	RT	35	20	0
B 19+97	B 20+16	LT	19	12	0
B 20+15	B 20+18	RT	3	2	0
B 20+21	B 20+49	LT	19	17	0
B 20+44	B 20+61	RT	17	9	0
B 20+52	B 20+59	LT	7	3	0
B 20+64	B 20+74	LT	10	8	0
B 20+69	B 20+86	RT	17	5	0
B 21+27	B 21+28	RT	1	11	0
		RT	1	18	0
B 21+27	B 21+28				
B 21+27	B 21+28	RT	1	7	0
B 21+28	B 21+48	RT	20	17	0
B 21+58	B 21+89	RT	31	12	0
B 21+87	B 22+12	LT	25	14	0
B 21+94	B 22+03	RT	9	3	0
B 22+38	B 22+46	LT	8	5	0
B 22+39	B 22+64	RT	25	7	0
B 22+51	B 22+70	LT	19	11	0
B 22+92	B 24+16	RT	124	34	0
B 22+96	B 23+01	LT	5	3	0
B 23+26	B 23+79	LT	53	29	0
B 24+05	B 24+16	LT	11	6	0
B 24+21	B 24+27	LT	6	2	0
B 24+21	B 24+64	RT	43	21	0
B 24+63	B 24+89	LT	26	14	0
B 24+94	B 25+14	LT	20	12	0
B 25+40	B 25+73	LT	33	18	0
B 25+66			77	43	0
	B 26+43	RT			
B 25+78	B 25+94	LT	16	9	0
B 25+99	B 26+17	LT	18	11	0
B 26+46	B 26+52	LT	6	5	0
B 26+62	B 26+74	RT	12	6	0
B 26+69	B 27+04	LT	35	10	0
B 26+79	B 27+00	RT	21	10	0
B 27+05	B 27+16	RT	11	5	0
B 27+67	B 27+91	LT	24	13	0
B 27+83	B 27+90	RT	7	3	0
B 27+95	B 28+19	RT	24	15	0
B 27+96	B 28+40	LT	44	25	0
B 28+45	B 28+53	RT	8	4	0
B 28+66	B 28+90	LT	24	13	0
B 28+76	B 28+92	RT	16	9	0
B 29+16	B 29+34	LT	18	8	0
B 29+18	B 29+54	RT	36	20	0

PIN 8780.49

RTE 32

BRIDGES

CULVERTS

SNOW STORAGE TABLE (CONT.)

LENGTH

STATION TO STATION | SIDE | (FT)

608.13000039

	ITEM			L	OCATION			LENGTH		ITEN
	608.12 (SY)		STATIO	N T	O STATIO	N	SIDE	LENGTH (FT)	608.	
	0		B 29+65		B 30+	03	RT	38		21
	0		B 29+96		B 30+		LT	24		13
	0		B 30+08	;	B 30+	37	RT	29		16
	0		B 30+46	;	B 30+	49	LT	3		2
	0		B 30+61		B 31+	03	LT	42		24
	0		B 30+63		B 33+	28	RT	265		148
	0		B 31+29	1	B 33+	17	LT	188		104
	0		B 32+43		B 34+	23	LT	180		44
	0		B 33+54		B 34+	24	RT	70		39
	0		B 34+49	1	B 34+	66	LT	17		10
	0		B 34+50)	B 34+	88	RT	38		22
	0		B 34+93	1	B 35+	48	RT	55		31
	0		B 35+00)	B 35+	27	LT	27		15
	0		B 35+53		B 35+	85	LT	32		18
	0		B 35+74		B 36+	17	RT	43		24
	0		B 36+07	'	B 36+	51	LT	44		22
	0		B 36+09	1	B 37+	72	RT	163		51
	0		B 36+22		B 36+	55	RT	33		19
	0		B 24+35		B 24+	64	RT	29		0
	0		B 24+69	1	B 24+	76	RT	7		0
	0		B 24+82	!	B 24+		RT	16		0
	0		B 24+81		B 24+	88	RT	7		0
_	0		B 24+93		B 24+		RT	5		0
_	0		B 25+24		B 25+		RT	24		0
_	0		B 25+24		B 25+		RT	7		0
_	0		B 25+36		B 25+		RT	14		0
	0		B 25+55		B 25+	63	RT	8		0
	0	L						TOTALS:	1	.837
_	0									
_	0									
_	0					HOF	RIZONTA	L CONTR	OL TAE	LE
	0		H.C.L.		H.C.L.		COOR	DINATES		1
_	0		POINT	S	TATION	N	ORTH	EAS	Τ	
	0						BR	OADWAY		_
	0		P.O.B.	В	1+00.00	1129	9309.800	62483	1.054	
	0		P.C.	_	38+25.82		7647.321	+		
	0		P.I.	В:	39+57.88	1127	7588.395	62828	3.595	
	0			_						_
_	0		P.T.	_	40+89.85		7537.392			L
	0		P.C.	-	53+30.45		7058.269			_
	0		P.I.	В:	53+66.64	1127	7044.290	62958	3.140	
	0									_
_	0		P.T.	_	54+02.73		7034.807			_
_	0		P.C.		57+07.60		692.952	_		L
	0		P.I.	В	58+16.89	1126	6664.317	63098	2.837	
	0					4404		50400		_
_	0		P.T.	_	59+25.59		617.098			
	0		P.O.E.	B.	70+73.54	1126	5553.178	63121	+.841	
	0									
	0									
	0									
	0									
	0									
	_	l								

	SNC	w stor	RAGE TABL	E (CONT.)	
	OCATION O STATION	SIDE	LENGTH (FT)	ITEM 608.13000039 (SY)	ITEM 608.12 (SY)
B 29+65	B 30+03	RT	38	21	0
B 29+96	B 30+20	LT	24	13	0
B 30+08	B 30+37	RT	29	16	0
B 30+46	B 30+49	LT	3	2	0
B 30+61	B 31+03	LT	42	24	0
B 30+63	B 33+28	RT	265	148	0
B 31+29	B 33+17	LT	188	104	0
B 32+43	B 34+23	LT	180	44	0
B 33+54	B 34+24	RT	70	39	0
B 34+49	B 34+66	LT	17	10	0
B 34+50	B 34+88	RT	38	22	0
B 34+93	B 35+48	RT	55	31	0
B 35+00	B 35+27	LT	27	15	0
B 35+53	B 35+85	LT	32	18	0
B 35+74	B 36+17	RT	43	24	0
B 36+07	B 36+51	LT	44	22	0
B 36+09	B 37+72	RT	163	51	0
B 36+22	B 36+55	RT	33	19	0
B 24+35	B 24+64	RT	29	0	16
B 24+69	B 24+76	RT	7	0	4
B 24+82	B 24+98	RT	16	0	13
B 24+81	B 24+88	RT	7	0	4
B 24+93	B 24+98	RT	5	0	3
B 25+24	B 25+48	RT	24	0	13
B 25+24	B 25+31	RT	7	0	4
B 25+36	B 25+50	RT	14	0	8
B 25+55	B 25+63	RT	8	0	4
			TOTALS:	1837 SY	68 SY

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

GREENMAN-PEDERSEN, INC.

CONSULTING ENGINEERS

DESCRIPTION

BEGIN ALIGNMENT P.C. CURVE 1

P.I. CURVE 1 (R = 4000.00)

P.T. CURVE 1

P.C. CURVE 2

P.I. CURVE 2 (R = 550.00)

P.T. CURVE 2

P.C. CURVE 3

P.I. CURVE 3 (R = 1200.00)

P.T. CURVE 3 END ALIGNMENT

CONTRACT NUMBER

DL34880

CITY OF KINGSTON

DRAWING NO. MST-3 SHEET NO. 19

			STATION	OFFSET	SIDE	NOTES	680.82254310 (EA)			
AFFIX SEAL: GREENMAN - PEDERSEN. INC	ALTERED BY:	7	B 13+87	32.5' 32.0'	RT LT	SOLAR POWERED SOLAR POWERED	1			
ON: NOVEMBER 12, 2019	ON:		B 16+95	31.8'	RT	SOLAR POWERED	1			
		1	B 10+33	32.5'	LT	SOLAR POWERED	1			
IF NEW YOU			B 24+12	31.2'	RT	SOLAR POWERED	1			
CIMIL DEDICES	J		B 24+14	32.3'	LT	SOLAR POWERED	1			
ETHAN TOWN										_
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		Ē	KINGSTON CON BROADWAY STI CITY OF KING	REETSCAPE PROJECT		PIN 8780.49 RTE 32	BRIDGES	CUL
				E	BROADWAY ST	REETSCAPE PROJECT STON	REGION: 8	RTE 32	BRIDGES	CULV

DWY-19 B 17+42 B 17+70 LT AC C 28' 90° 13.5' 0.0 DWY-20 B 17+66 B 17+98 RT AC C 32' 90° 11.2' 0.0 DWY-21 B 18+52 B 18+84 RT AC C 32' 90° 10.7' 0.0 DWY-22 B 18+79 B 19+01 LT AC C 22' 90° 13.6' 0.0 DWY-23 B 20+18 B 20+44 RT AC C 26' 90° 10.4' 0.0 DWY-24 B 22+03 B 22+39 RT AC C 36' 90° 10.4' 0.0 DWY-25 B 22+64 B 22+92 RT AC C 28' 90° 10.6' 0.0 DWY-26 B 23+02 B 23+26 LT AC C 24' 90° 12.7' 0.0 DWY-27 B 28+53 B 28+77 RT AC C <th>DWY-17</th> <th>B 15+42</th> <th>B 15+66</th> <th>RT</th> <th>AC</th> <th>С</th> <th>24'</th> <th>90°</th> <th>11.3'</th> <th>0.0</th>	DWY-17	B 15+42	B 15+66	RT	AC	С	24'	90°	11.3'	0.0
DWY-20 B 17+66 B 17+98 RT AC C 32' 90° 11.2' 0.0 DWY-21 B 18+52 B 18+84 RT AC C 32' 90° 10.7' 0.0 DWY-22 B 18+79 B 19+01 LT AC C 22' 90° 13.6' 0.0 DWY-23 B 20+18 B 20+44 RT AC C 26' 90° 10.4' 0.0 DWY-24 B 22+03 B 22+39 RT AC C 36' 90° 10.4' 0.0 DWY-25 B 22+64 B 22+92 RT AC C 28' 90° 10.6' 0.0 DWY-25 B 28+53 B 28+77 RT AC C 24' 90° 12.7' 0.0 DWY-27 B 28+53 B 28+77 RT AC C 24' 90° 13.3' 0.0 DWY-28 B 30+49 B 30+61 LT AC C <td>DWY-18</td> <td>B 15+81</td> <td>B 16+17</td> <td>RT</td> <td>AC</td> <td>С</td> <td>36'</td> <td>90°</td> <td>11.2'</td> <td>0.0</td>	DWY-18	B 15+81	B 16+17	RT	AC	С	36'	90°	11.2'	0.0
DWY-21 B 18+52 B 18+84 RT AC C 32' 90° 10.7' 0.0 DWY-22 B 18+79 B 19+01 LT AC C 22' 90° 13.6' 0.0 DWY-23 B 20+18 B 20+44 RT AC C 26' 90° 10.4' 0.0 DWY-24 B 22+03 B 22+39 RT AC C 36' 90° 10.4' 0.0 DWY-25 B 22+64 B 22+92 RT AC C 28' 90° 10.6' 0.0 DWY-26 B 23+02 B 23+26 LT AC C 24' 90° 12.7' 0.0 DWY-27 B 28+53 B 28+77 RT AC C 24' 90° 13.3' 0.0 DWY-28 B 29+54 B 29+65 RT AC C 11' 90° 15.6' 0.0 DWY-30 B 34+66 B 35+00 LT AC C <td>DWY-19</td> <td>B 17+42</td> <td>B 17+70</td> <td>LT</td> <td>AC</td> <td>С</td> <td>28'</td> <td>90°</td> <td>13.5'</td> <td>0.0</td>	DWY-19	B 17+42	B 17+70	LT	AC	С	28'	90°	13.5'	0.0
DWY-22 B 18+79 B 19+01 LT AC C 22' 90° 13.6' 0.0 DWY-23 B 20+18 B 20+44 RT AC C 26' 90° 10.4' 0.0 DWY-24 B 22+03 B 22+39 RT AC C 36' 90° 10.4' 0.0 DWY-25 B 22+64 B 22+92 RT AC C 28' 90° 10.6' 0.0 DWY-26 B 23+02 B 23+26 LT AC C 24' 90° 12.7' 0.0 DWY-27 B 28+53 B 28+77 RT AC C 24' 90° 13.3' 0.0 DWY-28 B 29+54 B 29+65 RT AC C 11' 90° 12.1' 0.0 DWY-29 B 30+49 B 30+61 LT AC C 12' 90° 13.8' 0.0 DWY-30 B 34+66 B 35+00 LT AC C <td>DWY-20</td> <td>B 17+66</td> <td>B 17+98</td> <td>RT</td> <td>AC</td> <td>С</td> <td>32'</td> <td>90°</td> <td>11.2'</td> <td>0.0</td>	DWY-20	B 17+66	B 17+98	RT	AC	С	32'	90°	11.2'	0.0
DWY-23 B 20+18 B 20+44 RT AC C 26' 90° 10.4' 0.0 DWY-24 B 22+03 B 22+39 RT AC C 36' 90° 10.4' 0.0 DWY-25 B 22+64 B 22+92 RT AC C 28' 90° 10.6' 0.0 DWY-26 B 23+02 B 23+26 LT AC C 24' 90° 12.7' 0.0 DWY-27 B 28+53 B 28+77 RT AC C 24' 90° 13.3' 0.0 DWY-28 B 29+54 B 29+65 RT AC C 11' 90° 12.1' 0.0 DWY-29 B 30+49 B 30+61 LT AC C 12' 90° 15.6' 0.0 DWY-30 B 34+66 B 35+00 LT AC C 34' 90° 13.4' 0.0 DWY-31 B 35+85 B 36+07 LT AC C <td>DWY-21</td> <td>B 18+52</td> <td>B 18+84</td> <td>RT</td> <td>AC</td> <td>С</td> <td>32'</td> <td>90°</td> <td>10.7'</td> <td>0.0</td>	DWY-21	B 18+52	B 18+84	RT	AC	С	32'	90°	10.7'	0.0
DWY-24 B 22+03 B 22+39 RT AC C 36' 90° 10.4' 0.0 DWY-25 B 22+64 B 22+92 RT AC C 28' 90° 10.6' 0.0 DWY-26 B 23+02 B 23+26 LT AC C 24' 90° 12.7' 0.0 DWY-27 B 28+53 B 28+77 RT AC C 24' 90° 13.3' 0.0 DWY-28 B 29+54 B 29+65 RT AC C 11' 90° 12.1' 0.0 DWY-29 B 30+49 B 30+61 LT AC C 12' 90° 15.6' 0.0 DWY-30 B 34+66 B 35+00 LT AC C 34' 90° 13.8' 0.0 DWY-31 B 35+85 B 36+07 LT AC C 22' 90° 13.4' 0.0 DWY-32 B 21+13 B 21+24 RT AC C <td>DWY-22</td> <td>B 18+79</td> <td>B 19+01</td> <td>LT</td> <td>AC</td> <td>С</td> <td>22'</td> <td>90°</td> <td>13.6'</td> <td>0.0</td>	DWY-22	B 18+79	B 19+01	LT	AC	С	22'	90°	13.6'	0.0
DWY-25 B 22+64 B 22+92 RT AC C 28' 90° 10.6' 0.0 DWY-26 B 23+02 B 23+26 LT AC C 24' 90° 12.7' 0.0 DWY-27 B 28+53 B 28+77 RT AC C 24' 90° 13.3' 0.0 DWY-28 B 29+54 B 29+65 RT AC C 11' 90° 12.1' 0.0 DWY-29 B 30+49 B 30+61 LT AC C 12' 90° 15.6' 0.0 DWY-30 B 34+66 B 35+00 LT AC C 34' 90° 13.8' 0.0 DWY-31 B 35+85 B 36+07 LT AC C 22' 90° 13.4' 0.0 DWY-32 B 21+13 B 21+24 RT AC C 24' 90° 16.9' 0.0 DWY-33 B 5+59 B 5+85 RT AC C	DWY-23	B 20+18	B 20+44	RT	AC	С	26'	90°	10.4'	0.0
DWY-26 B 23+02 B 23+26 LT AC C 24' 90° 12.7' O. DWY-27 B 28+53 B 28+77 RT AC C 24' 90° 13.3' O. DWY-28 B 29+54 B 29+65 RT AC C 11' 90° 12.1' O. DWY-29 B 30+49 B 30+61 LT AC C 12' 90° 15.6' O. DWY-30 B 34+66 B 35+00 LT AC C 34' 90° 13.8' O. DWY-31 B 35+85 B 36+07 LT AC C 22' 90° 13.4' O. DWY-32 B 21+13 B 21+24 RT AC C 24' 90° 16.9' O. DWY-33 B 5+59 B 5+85 RT AC C 26' 90° 10.4' O. DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITIO	DWY-24	B 22+03	B 22+39	RT	AC	С	36'	90°	10.4'	0.0
DWY-27 B 28+53 B 28+77 RT AC C 24' 90° 13.3' 0.0 DWY-28 B 29+54 B 29+65 RT AC C 11' 90° 12.1' 0.0 DWY-29 B 30+49 B 30+61 LT AC C 12' 90° 15.6' 0.0 DWY-30 B 34+66 B 35+00 LT AC C 34' 90° 13.8' 0.0 DWY-31 B 35+85 B 36+07 LT AC C 22' 90° 13.4' 0.0 DWY-32 B 21+13 B 21+24 RT AC C 24' 90° 16.9' 0.0 DWY-33 B 5+59 B 5+85 RT AC C 26' 90° 10.4' 0.0 DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF	DWY-25	B 22+64	B 22+92	RT	AC	С	28'	90°	10.6'	0.0
DWY-28 B 29+54 B 29+65 RT AC C 11' 90° 12.1' O DWY-29 B 30+49 B 30+61 LT AC C 12' 90° 15.6' O DWY-30 B 34+66 B 35+00 LT AC C 34' 90° 13.8' O DWY-31 B 35+85 B 36+07 LT AC C 22' 90° 13.4' O DWY-32 B 21+13 B 21+24 RT AC C 24' 90° 16.9' O DWY-33 B 5+59 B 5+85 RT AC C 26' 90° 10.4' O DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF	DWY-26	B 23+02	B 23+26	LT	AC	С	24'	90°	12.7'	0.0
DWY-29 B 30+49 B 30+61 LT AC C 12' 90° 15.6' 0.0 DWY-30 B 34+66 B 35+00 LT AC C 34' 90° 13.8' 0.0 DWY-31 B 35+85 B 36+07 LT AC C 22' 90° 13.4' 0.0 DWY-32 B 21+13 B 21+24 RT AC C 24' 90° 16.9' 0.0 DWY-33 B 5+59 B 5+85 RT AC C 26' 90° 10.4' 0.0 DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF	DWY-27	B 28+53	B 28+77	RT	AC	С	24'	90°	13.3'	0.0
DWY-30 B 34+66 B 35+00 LT AC C 34' 90° 13.8' 0. DWY-31 B 35+85 B 36+07 LT AC C 22' 90° 13.4' 0. DWY-32 B 21+13 B 21+24 RT AC C 24' 90° 16.9' 0. DWY-33 B 5+59 B 5+85 RT AC C 26' 90° 10.4' 0. DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF	DWY-28	B 29+54	B 29+65	RT	AC	С	11'	90°	12.1'	0.0
DWY-31 B 35+85 B 36+07 LT AC C 22' 90° 13.4' 0. DWY-32 B 21+13 B 21+24 RT AC C 24' 90° 16.9' 0. DWY-33 B 5+59 B 5+85 RT AC C 26' 90° 10.4' 0. DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF	DWY-29	B 30+49	B 30+61	LT	AC	С	12'	90°	15.6'	0.0
DWY-32 B 21+13 B 21+24 RT AC C 24' 90° 16.9' 0. DWY-33 B 5+59 B 5+85 RT AC C 26' 90° 10.4' 0. DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF	DWY-30	B 34+66	B 35+00	LT	AC	С	34'	90°	13.8'	0.0
DWY-33 B 5+59 B 5+85 RT AC C 26' 90° 10.4' 0. DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF	DWY-31	B 35+85	B 36+07	LT	AC	С	22'	90°	13.4'	0.0
DRIVEWAY TABLE NOTE: 1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF		D 24 : 42	D 21 . 24		4.0	•	241	0.00		
1. SEE NYSDOT 608 STANDARD SHEETS FOR DEFEINITION OF	DWY-32	B 21+13	B 21+24	K I	AC	C	24	90-	16.9'	0.0
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			0.0
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			
	DWY-33 DRIVEWAY 1. SE	B 5+59 ' TABLE NOTE: E NYSDOT 608	B 5+85	RT EETS FO	AC OR DEFEINITI	C ON OF	26'			

DRIVEWAY TABLE

CORNER

ANGLE

90°

90°

90°

90°

90°

90°

90°

90°

90°

90°

90°

90°

90°

90°

90°

90°

(FT)

13.1'

10.7'

12.6'

12.3'

10.7'

12.6'

11.5'

12.4'

7.5'

12.4'

11.2'

11.3'

12.6'

13.0'

11.3'

13.0'

(FT)

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

0.0'

(FT)

26'

11'

30'

24'

26'

10'

24'

18'

18'

21'

20'

12'

16'

23'

20'

ENTRANCE

TYPE

STATION

B 06+22

B 07+43

B 09+57

B 11+41

B 12+24

B 13+01

B 13+56

B 15+18

B 16+38

B 18+11

B 19+02

B 20+43

B 21+87

B 22+68

B 24+62

B 26+15

B 26+41

B 28+38

B 28+90

B 31+01

B 34+52

B 35+25

OFFSET

20.6'

28.5'

30.0'

29.4'

31.2'

31.1'

31.1'

31.2'

32.3'

31.2'

31.2'

34.4'

37.3'

31.2'

31.2'

31.2'

31.2'

27.7'

26.7'

26.7'

27.7'

RT

RT

RT

RT

RT

LT

LT

RT

LT

LT

RT

LT

LT

RT

RT

LT

TOTALS:

EXISTING

MATERIAL

AC

AC

AC

AC

AC

AC

AC

AC

AC

AC

AC

AC

AC

AC

AC

AC

SIDE

RT

LT

RT

LT

RT

LT

RT

LT

RT

RT

LT

LT

RT

LT

END STA

B 6+83

B 7+11

B 7+50

B 8+05

B 8+49

B 8+83

B 9+76

B 10+24

B 10+42

B 10+73

B 12+65

B 13+37

B 13+70

B 14+39

B 14+78

B 15+15

BEGIN STA

B 6+55

B 6+85

B 7+75

B 8+25

B 8+57

B 9+66

B 10+00

B 10+24

B 10+55

B 12+44

B 13+17

B 13+58

B 14+23

B 14+55

B 14+95

DWY-1

DWY-2

DWY-6

DWY-7

DWY-9

DWY-10

DWY-11

DWY-12

DWY-13

DWY-15

DWY-16

			BENCH TABLE		
STATION	OFFSET	SIDE	ITEM 615.08010005 (EA)	ITEM 304.11000008 (CY)	ITEM 608.0101 (CY)
B 05+99	20.4'	LT	1	0.4	0.5
B 07+93	29.7'	RT	1	0.4	0.5
B 09+32	24.7'	LT	1	0.4	0.5
B 11+18	32.7'	RT	1	0.4	0.5
B 11+31	28.6'	LT	1	0.4	0.5
B 12+36	31.9'	RT	1	0.4	0.5
B 13+10	31.9'	LT	1	0.4	0.5
B 13+45	31.9'	RT	1	0.4	0.5
B 14+90	31.9'	RT	1	0.4	0.5
B 16+33	32.2'	LT	1	0.4	0.5
B 18+20	31.9'	RT	1	0.4	0.5
B 20+50	29.9'	LT	1	0.4	0.5
B 20+53	29.9'	LT	1	0.4	0.5
B 21+79	37.4'	RT	1	0.4	0.5
B 23+76	31.9'	LT	1	0.4	0.5
B 24+45	31.9'	RT	1	0.4	0.5
B 26+83	32.7'	RT	1	0.4	0.5
B 26+89	38.8'	LT	1	0.4	0.5
B 29+21	27.4'	RT	1	0.4	0.5
B 30+17	28.4'	LT	1	0.4	0.5
B 35+44	27.4'	RT	1	0.4	0.5
B 35+57	28.4'	LT	1	0.4	0.5
		TOTALS:	22	8.8	11.0

23

LITTER AND RECYCLING RECEPTICAL TABLE

615.01010110 (EA) 615.01010210 (EA) 304.11000008 (CY)

23

	RRFB TABLE							
STATION	OFFSET	SIDE	NOTES	ITEM 680.82254310 (EA)				
B 13+87	32.5'	RT	SOLAR POWERED	1				
B 13+89	32.0'	LT	SOLAR POWERED	1				
B 16+95	31.8'	RT	SOLAR POWERED	1				
B 17+10	32.5'	LT	SOLAR POWERED	1				
B 24+12	31.2'	RT	SOLAR POWERED	1				
B 24+14	32.3'	LT	SOLAR POWERED	1				

STATION	OFFSET	SIDE	1TEM 615.27020010 (EA)
B 06+06	21.7'	LT	1
B 06+14	21.7'	LT	1
B 06+73	29.4'	RT	1
B 08+88	30.9'	RT	1
B 09+41	25.4'	LT	1
B 11+49	30.4	LT	1
B 12+80	32.4'	RT	1
B 13+21	32.4'	LT	1
B 13+80	32.4'	LT	1
B 14+84	32.4'	LT	1
B 15+31	32.4'	RT	1
B 16+16	32.4'	LT	1
B 18+32	32.4'	RT	1
B 18+56	32.4'	LT	1
B 19+92	32.5'	RT	1
B 20+04	32.4'	LT	1
B 21+68	42.7'	RT	1
B 21+99	32.4'	LT	1
B 23+66	32.4'	LT	1
B 26+05	32.4'	LT	1
B 26+14	32.4'	RT	1
B 26+22	32.4'	RT	1
B 26+30	32.4'	RT	1
B 28+01	28.9'	LT	1
B 28+04	27.9'	RT	1
B 29+36	27.9'	RT	1
B 30+05	28.9'	LT	1
B 36+24	28.9'	LT	1

TOTALS:

28

BIKE RACK TABLE

ITEM

608.0101 (CY)

0.31

0.31

0.31

0.31

0.31

0.31

0.31

0.31

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5.3

METER KIOSK PAD TABLE									
STATION	ATION OFFSET		OFFSET SIDE		ITEM 304.11000008 (CY)	1TEM 608.0101 (CY)			
B 08+08	23.9'	LT	0.07	0.1					
B 11+16	34.7'	RT	0.07	0.1					
B 13+14	33.9'	LT	0.07	0.1					
B 15+69	33.9'	LT	0.07	0.1					
B 15+70	34.0'	RT	0.07	0.1					
B 18+98	34.0'	RT	0.07	0.1					
B 19+06	33.9'	LT	0.07	0.1					
B 22+99	33.9'	LT	0.07	0.1					
B 24+91	34.0'	RT	0.07	0.1					
B 25+56	33.9'	LT	0.07	0.1					
B 28+69	30.5'	LT	0.07	0.1					
B 31+45	30.4'	LT	0.07	0.1					
B 34+07	30.3'	LT	0.07	0.1					
•	•	TOTALS:	0.96	1.3					

*CITY SHALL INSTALL METER KIOSK ON PAD.

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

GREENMAN-PEDERSEN, INC

CONSULTING ENGINEERS

CONTRACT NUMBER

DL34880

CITY OF KINGSTON

DRAWING NO. MST-4 SHEET NO. 20

FROM STATION	SIDE	ITEM 634.06030007 (EA)
B 5+16	RT	1
B 5+52	RT	1
B 6+19	RT	1
B 6+57	RT	1
B 7+16	LT	1
B 7+50	RT	1
B 7+53	LT	1
B 7+91	RT	1
B 8+35	LT	1
B 8+87	LT	1
B 8+87	RT	1
B 9+32	LT	1
B 9+35	RT	1
B 9+73	LT	1
B 10+29	LT	1
B 10+71	RT	1
B 10+95	RT	1
B 11+03	LT	1
B 12+13	RT	1
B 12+87	RT	1
B 12+92	LT	1
B 13+36	LT	1
B 13+63	RT	1
B 14+00	LT	1
B 14+72	LT	1
B 15+00	RT	1
B 15+20	LT	1
B 15+40	RT	1
B 15+68	LT	1
B 16+09	LT	1
B 16+53	RT	1
B 16+98	RT	1
B 17+73	LT	1
B 18+13	LT	1
B 18+25	RT	1
B 18+52	LT	1
B 19+30	LT	1
		1
B 19+71	LT	
B 19+87	RT	1
B 20+12	LT	1
B 20+33	LT	1
B 21+73	LT	1
B 22+14	LT	1
B 22+54	LT	1
B 22+74	LT	1
B 23+23	RT	1
B 23+48	LT	1
B 23+59	RT	1
B 23+92	LT	1
B 24+00	RT	1
B 24+45	RT	1
B 25+05	LT	1
B 25+45	RT	1
B 25+46	LT	1
B 25+87	RT	1
B 25+90	LT	1
B 26+27	RT	1
B 26+33	LT	1
B 26+69	RT	1
B 26+93	RT	1
B 28+02		1
	LT	
B 28+44	LT	1
B 28+90	LT	1
	TOTAL:	63
	S ARE TO BE REMOVED A	

TABLE OF PARKING METER REMOVAL

FILE NAME = ...\CADD\878849.CPH_MST-5.dgn DATE/TIME = 11/25/2019 +

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019

SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND INCLUDE THE NOTATION OF THE ALTERATION OF THE ALTERATION.

GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

CITY OF KINGSTON

DECODED TO ALL TERATIONS			
DESCRIPTION OF ALTERATIONS:	BROADWAY STREETSCAPE PROJECT	RTE 32	
	CITY OF KINGSTON		
	COUNTY: ULSTER REGION: 8		
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING U			
TO ALTER AN ITEM IN ANY WAY. IF AN ITEM REARING THE STAMP OF A LI	ICENSED PROFESSIONAL IS ALTERED. THE ALTERING ENGINEER. ARCHI	TECT. LANDSCAPE ARCHITECT. O	R LAND SURVEYOF

B 23+74 B 23+74 12 20

B 23+94 B 23+94 12 20

PARKING STRIP - LT

PARKING STRIP - LT

TABLE OF PAVEMENT MARKINGS - WHITE (CONT.)

KINGSTON CONNECTIVITY			PIN 8	780.49	
BROADWAY STREETSCAPE PROJECT			RTE 3	32	
CITY OF KINGSTON					
COUNTY: ULSTER	REGION:	8			
 INDED THE DIDECTION OF A LIGHTED DOCESCIONAL	ENATHEED		TEAT		-

ITEM

685.11

(FT)

LENGTH PAY

(FT) FACTOR

DL34880

CONTRACT NUMBER

DRAWING NO. MST-5 SHEET NO. 21

CONNECTIVITY	PIN 8780.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED
STREETSCAPE PROJECT	RTE 32			
KINGSTON				MOSEL LANGUE TABLES
				MISCELLANEOUS TABLES

			TABLE OF PAVEMENT MARKINGS - WHITE						TA	BLE OF PAVEMENT MARKINGS - WHITE (CONT.)
STATION TO STATION	BEGIN OFFSET	END OFFSET	DESCRIPTION	LENGTH (FT)	PAY FACTOR	ITEM 685.11	STATION TO STATION	BEGIN OFFSET	END OFFSET	DESCRIPTION
	(FT)	(FT)		, ,		(FT)		(FT)	(FT)	
B 6+13 B 9+03	24	26	EB LANE - RT EDGE LINE	290	1	290	B 10+30 B 10+50 B 10+80 B 10+99	7 8	7 8	WB LANE - LT EDGE LINE
B 9+03 B 9+79	26	24 24	EB LANE - RT EDGE LINE	77	1	77 40	B 10+80 B 10+99 B 12+35 B 13+50	12	12	WB LANE - LT EDGE LINE WB LANE - LT EDGE LINE
B 9+79 B 10+19 B 10+48 B 11+10		25	EB LANE - RT EDGE LINE EB LANE - RT EDGE LINE	40 62	1	62	B 13+78 B 13+92	12	12	WB LANE - LT EDGE LINE
B 12+71 B 13+11	22	22	EB LANE - RT EDGE LINE	40	1	40	B 14+01 B 14+15	12	12	WB LANE - LT EDGE LINE
B 13+43 B 13+63		22	EB LANE - RT EDGE LINE	20	1	20	B 14+47 B 14+87	12	12	WB LANE - LT EDGE LINE
B 14+80 B 15+40		22	EB LANE - RT EDGE LINE	60	1	60	B 15+23 B 16+01	12	12	WB LANE - LT EDGE LINE
B 16+27 B 16+67	22	22	EB LANE - RT EDGE LINE	40	1	40	B 17+18 B 17+34	12	12	WB LANE - LT EDGE LINE
B 18+04 B 18+44		22	EB LANE - RT EDGE LINE	40	1	40	B 17+79 B 18+71	12	12	WB LANE - LT EDGE LINE
B 19+72 B 20+12	 	22	EB LANE - RT EDGE LINE	40	1	40	B 19+09 B 20+13	12	12	WB LANE - LT EDGE LINE
B 23+09 B 23+93	22	22	EB LANE - RT EDGE LINE	84	1	84	B 21+66 B 22+94	12	12	WB LANE - LT EDGE LINE
B 25+29 B 25+49	22	22	EB LANE - RT EDGE LINE	20	1	20	B 23+34 B 24+06	12	12	WB LANE - LT EDGE LINE
B 25+63 B 26+49	22	22	EB LANE - RT EDGE LINE	86	1	86	B 24+76 B 26+45	12	12	WB LANE - LT EDGE LINE
B 9+79 B 9+79	24	32	PARKING STRIP - RT	8	1	8	B 28+09 B 29+15	8	8	WB LANE - LT EDGE LINE
B 9+99 B 9+99	24	32	PARKING STRIP - RT	8	1	8	B 30+09 B 30+41	8	8	WB LANE - LT EDGE LINE
B 10+19 B 10+19	24	32	PARKING STRIP - RT	8	1	8	B 30+69 B 34+58	8	8	WB LANE - LT EDGE LINE
B 10+48 B 10+48	 	32	PARKING STRIP - RT	8	1	8	B 35+08 B 35+77	8	8	WB LANE - LT EDGE LINE
B 10+68 B 10+68		33	PARKING STRIP - RT	8	1	8	B 36+15 B 36+34	8	8	WB LANE - LT EDGE LINE
B 10+90 B 10+90		33	PARKING STRIP - RT	8	1	8	B 6+05 B 6+05	1	9	PARKING STRIP - LT
B 11+10 B 11+10		33	PARKING STRIP - RT	8	1	8	B 6+25 B 6+25	1	9	PARKING STRIP - LT
B 12+71 B 12+71		30	PARKING STRIP - RT	8	1	8	B 6+45 B 6+45	1	9	PARKING STRIP - LT
B 12+91 B 12+91	22	30	PARKING STRIP - RT	8	1	8	B 6+91 B 6+91	1	9	PARKING STRIP - LT
B 13+11 B 13+11	22	30	PARKING STRIP - RT	8	1	8	B 7+11 B 7+11	1	9	PARKING STRIP - LT
B 13+43 B 13+43		30	PARKING STRIP - RT	8	1	8	B 7+31 B 7+31	1	9	PARKING STRIP - LT
B 13+63 B 13+63		30 30	PARKING STRIP - RT	8	1	8	B 7+52 B 7+52 B 7+72 B 7+72	2 2	9	PARKING STRIP - LT
B 14+80 B 14+80 B 15+01 B 15+01	22	30	PARKING STRIP - RT	8	1	8	B 8+21 B 8+21	2	10	PARKING STRIP - LT PARKING STRIP - LT
B 15+01 B 15+01		30	PARKING STRIP - RT PARKING STRIP - RT	8	1	8	B 8+41 B 8+41	2	10	PARKING STRIP - LT
B 15+40 B 15+40		30	PARKING STRIF - RT	8	1	8	B 9+04 B 9+04	4	12	PARKING STRIP - LT
B 16+27 B 16+27	22	30	PARKING STRIP - RT	8	1	8	B 9+24 B 9+24	4	12	PARKING STRIP - LT
B 16+47 B 16+47	22	30	PARKING STRIP - RT	8	1	8	B 9+46 B 9+46	5	13	PARKING STRIP - LT
B 16+67 B 16+67	22	30	PARKING STRIP - RT	8	1	8	B 9+68 B 9+68	5	13	PARKING STRIP - LT
B 18+04 B 18+04		30	PARKING STRIP - RT	8	1	8	B 9+88 B 9+88	6	14	PARKING STRIP - LT
B 18+24 B 18+24	22	30	PARKING STRIP - RT	8	1	8	B 12+49 B 12+49	12	20	PARKING STRIP - LT
B 18+44 B 18+44	22	30	PARKING STRIP - RT	8	1	8	B 12+69 B 12+69	12	20	PARKING STRIP - LT
B 19+72 B 19+72	22	30	PARKING STRIP - RT	8	1	8	B 12+91 B 12+91	12	20	PARKING STRIP - LT
B 19+92 B 19+92	22	30	PARKING STRIP - RT	8	1	8	B 13+13 B 13+13	12	20	PARKING STRIP - LT
B 20+12 B 20+12	22	30	PARKING STRIP - RT	8	1	8	B 13+35 B 13+35	12	20	PARKING STRIP - LT
B 23+09 B 23+09	22	30	PARKING STRIP - RT	8	1	8	B 13+92 B 13+92	12	22	PARKING STRIP - LT
B 23+29 B 23+29	22	30	PARKING STRIP - RT	8	1	8	B 14+01 B 14+01	12	22	PARKING STRIP - LT
B 23+51 B 23+51	22	30	PARKING STRIP - RT	8	1	8	B 14+57 B 14+57	12	20	PARKING STRIP - LT
B 23+73 B 23+73		30	PARKING STRIP - RT	8	1	8	B 14+77 B 14+77	12	20	PARKING STRIP - LT
B 23+93 B 23+93		30	PARKING STRIP - RT	8	1	8	B 15+35 B 15+35	12	20	PARKING STRIP - LT
B 25+29 B 25+29		30	PARKING STRIP - RT	8	1	8	B 15+57 B 15+57	12	20	PARKING STRIP - LT
B 25+49 B 25+49		30	PARKING STRIP - RT	8	1	8	B 15+79 B 15+79	12	20	PARKING STRIP - LT
B 25+63 B 25+63		30	PARKING STRIP - RT	8	1	8	B 16+01 B 16+01	12	20	PARKING STRIP - LT
B 25+83 B 25+83		30	PARKING STRIP - RT	8	1	8	B 17+92 B 17+92 B 18+14 B 18+14	12	20 20	PARKING STRIP - LT
B 26+05 B 26+05		30	PARKING STRIP - RT	8	1	8	B 18+14 B 18+14 B 18+36 B 18+36	12 12	20	PARKING STRIP - LT PARKING STRIP - LT
B 26+27 B 26+27 B 26+49 B 26+49		30 30	PARKING STRIP - RT PARKING STRIP - RT	8	1	8	B 18+58 B 18+58	12	20	PARKING STRIP - LT
B 6+01 B 6+05	1	1	WB LANE - LT EDGE LINE	4	1	4	B 19+25 B 19+25	12	20	PARKING STRIP - LT
B 6+05 B 6+45	1	1	WB LANE - LT EDGE LINE WB LANE - LT EDGE LINE	40	1	40	B 19+47 B 19+47	12	20	PARKING STRIP - LT
B 6+45 B 6+49	1	1	WB LANE - LT EDGE LINE WB LANE - LT EDGE LINE	40	1	40	B 19+69 B 19+69	12	20	PARKING STRIP - LT
B 6+89 B 6+91	1	1	WB LANE - LT EDGE LINE WB LANE - LT EDGE LINE	2	1	2	B 19+91 B 19+91	12	20	PARKING STRIP - LT
B 6+91 B 7+31	1	1	WB LANE - LT EDGE LINE WB LANE - LT EDGE LINE	40	1	40	B 20+13 B 20+13	12	20	PARKING STRIP - LT
B 7+31 B 7+33	1	1	WB LANE - LT EDGE LINE	2	1	2	B 21+90 B 21+90	12	20	PARKING STRIP - LT
B 7+56 B 7+69	1	1	WB LANE - LT EDGE LINE	14	1	14	B 22+10 B 22+10	12	20	PARKING STRIP - LT
B 8+11 B 8+51	2	2	WB LANE - LT EDGE LINE	41	1	41	B 22+32 B 22+32	12	20	PARKING STRIP - LT
B 8+90 B 9+95	 	6	WB LANE - LT EDGE LINE	104	1	104	B 22+54 B 22+54	12	20	PARKING STRIP - LT
,			1]			B 22+76 B 22+76	12	20	PARKING STRIP - LT
EAL: GREENMAN - EMBER 12, 2019	PEDERSEN	, INC	ALTERED BY: ON:				B 23+52 B 23+52	12	20	PARKING STRIP - LT
.mocn 12, 2013			····				B 23+74 B 23+74	12	20	PARKING STRIP - LT

AS-BUILT REVISIONS

TABLE OF PAVEMENT MARKINGS - WHITE

STATIO		BEGIN OFFSET (FT)	END OFFSET (FT)	DESCRIPTION	LENGTH (FT)	PAY FACTOR	ITEM 685.11 (FT)
B 24+88	B 24+88	12	20	PARKING STRIP - LT	8	1	8
	B 25+08	12	20	PARKING STRIP - LT	8	1	8
	B 25+30	12	20	PARKING STRIP - LT	8	1	8
	B 25+52	12	20	PARKING STRIP - LT	8	1	8
	B 25+74	12	20	PARKING STRIP - LT	8	1	8
	B 25+96	12	20	PARKING STRIP - LT	8	1	8
B 26+18	B 26+18	12	20	PARKING STRIP - LT	8	1	8
B 26+40	B 26+40	12	20	PARKING STRIP - LT	8	1	8
B 28+23	B 28+23	8	16	PARKING STRIP - LT	8	1	8
B 28+43	B 28+43	8	16	PARKING STRIP - LT	8	1	8
B 28+65	B 28+65	8	16	PARKING STRIP - LT	8	1	8
B 28+87	B 28+87	8	16	PARKING STRIP - LT	8	1	8
B 29+07	B 29+07	8	16	PARKING STRIP - LT	8	1	8
B 30+82	B 30+82	8	16	PARKING STRIP - LT	8	1	8
B 31+04	B 31+04	8	16	PARKING STRIP - LT	8	1	8
B 31+24	B 31+24	8	16	PARKING STRIP - LT	8	1	8
B 31+44	B 31+44	8	16	PARKING STRIP - LT	8	1	8
B 31+64	B 31+64	8	16	PARKING STRIP - LT	8	1	8
B 31+84	B 31+84	8	16	PARKING STRIP - LT	8	1	8
B 32+04	B 32+04	8	16	PARKING STRIP - LT	8	1	8
B 32+24	B 32+24	8	16	PARKING STRIP - LT	8	1	8
B 32+44	B 32+44	8	16	PARKING STRIP - LT	8	1	8
B 32+64	B 32+64	8	16	PARKING STRIP - LT	8	1	8
B 32+84	B 32+84	8	16	PARKING STRIP - LT	8	1	8
B 33+04	B 33+04	8	16	PARKING STRIP - LT	8	1	8
B 33+24	B 33+24	8	16	PARKING STRIP - LT	8	1	8
B 33+44	B 33+44	8	16	PARKING STRIP - LT	8	1	8
B 33+64	B 33+64	8	16	PARKING STRIP - LT	8	1	8
B 33+84	B 33+84	8	16	PARKING STRIP - LT	8	1	8
B 34+04	B 34+04	8	16	PARKING STRIP - LT	8	1	8
B 34+24	B 34+24	8	16	PARKING STRIP - LT	8	1	8
B 34+46	B 34+46	8	16	PARKING STRIP - LT	8	1	8
B 35+20	B 35+20	8	16	PARKING STRIP - LT	8	1	8
B 35+42	B 35+42	8	16	PARKING STRIP - LT	8	1	8
B 35+64	B 35+64	8	16	PARKING STRIP - LT	8	1	8
B 5+64	B 6+55	7	7	HATCH W/ SURROUNDING EDGE LINES	196	1	196
B 6+83	B 7+39	7	7	HATCH W/ SURROUNDING EDGE LINES	178	1	178
B 7+50	B 7+75	7	7	HATCH W/ SURROUNDING EDGE LINES	94	1	94
B 8+05	B 8+58	7	8	HATCH W/ SURROUNDING EDGE LINES	187	1	187
B 8+84	B 9+99	9	12	HATCH W/ SURROUNDING EDGE LINES	346	1	346
B 10+24		13	13	HATCH W/ SURROUNDING EDGE LINES	133	1	133
	B 11+49	14	19	HATCH W/ SURROUNDING EDGE LINES	230	1	230
	B 13+58	20	20	HATCH W/ SURROUNDING EDGE LINES	383	1	383
	B 14+23	20	20	HATCH W/ SURROUNDING EDGE LINES	178	1	178
	B 14+95		20	HATCH W/ SURROUNDING EDGE LINES	189	1	189
	B 16+39	20	21	HATCH W/ SURROUNDING EDGE LINES	344	1	344
	B 17+42	20	20	HATCH W/ SURROUNDING EDGE LINES	132	1	132
	B 18+79	20	20	HATCH W/ SURROUNDING EDGE LINES	330	1	330
	B 20+57	20	21	HATCH W/ SURROUNDING EDGE LINES	423	1	423
	B 23+02	20	20	HATCH W/ SURROUNDING EDGE LINES	421	1	421
	B 24+14	20	19	HATCH W/ SURROUNDING EDGE LINES	278	1	278
	B 27+07	20	21	HATCH W/ SURROUNDING EDGE LINES	642	1	642
	B 29+23	16	16	HATCH W/ SURROUNDING EDGE LINES	355	1	355
	B 30+49	16	16	HATCH W/ SURROUNDING EDGE LINES	185	1	185
	B 34+66	16	16	HATCH W/ SURROUNDING EDGE LINES	1071	1	1071
	B 35+85	16	16	HATCH W/ SURROUNDING EDGE LINES	268	1	268
B 36+07	B 36+42	16	16	HATCH W/ SURROUNDING EDGE LINES	138	1	138
B 5+51	B 5+64	27	21	EDGE LINES - GREEN CONFLICT ZONE	14	1	14
B 6+55	B 6+55	11	19	EDGE LINES - GREEN CONFLICT ZONE	8	1	8
B 6+56	B 6+82	11	11	DOTTED EXTENSION LINE - GREEN CONFLICT ZONE	27	0.33	9
B 6+83	B 6+83	11	19	EDGE LINES - GREEN CONFLICT ZONE	8	1	8
	D 7 . 20	1 11	10	EDGE LINES - GREEN CONFLICT ZONE	8	1	8
B 7+39 B 7+40	B 7+39 B 7+48	11 11	19 11	DOTTED EXTENSION LINE - GREEN CONFLICT ZONE	9	0.33	3

TABLE OF PAVEMENT MARKINGS - WHITE (CONT.)

E = ...\CADD\878849_CPH_MST-6 E = 11/25/2019 R = tvuce

B 21+59 B 21+59 26 30 AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 WAN PEDERSEN

56

ALTERED BY: ON:

TABLE OF PAVEMENT MARKINGS - WHITE (CONT.)

DESCRIPTION

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN BIKE BOX

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN CONFLICT ZONE

EDGE LINES - GREEN BIKE BOX

EDGE LINES - GREEN BIKE BOX

18" STOP BAR

18" STOP BAR

12" STOP BAR

18" STOP BAR

18" STOP BAR

12" STOP BAR

18" STOP BAR

18" STOP BAR

18" STOP BAR

18" STOP BAR

12" STOP BAR

18" STOP BAR

18" STOP BAR

12" STOP BAR

18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

29 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

11 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

LENGTH PAY

15

38

81

13

27

11

4

17

22

4

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

32

(FT) FACTOR

685.11

(FT)

BEGIN FND

OFFSET OFFSET

19

21

26

30

B 8+62 | B 8+83 | 13 | 13 | DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 10+02 B 10+23 16 17 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 10+59 B 10+72 17 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 11+69 B 12+07 20 21 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 13+60 B 13+70 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 14+24 B 14+38 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 14+98 B 15+12 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 16+43 B 16+99 22 2 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 16+43 B 16+99 30 30 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 17+43 B 17+69 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 18+80 B 19+01 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

 B 20+69
 B 21+43
 22
 22
 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

 B 20+69
 B 21+43
 30
 30
 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 21+54 B 21+54 26 12 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 21+62 B 21+62 26 12 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 23+04 B 23+25 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 24+22 B 24+66 22 22 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 24+22 B 24+66 30 30 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 27+05 B 27+85 21 19 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 27+05 B 27+86 29 26 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 29+25 B 29+99 18 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 29+25 | B 29+99 | 26 | 26 | DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 30+49 B 30+59 18 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

B 35+86 B 36+06 18 18 DOTTED EXTENSION LINE - GREEN CONFLICT ZONE

(FT)

16

B 7+50 B 7+50 11 19

B 7+75 | B 7+75 | 11 | 19

B 8+58 B 8+58 12 20

B 10+24 B 10+24 17 25

B 10+56 B 10+56 17 25

B 11+64 B 11+78 16 16

B 13+70 B 13+70 22 30

B 14+23 | B 14+23 | 22 | 30

B 14+39 B 14+39 22 30

B 15+15 | B 15+15 | 22 | 30

B 21+54 B 21+68 14 14

B 27+17 B 27+31 17 16

B 34+70 B 34+96 18

B 11+27 B 11+27 25 13

B 11+30 B 11+31 13 2 B 11+49 B 11+49 20 24

B 11+81 B 11+98 49 46

B 12+27 B 12+27 11 12

B 12+27 B 12+27 26 30

B 16+50 B 16+72 43 44

B 19+39 B 19+47 44 48

B 20+54 B 20+54 1 30

B 20+56 B 20+56 22 26

B 21+05 B 21+26 58 57

B 20+78 B 21+09 61

B 14+17 | B 14+31 | 48

B 7+78 | B 8+04 | 11

B 8+05 | B 8+05 | 11

B 8+84 | B 8+84 | 13

B 10+74 B 10+74 18

B 11+69 | B 12+07 | 28

B 10+01 B 10+01

STATION TO

STATION

1 27 0.33 9 1 8 1 21 0.33 1 0.33 0.33 38 0.33 13 0.33 13 10 0.33 1 0.33 1 14 0.33 1 0.33 58 0.33 20 27 0.33 21 0.33 0.33 0.33 25 38 0.33 13 0.33 0.33 44 0.33 44 0.33 15 27 0.33 44 1 44 74 0.33 74 0.33 25 0.33 0.33 21 0.33 12 4.5 54 4.5 50 3 12 17 4.5 77 4.5 99 3.00 12 4.5 77 4.5 99 10 4.5 45 30 4.5 135 3.0 12 4.5 144 22 4.5 99 4 3.0 12

STATIO	N TO	BEGIN	END			PAY	ITEM
STATI			OFFSET	DESCRIPTION	LENGTH (FT)	FACTOR	685.11
		(FT)	(FT)		` '	.,	(FT)
B 21+70	B 21+70	11	12	18" STOP BAR	22	4.5	99
B 24+31	B 24+46	52	49	18" STOP BAR	15	4.5	68
B 26+92	B 26+92	1	30	18" STOP BAR	30	4.5	135
B 27+07	B 27+07	22	26	12" STOP BAR	4	3.0	12
B 27+22	B 27+41	49	45	18" STOP BAR	20	4.5	90
B 27+47	B 27+64	45	57	18" STOP BAR	20	4.5	90
B 28+01	B 28+01	22	26	12" STOP BAR	4	3.0	12
B 28+12	B 28+12	14	8	18" STOP BAR	22	4.5	99
B 36+59	B 36+75	36	36	18" STOP BAR	16	4.5	72
B 37+70	B 37+70	3	25	18" STOP BAR	22	4.5	99
B 11+01		8	10	MISC EDGE LINES	45	1	45
B 11+96		26	26	MISC EDGE LINES	61	1	61
B 20+13		12	16	MISC EDGE LINES	52	1	52
B 21+56		26	26	MISC EDGE LINES	61	1	61
B 26+47		22	26	MISC EDGE LINES	51	1	51
B 9+86		13	14	DOTTED EXTENSION LINE - RT	108	0.33	36
B 10+94		14	14	LANE EDGE LINE - RT	36	1	36
B 12+27		1	1	LANE EDGE LINE - LT DOTTED EXTENSION LINE - LT	50	1 0.22	50 9
B 12+77		1	1		25	0.33	
B 14+53		1	1	LANE EDGE LINE - LT	50	1 0.22	50
B 15+03		1	1	DOTTED EXTENSION LINE - LT	25	0.33	9
B 15+78		1	1	DOTTED EXTENSION LINE - RT	25	0.33	9
B 16+03		1	1	LANE EDGE LINE - RT	50	1	50
B 17+58		1	1	LANE EDGE LINE - LT	50	1	50
B 18+08		1	1	DOTTED EXTENSION LINE - LT	25	0.33	9
B 19+64	B 20+54	11	11	LANE EDGE LINE - RT	90	1	90
B 21+70	B 22+16	1	1	LANE EDGE LINE - LT	46	1	46
B 22+16	B 22+45	1	1	DOTTED EXTENSION LINE - LT	29	0.33	10
B 23+36	B 23+61	11	11	DOTTED EXTENSION LINE - RT	26	0.33	9
B 23+61	B 24+11	11	11	LANE EDGE LINE - RT	50	1	50
B 25+66	B 26+42	11	11	DOTTED EXTENSION LINE - RT	64	0.33	22
B 26+42	B 26+92	11	11	LANE EDGE LINE - RT	50	1	50
B 27+05	B 27+87	11	14	DOTTED EXTENSION LINE - RT	82	0.33	28
B 28+11		3	3	LANE EDGE LINE - RT	47	1	47
B 28+58		3	3	DOTTED EXTENSION LINE - RT	136	0.33	45
B 34+03		14	14	DOTTED EXTENSION LINE - RT	150	0.33	50
B 35+53		14	14	LANE EDGE LINE - RT	98	1	98
B 36+91		14	14	LANE EDGE LINE - RT	79	1	79
						1	
B 11+11		19	27	CROSSWALK - TYPE LS	8		112
B 11+33		30	26	CROSSWALK - TYPE LS	62		868
B 11+37		36	36	CROSSWALK - TYPE LS	25		350
B 11+80		35	38	CROSSWALK - TYPE LS	34		476
B 11+87		28	33	CROSSWALK - TYPE LS	67		938
B 13+96		29	33	CROSSWALK - TYPE LS	62		868
B 13+98	B 14+38	40	39	CROSSWALK - TYPE LS	40		560
B 16+50	B 16+90	36	37	CROSSWALK - TYPE LS	40		560
B 17+02		29	29	CROSSWALK - TYPE LS	58		812
B 17+13	B 17+44	40	39	CROSSWALK - TYPE LS	31		434
B 19+26	B 19+52	35	35	CROSSWALK - TYPE LS	25		350
B 20+20	B 20+20	22	30	CROSSWALK - TYPE LS	8		112
B 20+62	B 20+65	29	29	CROSSWALK - TYPE LS	58		812
B 20+76	B 21+24	43	47	CROSSWALK - TYPE LS	48		672
B 20+88	B 21+25	50	48	CROSSWALK - TYPE LS	37		518
B 21+49		29	29	CROSSWALK - TYPE LS	58		812
B 24+19		29	30	CROSSWALK - TYPE LS	58		812
B 24+29		40	40	CROSSWALK - TYPE LS	33		462
B 26+55		22	30	CROSSWALK - TYPE LS	8		112
B 27+03		29	31	CROSSWALK - TYPE LS	62		868
B 27+20					41		
B 27+20		39 37	37 36	CROSSWALK - TYPE LS CROSSWALK - TYPE LS	56		574 784
B 27+92		25	26	CROSSWALK - TYPE LS	51		714
B 29+41		33	36	CROSSWALK - TYPE LS	41		574
B 17+25		43	48	BLUE LANE LINE - HATCHING	88		88
B 17+27		57	62	BLUE LANE LINE - HATCHING	88		88
B 17+29	B 17+47	71	71	BLUE LANE LINE	18		18
						TOTAL:	28173

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

BROADWAY STREETSCAPE PROJECT

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

TABLE OF PAVEMENT MARKINGS - WHITE (CONT.)

BEGIN END

685.11 (FT)	
(FT)	
68 135	
12	
90	
90	
12	
99	
72 99	
45	
61	
52	
61	
51 36	
36	
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9	
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9	
90	
46	
10 9	
50	
22	
50	
28	
47	
45 50	
98	
79	
112	
868 350	
476	
938	
868	
560	
560 812	
434	
350	
112	
812	
672 518	
812	
812	
462	
112	
868	
574 784	
714	
574	
88	
88	

PIN 8780.49

RTE 32

BRIDGES

CULVERTS

ITEM

	TABLE OF PAVEMENT MARKINGS - GREEN								
		BEGIN	END		ITEM				
STATION T	O STATION	OFFSET	OFFSET	DESCRIPTION	601.01000018				
		(FT)	(FT)		(SY)				
B 5+48	B 5+64	26	13	GREEN CONFLICT ZONE	5				
B 6+56	B 6+82	11	11	GREEN CONFLICT ZONE	8				
B 7+40	B 7+48	11	11	GREEN CONFLICT ZONE	4				
B 7+78	B 8+04	11	11	GREEN CONFLICT ZONE	8				
B 8+62	B 8+82	13	13	GREEN CONFLICT ZONE	7				
B 10+02	B 10+22	16	17	GREEN CONFLICT ZONE	7				
B 10+59	B 10+72	17	18	GREEN CONFLICT ZONE	5				
B 11+04	B 11+06	19	19	GREEN CONFLICT ZONE	2				
B 11+17	B 11+19	19	19	GREEN CONFLICT ZONE	2				
B 11+64	B 11+71	12	12	BIKE BOX (7 FT x 8 FT)	7				
B 11+71	B 11+78	12	12	BIKE BOX (7 FT x 8 FT)	7				
B 11+69	B 12+07	20	21	GREEN CONFLICT ZONE	16				
B 13+60	B 13+68	22	22	GREEN CONFLICT ZONE	4				
B 13+89	B 13+91	22	22	GREEN CONFLICT ZONE	1				
B 14+01	B 14+03	22	22	GREEN CONFLICT ZONE	1				
B 14+24	B 14+38	22	22	GREEN CONFLICT ZONE	5				
B 14+98	B 15+12	22	22	GREEN CONFLICT ZONE	5				
B 16+43	B 16+99	22	22	GREEN CONFLICT ZONE	16				
B 17+11	B 17+13	22	22	GREEN CONFLICT ZONE	2				
B 17+43	B 17+69	22	22	GREEN CONFLICT ZONE	8				
B 18+80	B 19+00	22	22	GREEN CONFLICT ZONE	7				
B 20+12	B 20+14	21	21	GREEN CONFLICT ZONE	2				
B 20+25	B 20+27	21	21	GREEN CONFLICT ZONE	2				
B 20+69	B 21+43	22	22	GREEN CONFLICT ZONE	21				
B 21+54	B 21+61	14	13	BIKE BOX (7 FT x 8 FT)	7				
B 21+61	B 21+68	13	13	BIKE BOX (7 FT x 8 FT)	7				
B 21+58	B 21+58	26	12	GREEN CONFLICT ZONE	7				
B 23+04	B 23+24	22	22	GREEN CONFLICT ZONE	7				
B 24+11	B 24+13	22	22	GREEN CONFLICT ZONE	2				
B 24+22	B 24+66	22	22	GREEN CONFLICT ZONE	13				
B 26+47	B 26+49	21	21	GREEN CONFLICT ZONE	2				
B 26+60	B 26+62	21	21	GREEN CONFLICT ZONE	2				
B 27+17	B 27+85	21	18	GREEN CONFLICT ZONE	19				
B 27+17	B 27+24	13	12	BIKE BOX (7 FT x 8 FT)	7				
B 27+24	B 27+31	12	12	BIKE BOX (7 FT x 8 FT)	7				
B 29+25	B 29+99	18	18	GREEN CONFLICT ZONE	21				
B 30+51	B 30+59	18	18	GREEN CONFLICT ZONE	4				
B 34+70	B 34+96	18	18	GREEN CONFLICT ZONE	8				
B 35+86	B 36+06	18	18	GREEN CONFLICT ZONE	7				
				TOTAL	272				

24	В 27-	+31	12	12	BIKE BOX (7 FT)	(8 FT)	
25	B 29-	+99	18	18	GREEN CONFLICT	ZONE	
51	B 30-	+59	18	18	GREEN CONFLICT	ZONE	
70	B 34-	+96	18	18	GREEN CONFLICT	ZONE	
86	В 36-	+06	18	18	GREEN CONFLICT	ZONE	
						TOTAL:	
		TABLE	OF PAVEM	ENT LETTE	ERS - WHITE		
STA	TION	OF	FSET	_	ECCDIDITION.	ITEM	.
(TO C	ENTER)	(TO CE	NTER) (FT)	D	ESCRIPTION	685.13 (EA)	
B 1	1+13		12'		STOP	4	
В 1	1+32		13'		BUS	3	
В 1	2+09		25'		BUS	3	
В 1	2+28		25'		STOP	4	
B 2	20+26		15'		STOP	4	
B 2	0+44		15'		BUS	3	
B 2	1+72		25'		BUS	3	
B 2	1+92		25'		STOP	4	
B 2	26+58		25'		BUS	3	
B 2	26+58		16'		STOP	4	
В 2	26+78		25'		STOP	4	
В 2	26+78		15'		BUS	3	
В 3	35+49		9'		ONLY	4	

TOTAL:

CONTRACT NUMBER

DL34880

CITY OF KINGSTON

DRAWING NO. MST-6 SHEET NO. 22

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

GREENMAN-PEDERSEN, INC.

CONSULTING ENGINEERS

= ...\CADD\878049_CPH_MST-7.dgr = 11/25/2019 = tvuce*

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019

ALTERED BY:

TABLE OF PAVEMENT MARKINGS - YELLOW

DESCRIPTION

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

SOLID EDGE LINE

SOLID EDGE LINE

FULL BARRIER LINE

PARTIAL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

PARTIAL BARRIER LINE

PARTIAL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

PARTIAL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

PARTIAL BARRIER LINE

PARTIAL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

FULL BARRIER LINE

HATCH LINES

HATCH LINES

HATCH LINES

DOTTED SHORT LINE

DOTTED SHORT LINE

DOTTED SHORT LINE

DOTTED SHORT LINE

DOTTED SHORT LINE

DOTTED SHORT LINE

DOTTED SHORT LINE

EDGE LINE

DOTTED SHORT LINE

DOTTED SHORT LINE

DOTTED SHORT LINE

EDGE LINE

DOTTED SHORT LI

DOTTED SHORT LI

DOTTED SHORT L

DOTTED SHORT L

DOTTED SHORT L

DOTTED SHORT LI

DOTTED SHORT LI

PARTIAL BARRIER LINE

PARTIAL BARRIER LINE

LENGTH

(FT)

177

408

332

95

10

10

75

83

83

8

75

50

50

12

75

83

83

7

90

75

85

76

210

169

22

10

645

736

80

89

165

629

570

23

40

155

239

59

304

32

75

260

PAY

FACTOR

2.0

2.0

2.0

1.0

1.0 2.0

1.3

1.3

2.0

2.0

1.3

1.3

2.0

2.0

2.0

1.3

1.3

2.0

2.0

2.0

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1.3

1.3

2.0

2.0

2.0

2.0

2.0

2.0

2.0

2.0

6.0

6.0

6.0

0.33

0.33

0.33

0.33

0.33

0.33

0.33

1.00

0.33

0.33 0.33 ITEM 685.12

(FT)

354

816

664

190

10

10

150

104

104

16

150

63

63

150

24

150

104

104

14

180

14

150

106

106

152

420

338

44

20

1,290

1,472

160

532

990

3,774

190

13

52

80

20

101

32

25 87

13

10

BEGIN

OFFSET

(FT)

0

13

13

23

26

11

11

55

11

11

44

11

11

49

55

11

11

11

45

54

14

23

23

24

26

26

26

26

26

26

26

26

26

26

26

25

22

22

OFFSET

13

3

24

26

11

11

48

11

11

1

56

11

11

43

1

62

11

11

66

46

23

24

25

26

26

26

26

26

26

26

26

26

26

26

22

22

22

STATION TO STATION

B 5+78

B 9+86

B 12+27

B 12+27

B 13+02

B 13+02 B 14+13

B 14+53

B 15+28

B 15+28 B 15+78

B 16+72

B 17+58

B 18+33

B 18+33

B 19+36

B 19+64

B 21+70

B 22+45

B 22+45

B 23+36

B 24+75

B 24+75 B 27+41

B 27+41

B 29+16 B 36+91

B 7+99

B 24+75

B 29+16

B 5+49

B 11+16

B 11+68

B 12+37

B 14+01

B 16+40

B 20+25

B 20+70

B 21+54

B 21+58

B 21+58

B 24+23

B 24+68

B 26+60

B 28+01 B 29+23 B 5+78

B 9+86

B 11+30

B 10+81

B 11+49

B 12+37

B 13+02

B 13+85

B 13+85

B 14+17

B 15+28

B 15+78

B 15+78

B 16+53

B 16+74

B 18+33

B 19+16

B 19+16

B 19+39

B 20+54

B 21+11

B 22+45

B 23+36

B 23+36

B 24+11

B 26+92

B 26+33

B 27+46

B 27+47

B 34+56

B 36+51

B 37+70

B 10+81

B 26+33

B 34+56

B 11+07

B 11+39

B 12+08

B 13+92

B 16+40

B 16+99

B 20+14

B 20+57

B 21+45

B 24+14

B 21+58

B 21+68

B 24+68

B 26+49

B 27+09

B 27+85

B 29+23

B 30+01

B 30+01 B 36+42 22 22

	IT IS TO AL SHALL	A VIOLATI TER AN II STAMP T	ION OF LAW F FEM IN ANY W HE DOCUMENT	OR ANY AY. IF A	PERSON, UNLESS TO AN ITEM BEARING TO CLUDE THE NOTATION	HEY ARE ACTING HE STAMP OF A L IN "AI TERED BY"	UNDER THE DIRECTION OF A LICEN ICENSED PROFESSIONAL IS ALTERE FOLLOWED BY THEIR SIGNATURE. 1	SED PROFESS D, THE ALTE THE DATE OF	SIONAL ENGINE RING ENGINEEI SUCH ALTERA	ER, ARCHITE R, ARCHITEC TION, AND A	CT, LANDSCAPE ARC T, LANDSCAPE ARCH SPECIFIC DESCRIPT	HITECT, OR LAND SURV ITECT, OR LAND SURVE ITON OF THE ALTERATI	EYOR, YOR ON.
							COUNTY: ULSTER			ION: 8			
						CITY OF KINGSTON							
	DESC	11 11014 01	AL ILIA IION	•			BROADWAY STREETSCAPE PROJECT	Т		K	E 32		
		ILT REVIS	IONS ALTERATIONS	:•			KINGSTON CONNECTIVITY				N 8780.49	BRIDGES	CUL
													TO
				L	B 21+72	28'	STRAIGHT ARROW	1	B	17+43	66'	BLUE ADA	
				-	B 21+72	24'	PED/BIKE	1		17+41	53'	BLUE ADA	
				-	B 21+64	20'	PED/BIKE	1		37+47	8'	LEFT TURN ARE	
	_				B 21+63	16'	LEFT TURN ARROW	1	В	37+05	8'	LEFT TURN ARE	ROW
					B 21+58	16'	RIGHT TURN ARROW	1	В	36+33	24'	PED/BIKE	
	_				B 21+57	20'	PED/BIKE	1	В	36+33	20'	STRAIGHT ARE	ROW
					B 21+19	69'	RIGHT/STRAIGHT	1	В	36+27	6'	LEFT TURN ARE	ROW
				[B 21+10	68'	LEFT TURN ARROW	1	В	36+21	24'	STRAIGHT ARE	ROW
		TOTAL:	14,010	Ī	B 21+08	66'	LEFT TURN ARROW	1	В	36+21	20'	PED/BIKE	
LINE	641	0.33	214	Ì	B 20+96	70'	RIGHT/STRAIGHT	1	В	34+66	9'	LEFT TURN ARE	ROW
LINE	78	0.33	26	Ī	B 20+51	28'	PED/BIKE	1		34+55	24'	PED/BIKE	
LINE	122	0.33	41	Ī	B 20+51	24'	STRAIGHT ARROW	1		34+55	20'	STRAIGHT ARE	
LINE	68	0.33	23	Ī	B 20+50	28'	STRAIGHT ARROW	1		34+43	24'	STRAIGHT ARE	
LINE	49	0.33	16	Ī	B 20+39	24'	PED/BIKE	1		34+43	20'	PED/BIKE	
LINE	181	0.33	60	İ	B 20+30	5'	LEFT TURN ARROW	1		32+52	24'	PED/BIKE	
LINE	45	0.33	15	ŀ	B 19+90	6'	LEFT TURN ARROW	1		32+52	20'	STRAIGHT ARE	
	10	1.00 l	10		B 19+21	28'	I PED/BIKE	1 1	l P	32+40	24'	STRAIGHT ARE	NOS

STATION OFFSET (TO CENTER) (FT)			DESCRIPTION	1TEM 685.14 (EA)
f	B 5+80	15'	CHEVRON	1
ı	B 6+15	17'	STRAIGHT ARROW	1
	B 6+16	13'	PED/BIKE	1
ı	B 6+27	17'	PED/BIKE	1
ı	B 6+28	13'	STRAIGHT ARROW	1
F	B 9+30	16'	PED/BIKE	1
	B 9+31	20'	STRAIGHT ARROW	1
	B 9+42	16'	STRAIGHT ARROW	1
	B 9+42	20'	PED/BIKE	1
	B 11+03	8'	LEFT TURN ARROW	1
	B 11+23	22'	PED/BIKE	1
	B 11+23	25'	STRAIGHT ARROW	1
	B 11+35	22'	STRAIGHT ARROW	1
	B 11+35	25'	PED/BIKE	1
	B 11+66	18'	PED/BIKE	1
L	B 11+69	15'	RIGHT TURN ARROW	1
L	B 11+73	13'	LEFT TURN ARROW	1
L	B 11+74	18'	PED/BIKE	1
L	B 12+34	24'	PED/BIKE	1
L	B 12+34	28'	STRAIGHT ARROW	1
L	B 12+45	24'	STRAIGHT ARROW	1
L	B 12+45	28'	PED/BIKE	1
	B 12+50	5'	LEFT TURN ARROW	1
	B 13+38	5'	LEFT TURN ARROW	1
	B 13+51	5'	LEFT TURN ARROW	1
	B 14+62	24'	PED/BIKE	1
	B 14+62	28'	STRAIGHT ARROW	1
	B 14+73	24'	STRAIGHT ARROW	1
	B 14+73	28'	PED/BIKE	1
L	B 14+76	5'	LEFT TURN ARROW	1
L	B 15+47	5'	LEFT TURN ARROW	1
-	B 15+58	5'	LEFT TURN ARROW	1
-	B 16+24	24'	PED/BIKE	1
-	B 16+24	28'	STRAIGHT ARROW	1
-	B 16+30	3'	LEFT TURN ARROW	1
ŀ	B 16+36	24'	STRAIGHT ARROW	1
-	B 16+36	28'	PED/BIKE	1
ŀ	B 17+20	24'	PED/BIKE	1 1
ŀ	B 17+20	28'	STRAIGHT ARROW	1
ŀ	B 17+32	24'	STRAIGHT ARROW	1
F	B 17+32	28' 5'	PED/BIKE	1
H	B 17+81	5'	LEFT TURN ARROW LEFT TURN ARROW	1
H	B 18+69	5'		1
H	B 18+81 B 19+09	24'	LEFT TURN ARROW	1 1
H	В 19+09	28'	PED/BIKE STRAIGHT ARROW	1
H	B 19+21	24'	STRAIGHT ARROW	1
H	B 19+21	28'	PED/BIKE	1
H	B 19+90	6'	LEFT TURN ARROW	1
H	B 20+30	5'	LEFT TURN ARROW	1
H	B 20+39	24'	PED/BIKE	1
F	B 20+50	28'	STRAIGHT ARROW	1
-	B 20+51	24'	STRAIGHT ARROW	1
H	B 20+51	28'	PED/BIKE	1
ŀ	B 20+96	70'	RIGHT/STRAIGHT	1
+	B 21+08	66'	LEFT TURN ARROW	1
ŀ	B 21+10	68'	LEFT TURN ARROW	1
ŀ	B 21+19	69'	RIGHT/STRAIGHT	1
ŀ	B 21+57	20'	PED/BIKE	1
f	B 21+58	16'	RIGHT TURN ARROW	1
+	B 21+63	16'	LEFT TURN ARROW	1
+	B 21+64	20'	PED/BIKE	1
f	B 21+72	24'	PED/BIKE	1
	B 21+72	28'	STRAIGHT ARROW	1
_				

TABLE OF PAVEMENT SYMBOLS - WHITE

'	ABLE OF PAVEMENT	SYMBOLS - WHITE (CONT.)	
STATION	OFFSET (TO CENTER)	DESCRIPTION	ITEM
(TO CENTER)	(TO CENTER) (FT)	DESCRIPTION	685.14 (EA)
B 21+84	24'	STRAIGHT ARROW	1
B 21+84	28'	PED/BIKE	1
B 21+93	5'	LEFT TURN ARROW	1
B 22+80	24'	PED/BIKE	1
B 22+80	28'	STRAIGHT ARROW	1
B 22+86	6'	LEFT TURN ARROW	1
B 22+92	24'	STRAIGHT ARROW	1
B 22+92	27'	PED/BIKE	1
B 22+98	5'	LEFT TURN ARROW	1
B 23+88	5'	LEFT TURN ARROW	1
B 23+91	24'	PED/BIKE	1
B 23+91	28'	STRAIGHT ARROW	1
B 24+03	24'	STRAIGHT ARROW	1
B 24+03	28'	PED/BIKE	1
B 24+73	24'	PED/BIKE	1
B 24+73	28'	STRAIGHT ARROW	1
B 24+85	24'	STRAIGHT ARROW	1
B 24+85	28'	PED/BIKE	1
B 25+76	24'	PED/BIKE	1
B 25+76	28'	STRAIGHT ARROW	1
B 25+88	24'	STRAIGHT ARROW	1
B 25+88	28'	PED/BIKE	1
B 26+28	5'	LEFT TURN ARROW	1
B 26+69	5'	LEFT TURN ARROW	1
B 26+74	24'	PED/BIKE	1
B 26+74	28'	STRAIGHT ARROW	1
B 26+86	24'	STRAIGHT ARROW	1
B 26+86	28'	PED/BIKE	1
B 27+21 B 27+23	19' 15'	PED/BIKE RIGHT TURN ARROW	1
B 27+25 B 27+26	15'	LEFT TURN ARROW	1
B 27+28	19'	PED/BIKE	1
B 28+09	20'	PED/BIKE	1
B 28+09	24'	STRAIGHT ARROW	1
B 28+21	20'	STRAIGHT ARROW	1
B 28+21	24'	PED/BIKE	1
B 28+31	8'	LEFT TURN ARROW	1
B 28+72	8'	LEFT TURN ARROW	1
B 29+06	20'	PED/BIKE	1
B 29+06	24'	STRAIGHT ARROW	1
B 29+18	20'	STRAIGHT ARROW	1
B 29+18	24'	PED/BIKE	1
B 30+08	20'	PED/BIKE	1
B 30+08	24'	STRAIGHT ARROW	1
B 30+19	20'	STRAIGHT ARROW	1
B 30+19	24'	PED/BIKE	1
B 32+40	20'	PED/BIKE	1
B 32+40	24'	STRAIGHT ARROW	1
B 32+52	20'	STRAIGHT ARROW	1
B 32+52	24'	PED/BIKE	1
B 34+43	20'	PED/BIKE	1
B 34+43	24'	STRAIGHT ARROW	1
B 34+55	20'	STRAIGHT ARROW	1
B 34+55	24'	PED/BIKE	1
B 34+66	9'	LEFT TURN ARROW	1
B 36+21	20'	PED/BIKE	1
B 36+21	24'	STRAIGHT ARROW	1
B 36+27	6'	LEFT TURN ARROW	1
B 36+33	20'	STRAIGHT ARROW	1
B 36+33	24'	PED/BIKE	1
B 37+05	8'	LEFT TURN ARROW	1
B 37+47	8'	LEFT TURN ARROW	1
B 17+41	53'	BLUE ADA	1
B 17+43	66'	BLUE ADA TOTAL:	1 128

D 21+04	20	PED/ BIKE				
B 21+93	5'	LEFT TURN ARROW	1			
B 22+80	24'	PED/BIKE	1			
B 22+80	28'	STRAIGHT ARROW	1			
B 22+86	6'	LEFT TURN ARROW	1			
B 22+92	24'	STRAIGHT ARROW	1			
B 22+92	27'	PED/BIKE	1			
B 22+98	5'	LEFT TURN ARROW	1			
B 23+88	5'	LEFT TURN ARROW	1			
B 23+91	24'	PED/BIKE	1			
B 23+91	28'	STRAIGHT ARROW	1			
B 24+03	24'	STRAIGHT ARROW	1			
	28'					
B 24+03		PED/BIKE	1			
B 24+73	24'	PED/BIKE	1			
B 24+73	28'	STRAIGHT ARROW	1			
B 24+85	24'	STRAIGHT ARROW	1			
B 24+85	28'	PED/BIKE	1			
B 25+76	24'	PED/BIKE	1			
		· ·				
B 25+76	28'	STRAIGHT ARROW	1			
B 25+88	24'	STRAIGHT ARROW	1			
B 25+88	28'	PED/BIKE	1			
B 26+28	5'	LEFT TURN ARROW	1			
B 26+69	5'	LEFT TURN ARROW	1			
B 26+74	24'		1			
		PED/BIKE				
B 26+74	28'	STRAIGHT ARROW	1			
B 26+86	24'	STRAIGHT ARROW	1			
B 26+86	28'	PED/BIKE	1			
B 27+21	19'	PED/BIKE	1			
B 27+23	15'	RIGHT TURN ARROW	1			
B 27+26	15'	LEFT TURN ARROW	1			
B 27+28	19'	PED/BIKE	1			
B 28+09	20'	PED/BIKE	1			
B 28+09	24'	STRAIGHT ARROW	1			
B 28+21	20'	STRAIGHT ARROW	1			
B 28+21	24'	PED/BIKE	1			
B 28+31	8'	LEFT TURN ARROW	1			
B 28+72	8'	LEFT TURN ARROW	1			
B 29+06	20'	PED/BIKE	1			
B 29+06	24'	STRAIGHT ARROW	1			
B 29+18	20'	STRAIGHT ARROW	1			
B 29+18	24'	PED/BIKE	1			
	20'	1				
B 30+08		PED/BIKE	1_			
B 30+08	24'	STRAIGHT ARROW	1			
B 30+19	20'	STRAIGHT ARROW	1			
B 30+19	24'	PED/BIKE	1			
B 32+40	20'	PED/BIKE	1			
B 32+40	24'	STRAIGHT ARROW	1			
B 32+52	20'	STRAIGHT ARROW	1			
B 32+52	24'	PED/BIKE	1			
B 34+43	20'	PED/BIKE	1			
B 34+43	24'	STRAIGHT ARROW	1			
B 34+55	20'	STRAIGHT ARROW	1			
B 34+55	24'	PED/BIKE	1			
B 34+66	9'	LEFT TURN ARROW	1			
B 36+21	20'	PED/BIKE	1			
B 36+21	24'	STRAIGHT ARROW	1			
B 36+27	6'	LEFT TURN ARROW	1			
B 36+33	20'	STRAIGHT ARROW	1			
B 36+33	24'	PED/BIKE	1			
	8'	,	1			
B 37+05	1	LEFT TURN ARROW				
B 37+47	8'	LEFT TURN ARROW	1			
B 17+41	53'	BLUE ADA	1			
B 17+43	66'	BLUE ADA	1			

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

GREENMAN-PEDERSEN, INC

CONSULTING ENGINEERS

CONTRACT NUMBER

DL34880

CITY OF KINGSTON

DRAWING NO. MST-7 SHEET NO. 23

٦́			1	
JOB MANAGER R. JOBIN	AFFIX SEAL: GREENMAN ON: NOVEMBER 12, 2019	- PEDERSEN, IN	ALTERED BY:	
IANA				
4GE!	B 19+84	LT	1	
~ '	B 19+41	LT	1	
الع	B 19+14	LT		1
힁	B 19+07	RT	1	
ᇎ	B 18+63	LT	1	
	B 18+46	RT	1	
	B 18+30	LT	1	
	B 18+08	RT	1	
	B 17+99	LT		1
DE	B 17+78	LT	1	
DESIGN R. JOBIN	B 17+37	LT	1	
ౣ'	B 16+89	RT	1	
ሬ	B 16+37	RT	1	
취	B 16+24	LT	1	
퇿	B 15+90	LT	1	
_[]	B 15+70	LT	1	
	B 15+44	LT	1	
	B 15+27	RT	1	
	B 15+26	LT	1	
٠,	B 14+88	RT	1	
3	B 14+62	LT	1	
쏭.	B 14+11	LT		1
اك	B 13+78	LT		1
CHECK C. CORNWELL	B 13+72	RT	1	
휥	B 13+51	LT	1	
ᆲ	B 13+22	LT		1
	B 13+02	RT	1	
	B 12+96	LT	1	
	B 12+64	LT	1	
ا ت	B 12+39	LT	1	
RAF	B 12+36	RT	1	
DRAFTING G. KEHN	B 11+36	LT	1	
<u>်</u>	B 11+08	RT		1
ای	B 10+94	LT		1
鱼	B 10+81	RT	1	
릙	B 10+55	RT	4	1
	B 10+40		1	- 1
	B 10+14	RT LT	1	
		RT	1	1
	B 9+89 B 9+92	LT	1	1
ت	B 9+60	LT	1	
ÆC	B 9+52	RT	1	
∠'	B 9+22	RT	1	
CHECK T.VYCE	B 9+18	LT	1	
뛜	B 8+91	LT	1	
	B 8+91	RT	1	1
	B 8+60	RT	1	1
	B 8+51	LT	1	
	B 8+23	LT	1	
- 1	B 8+16	RT	1	
P.	B 7+69	RT		1
PROJECT	B 7+68	LT		1
CI	B 7+30	RT		1
_				

TABLE OF TREE REMOVAL

RT RT

RT

STATION B 5+12

> B 5+47 B 6+37

B 6+71 B 7+12 ITEM 614.060304

ITEM 614.0701

	171522 01 111	ITEM 614.060304	ITEM 614.070
STATION	SIDE	(EA)	(EA)
D 20 : 00			(EA)
B 20+09	LT	1	
B 20+29	LT	1	
B 21+67	LT	1	
B 21+99	LT	1	
B 22+18	LT	1	
B 22+49	LT	1	
B 22+98	LT	1	
B 23+27	RT	1	
B 23+76	RT	1	
B 23+98	LT	1	
B 24+37	RT	1	
B 24+91	LT	1	
B 25+20	LT	1	
B 25+51	LT	1	
B 25+75	LT	1	
B 25+85	RT	1	
B 25+96	LT	1	
B 26+49	RT	1	
B 26+51	LT	1	
B 26+79	LT	1	
B 28+20	LT	1	
B 28+40	LT	1	
B 29+13	LT	1	
B 34+71	RT	1	
B 35+67	RT	1	
B 36+24	LT	1	
	TOTALS:	72	14

- ALL TREES MUST BE REMOVED IN ACCORDANCE WITH THE TREE CUTTING NOTES ON $\ensuremath{\mathsf{GNN-1}}$.
- 2. THE CITY MAY ELECT TO TRANSPLANT ANY TREES, IF POSSIBLE, PRIOR TO REMOVAL.

ID#	OPTION	STATION	SIDE	LENGTH (FT)	ITEM 203.02 (CY)	ITEM 207.21 (SY)	ITEM 208.01030022 (CY)	ITEM 420.98010104 (SF)	ITEM 603.98040007 (LF)	ITEM 605.1601 (EA)	ITEM 608.72113108 (EA)	ITEM 609.05 (LF)	ITEM 610.1101 (CY)	ITEM 623.10 (CY)
1-8	L	B 5+53	RT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
1-9 1-1	L C	B 5+96 B 6+38	RT LT	5 25	4.0 20.0	10 50	2.0 10.0	25	6	5 25	4	<u>0</u> 36	0.1	1.7 8.5
1-10	G	B 6+46	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
1-2	A	B 7+13	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
1-11 1-3	F	B 7+26 B 7+64	RT LT	25 5	20.0 4.0	50 10	10.0 2.0	25	6	25 5	4	36 0	0.5 0.1	8.5 1.7
1-12	L	B 7+70	RT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
1-13	F	B 8+08	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
1-4 1-5	F N	B 8+30	LT LT	25 5	20.0 4.0	50	10.0	25	6	25	4	36 0	0.5 0.1	8.5 1.7
1-14	G	B 8+51 B 8+67	RT	25	20.0	10 50	2.0 10.0	25	6	5 25	4	36	0.1	8.5
1-6	C	B 8+98	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
1-15	A	B 9+14	RT	25	20.0	50	10.0	25	6	25	4	36	0.5	8.5
1-7 2-10	N N	B 9+21 B 9+51	LT RT	<u>5</u>	4.0 4.0	10 10	2.0	25 25	6	5 5		0	0.1	1.7 1.7
2-1	N	B 9+52	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
2-2	G	B 9+77	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
2-3	L	B 10+41	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
2-4 2-11	L H	B 10+96 B 12+08	LT RT	5 25	4.0 20.0	10 50	2.0 10.0	25	6	5 25	4	0 36	0.1 0.5	1.7 8.5
2-5	H	B 12+46	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
2-6	1	B 12+86	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
2-12 2-7	L H	B 12+90 B 13+43	RT LT	5 25	4.0 20.0	10 50	2.0 10.0	25	6	5 25	4	0 36	0.1 0.5	1.7 8.5
2-7	Ī	B 13+43	RT	17.5	14.0	35	7.0		6	17.5	3	23	0.5	6.0
2-8	N	B 14+13	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
2-9	I	B 14+54	LT	25	20.0	50	10.0	25	6	25	4	36	0.5	8.5
3-9 3-1	K D	B 15+12 B 15+53	RT LT	5 25	4.0 20.0	10 50	2.0 10.0	25	6	5 25	4	<u>0</u> 36	0.1 0.5	1.7 8.5
3-1	D	B 15+97	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
3-3	L	B 16+24	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
3-10	В	B 16+76	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
3-4 3-5	E E	B 17+26 B 17+95	LT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
3-11	M	B 18+08	RT	5	4.0	10	2.0	25	6	5	·	0	0.1	1.7
3-6	E	B 18+33	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
3-12 3-13	M M	B 18+44 B 19+07	RT RT	5 5	4.0 4.0	10 10	2.0	25 25	6	5 5		0	0.1	1.7 1.7
3-13	A	B 19+07	LT	25	20.0	50	10.0	25	6	25	4	36	0.1	8.5
3-14	K	B 19+72	RT	5	4.0	10	2.0	25	6	5	·	0	0.1	1.7
3-8	A	B 19+84	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
3-15 4-1	K N	B 20+12 B 20+27	RT LT	5 5	4.0 4.0	10 10	2.0	25 25	6	5 5		0	0.1	1.7 1.7
4-9	M	B 21+30	RT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
4-10	М	B 21+33	RT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
4-2 4-3	J	B 21+74 B 22+25	LT LT	25 25	20.0	50	10.0 10.0		6	25 25	4	36	0.5 0.5	8.5 8.5
4-3	L	B 22+23	LT	5	4.0	50 10	2.0	25	6	5	4	36 0	0.5	1.7
4-5	J	B 22+83	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
4-6	С	B 23+92	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
4-11 4-7	B	B 24+73 B 24+91	RT LT	17.5 5	14.0 4.0	35 10	7.0 2.0	25	6	17.5 5	3	23 0	0.4	6.0 1.7
4- <i>7</i> 4-8	C	B 24+91 B 25+27	LT	25	20.0	50	10.0	23	6	25	4	36	0.1	8.5
5-7	В	B 25+57	RT	17.5	14.0	35	7.0		6	17.5	3	2 3	0.4	6.0
5-1	N	B 25+75	LT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
5-2 5-3	N C	B 25+97 B 26+30	LT LT	5 25	4.0 20.0	10 50	2.0 10.0	25	6 6	5 25	4	0 36	0.1	1.7 8.5
5-8	В	B 26+52	RT	17.5	14.0	35	7.0		6	17.5	3	2 3	0.4	6.0
5-9	A	B 28+32	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
5-4 5-5	A H	B 28+53 B 29+03	LT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
5-10	Н	B 29+05	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
5-11	М	B 30+05	RT	5	4.0	10	2.0	25	6	5		0	0.1	1.7
5-6	D	B 30+33	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
5-12 6-1	D D	B 30+50 B 31+16	RT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
6-2	C	B 33+30	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-5	В	B 33+41	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-3	F	B 34+36	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-6 6-7	G K	B 34+36 B 34+90	RT RT	25 5	20.0 4.0	50 10	10.0 2.0	25	6	<u>25</u> 5	4	36 0	0.5 0.1	8.5 1.7
6-4	C	B 35+41	LT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-8	F	B 35+60	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
6-9	K	B 36+18	RT	5	4.0	10	2.0	25	6	5	Δ.	0	0.1	1.7
6-10 6-11	P P	B 31+72 B 31+96	RT LT	25 25	20.0	50 50	10.0 10.0		6	25 25	4	36 36	0.5 0.5	8.5 8.5
7-11	F	B 36+67	RT	25	20.0	50	10.0		6	25	4	36	0.5	8.5
			OTALS:	1340	1072	2680	536	725	468	1340	192	1712	29	457
		Т	OTALS:	1340	1072	2680	536	725	468	1340	192	1712	29	

BIORETENTION AREAS

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

PIN 8780.49 RTE 32 KINGSTON CONNECTIVITY BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON COUNTY: ULSTER

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS TABLES

CONTRACT NUMBER DL34880

DRAWING NO. MST-8 SHEET NO. 24

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



CITY OF KINGSTON



FILE NAME = ...\CADD\878849_CPH_MST-9.dgr DATE/TIME = 11/25/2019 USER = tvyce

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019

ELECTRIC MANHOLES STATION OFFSET QUANTITY B 24+40 38.2' B 24+47 TOTAL: *BY OTHERS, SEE PROPOSAL FOR UTILITY COORDINATION NOTES. NO DIRECT PAYMENT

STORMWATER MANHOLES								
STATION	SIDE	OFFSET	ITEM 660.65000101 (EA)					
B 6+18	RT	13.6'	1					
B 8+15	RT	14.3'	1					
B 10+21	RT	13.3'	1					
B 12+19	RT	12.8'	1					
B 14+15	RT	13.0'	1					
B 16+21	RT	12.9'	1					
B 17+42	RT	12.7'	1					
B 18+26	RT	13.0'	1					
B 19+44	RT	12.3'	1					
B 20+23	RT	12.5'	1					
B 20+92	RT	11.6'	1					
B 20+96	RT	28.4'	1					
B 22+09	RT	6.8'	1					
B 24+41	RT	6.6'	1					
B 25+81	RT	7.2'	1					
B 27+23	RT	20.5'	1					
B 27+68	RT	18.0'	1					
B 29+16	RT	10.0'	1					
B 29+43	RT	9.7'	1					
B 30+66	RT	16.1'	1					
B 31+32	RT	16.4'	1					
B 32+22	RT	16.5'	1					
B 32+86	RT	16.9'	1					
B 34+21	RT	17.8'	1					
B 35+94	RT	18.3'	1					
B 36+52	RT	18.6'	1					
B 25+48	LT	42.8'	1					
B 27+43	LT	62.1'	1					

TOTAL:

28

ALTERED BY: ON:

TELEPHONE MANHOLES					
TATION	SIDE	OFFSET	QUANTITY		
B 7+65	RT	16.6'	1		
3 10+40	RT	35.1'	1		
3 11+62	RT	16.8'	1		
3 14+30	RT	14.6'	1		
3 14+75	RT	37.5'	1		
3 16+54	RT	17.6'	1		
3 21+10	RT	14.7'	1		
3 21+45	RT	36.1'	1		
3 21+45	RT	32.1'	1		
3 24+29	RT	12.9'	1		
3 24+56	RT	34.5'	1		
3 24+62	RT	34.7'	1		
3 26+96	RT	13.8'	1		
3 26+96	RT	32.8'	1		
3 28+18	RT	32.1'	1		
3 36+75	RT	13.2'	1		
3 21+17	LT	50.0'	1		
3 21+18	LT	20.9'	1		
·	·	TOTAL:	18		

*BY OTHERS, SEE PROPOS	SAL FOR UTILITY
COORDINATION NOTES. NO	DIRECT PAYMENT

SEWER MANHOLES

OFFSET

7.2'

7.0'

16.6'

7.7'

7.7' 7.9'

8.9'

40.3'

51.7'

19.0'

24.9'

9.5'

9.4'

7.9'

17.9'

6.9'

27.2'

18.9'

8.2' TOTAL:

660.65000101 (EA)

21

SIDE

LT

LT LT

LT

LT

LT

LT

LT

LT

RT

RT RT

RT

RT

RT

RT

RT

RT

RT

STATION

B 8+56

B 11+90

B 11+97

B 14+26 B 16+61

B 19+66

B 21+05

B 21+18

B 29+64

B 12+22

B 14+32

B 19+51

B 22+01

B 24+35

B 27+28

B 29+50 B 33+09

B 34+40

B 34+41

B 36+64

TELEFTIONE WANTOLES						
TATION	SIDE	OFFSET	QUANTITY			
B 7+65	RT	16.6'	1			
3 10+40	RT	35.1'	1			
3 11+62	RT	16.8'	1			
3 14+30	RT	14.6'	1			
3 14+75	RT	37.5'	1			
3 16+54	RT	17.6'	1			
3 21+10	RT	14.7'	1			
3 21+45	RT	36.1'	1			
3 21+45	RT	32.1'	1			
3 24+29	RT	12.9'	1			
3 24+56	RT	34.5'	1			
3 24+62	RT	34.7'	1			
3 26+96	RT	13.8'	1			
3 26+96	RT	32.8'	1			
3 28+18	RT	32.1'	1			
3 36+75	RT	13.2'	1			
3 21+17	LT	50.0'	1			
3 21+18	LT	20.9'	1			
	·	TOTAL:	18			
*BY OTHERS, SEE PROPOSAL FOR UTILITY						

10	TOTAL
AL FOR UTILITY	*BY OTHERS, SEE PROPOS
DIRECT PAYMENT	CORDINATION NOTES NO

ATION	SIDE	OFFSET	QUANTITY		
7+65	RT	16.6'	1		
10+40	RT	35.1'	1		
11+62	RT	16.8'	1		
14+30	RT	14.6'	1		
14+75	RT	37.5'	1		
16+54	RT	17.6'	1		
21+10	RT	14.7'	1		
21+45	RT	36.1'	1		
21+45	RT	32.1'	1		
24+29	RT	12.9'	1		
24+56	RT	34.5'	1		
24+62	RT	34.7'	1		
26+96	RT	13.8'	1		
26+96	RT	32.8'	1		
28+18	RT	32.1'	1		
36+75	RT	13.2'	1		
21+17	LT	50.0'	1		
21+18	LT	20.9'	1		
		TOTAL:	18		
*BY OTHERS, SEE PROPOSAL FOR UTILITY					

TOTAL:	18	
*BY OTHERS, SEE PROPOSAL	FOR UTILITY	

B 27+	52	RT	25.6'	1
			TOTAL:	1
		MANHOL	E SUMMA	RY TABLE
		UTILITY TYPE		ITEM
				660.65000101 (EA)
		ELECTRIC		2*
	STORMWATER		28	
	TELEPHONE		18*	
		SEWER		21
		WATER		1
			TOTAL:	50
*	BY	OTHERS, SE	E PROPOS	AL FOR UTILITY
coc	DRD	INATION N	IOTES. NO	DIRECT PAYMENT

WATER MANHOLES

OFFSET

660.65000101 (EA)

STATION

SIDE

B 6+29	LT	15.3'	1
B 6+34	LT	8.8'	1
B 6+35	RT	7.8'	1
B 6+89	LT	10.0'	1
B 7+60	LT	14.4'	1
B 8+12	RT	29.3'	1
B 8+53	LT	21.8'	1
B 8+58	LT	23.3'	1
B 11+17	LT	23.5'	1
B 11+17	LT	20.7'	1
B 11+25	LT	17.4'	1
B 11+47	LT	29.1'	1
B 11+86	LT	23.1'	1
B 11+90	LT	29.1'	1
B 11+94	LT	6.5'	1
B 12+15	LT	39.9'	1
B 12+33	LT	25.1'	1
B 12+87	LT	23.5'	1
B 13+04	LT	31.2'	1
B 13+40	LT	32.1'	1
B 14+20	LT	22.8'	1
B 14+31	LT	27.9'	1
B 15+22	LT	32.5'	1
B 15+78	LT	31.8'	1
B 16+71	LT	37.1'	1
B 16+77	LT	38.4'	1
B 16+80	LT	48.9'	1
B 16+82	LT	39.7'	1
B 16+84	LT	46.6'	1
B 16+92	LT	40.1'	1
B 17+61	LT	19.8'	1
B 17+77	LT	22.5'	1
B 18+16	LT	18.1'	1
B 19+36	RT	40.5'	1
B 19+47	LT	32.1'	1
B 19+79	LT	32.0'	1
B 19+88	RT	31.4'	1
B 20+05	LT	32.6'	1
B 20+84	LT	55.0'	1
B 21+57	LT	30.5'	1
B 21+62	LT	30.7'	1
B 21+84	LT	16.2'	1
B 21+86	LT	25.0'	1
B 22+24	LT	30.4'	1
B 22+43	LT	30.5'	1
B 22+72	LT	30.2'	1
B 23+54	LT	30.1'	1
B 23+79	LT	27.4'	1
B 23+86	RT	31.2'	1
B 26+59	RT	31.4'	1
B 27+41	LT	25.2'	1
B 27+41	LT	53.1'	1
		44.4'	
B 27+75	RT		1
B 27+85	LT	32.8'	1
B 27+98	LT	30.8'	1
	1.7	29.9'	1
B 28+04	LT		
	LT	27.3'	1
B 28+04		27.3' TOTAL:	1 60

VALVE BOX ADJUSTMENT - GAS

OFFSET

23.5'

23.6'

15.3'

QUANTITY

STATION

B 6+19

B 6+25

B 6+26

B 6+29

SIDE

RT

RT

LT

B 6+/3	LI	20.5	1
B 6+88	LT	20.9'	1
B 7+40	LT	19.6'	1
B 8+08	LT	20.4'	1
B 8+29	LT	12.5'	1
B 8+43	LT	21.3'	1
B 9+99	LT	26.6'	1
B 10+28	LT	18.6'	1
B 10+42	LT	26.0'	1
B 11+10	LT	28.4'	1
B 11+60	LT	29.7'	1
B 11+67	LT	27.1'	1
B 11+76	LT	23.2'	1
B 11+99		2.0'	1
	LT		1
B 12+99 B 13+30	LT	30.6'	
	LT	30.3'	1
B 14+89	LT	30.9'	1
B 15+49	LT	30.9'	1
B 15+52	LT	30.2'	1
B 15+78	LT	30.9'	1
B 15+85	LT	31.1'	1
B 16+53	LT	46.2'	1
B 17+44	LT	30.5'	1
B 17+71	LT	30.6'	1
B 18+18	LT	30.4'	1
B 18+47	LT	30.7'	1
B 20+19	LT	30.5'	1
B 20+21	LT	30.1'	1
B 21+08	LT	0.1'	1
B 21+19	LT	45.6'	1
B 23+51	LT	29.9'	1
B 23+81	LT	30.6'	1
B 24+03	LT	29.6'	1
B 24+61	LT	44.1'	1
B 24+84	LT	29.7'	1
B 25+06	LT	29.9'	1
B 25+48	LT	30.8'	1
B 25+67	LT	30.5'	1
B 25+78	LT	31.6'	1
			1
B 25+91	LT	30.3'	
B 26+06	LT	30.7'	1
B 26+07	LT	30.2'	1
B 26+56	LT	29.9'	1
B 26+85	RT	31.5'	1
B 28+30	LT	29.8'	1
B 31+31	LT	40.1'	1
B 35+23	LT	36.8'	1
B 35+60	LT	29.5'	1
B 36+02	LT	28.5'	1
B 7+46	RT	29.0'	1
B 7+47	RT	29.0'	1
B 10+07	RT	29.8'	1
B 11+39	RT	44.8'	1
B 11+59	RT	43.8'	1
B 11+99	RT	2.6'	1
B 12+33	RT	31.3'	1
B 12+81	RT	30.9'	1
B 14+12	RT	41.3'	1
В 17+19	RT	27.6'	1
B 21+04	RT	45.2'	1

VALVE BOX ADJUSTMENT - WATER

OFFSET

19.4'

19.6'

20.5'

663.33 (EA)

STATION

B 6+19

B 6+49

B 6+73

SIDE

LT

LT

B 21+07 B 22+11 B 23+46 B 23+50 B 24+81 B 26+26 B 26+85	SIDE RT RT RT RT RT RT RT RT	0FFSET 41.4' 30.5' 30.5' 24.3' 31.7'	1TEM 663.33 (EA) 1 1 1 1
B 22+11 B 23+46 B 23+50 B 24+81 B 26+26 B 26+85	RT RT RT RT	30.5' 30.5' 24.3' 31.7'	1 1 1
B 23+46 B 23+50 B 24+81 B 26+26 B 26+85	RT RT RT	30.5' 24.3' 31.7'	1 1
B 23+50 B 24+81 B 26+26 B 26+85	RT RT	24.3' 31.7'	1
B 24+81 B 26+26 B 26+85	RT	31.7'	_
B 26+26 B 26+85			1
B 26+85	RT		
		30.4'	1
	RT	31.6'	1
B 27+25	RT	51.0'	1
B 27+31	RT	56.7'	1
B 27+40	RT	18.5'	1
B 27+90	RT	33.8'	1
B 28+27	RT	19.9'	1
B 29+12	RT	37.4'	1
B 29+35	RT	16.3'	1
B 29+58	RT	37.0'	1
B 30+20	RT	25.8'	1
B 31+06	RT	26.1'	1
B 31+27	RT	19.9'	1
B 33+35	RT	26.5'	1
B 33+50	RT	40.4'	1
B 34+33	RT	27.4'	1
B 34+85	RT	19.6'	1
B 36+58	RT	19.4'	1
B 36+59	RT	24.5'	1
B 36+66	RT	24.4'	1
B 36+83	RT	21.1'	1
B 36+91	RT	24.7'	1
B 37+59	RT	24.5'	1
		TOTAL:	90

VALVE BOX ADJUSTMENT - UNKNOWN				
STATION	SIDE	OFFSET	ITEM 663.33 (EA)	
B 5+72	LT	27.5'	1	
B 19+39	LT	31.3'	1	

AS: DE:		

			TO	TAL: 60		B 17+19	RT	27.6'	
			*BY OTHERS, SEE PRO	POSAL FOR UTILITY	'	B 21+04	RT	45.2'	
			COORDINATION NOTES	. NO DIRECT PAYME	NT				
					•				
	AS-BUILT REVISIONS	KINGSTON CONNECTIV	/ITY		PIN 8780.4	19	В	RIDGES	CUL
1	DESCRIPTION OF ALTERATIONS:	BROADWAY STREETSCAPE PROJECT			RTE 32				
		CITY OF KINGSTON							
		CITT OF KINGSTON			1				1
					4				
		COUNTY: ULSTER		REGION: 8					
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURV									
	TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LI SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"								
	SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	FULLOWED BY THEIR	SIGNATURE, THE DATE OF S	UCH ALIEKATION, A	NU A SPECIF	IC DESCRIPTI	UN UF IN	E ALIEKATIU	l

	CULVERTS	ALL DIMENSIONS IN ++ UNLESS OTHERWISE NOTED	CONTRACT NUMBER		
			DL34880		
		MISCELLANEOUS TABLES	DRAWING NO. MST-9 SHEET NO. 25		
í(I	OR, ?	GPI GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS	CITY OF KINGSTON		

FILE NAME = ...\CADD\878049_CPH_MST-18.dgn DATE/TIME = 11/25/2019 USER = tvyce*

JOB

MAP# PARCEL

101 101

105 105

110 110

114 114

102

103

104

106

107

109

111

102

103

104

106

107

109

111

112

OWNER

DAGO ANTONIO MERAZ

VIRK CONVENIENT MART, INC.

BHAI-BOON, INC. (ANTHONY G. SACCOMAN)

EARL B. FEIDEN. INC. (ARTHUR FERRARO)

A&P FERRARO CORPORATION

CB 635, LLC

YI YUAN INC

THE PEOPLE OF THE STATE OF NEW YORK

BARDAVON 1869 OPERA HOUSE, INC. (UPAC)

GARRY J. TUMA

BASEMENT REALTY, INC.

MATTEO-BELLA, LLC

113 113 KINGSTON BROADWAY FD, LLC (HARLAND GENTILIN)

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019
PEDERSON AND AND AND AND AND AND AND AND AND AN

ALTERED BY: ON:

AS-BUILT REVISIONS		KINGSTON CONNECTIV		
DESCRIPTION OF ALTERATIO	NS:			BROADWAY STREETSC
				CITY OF KINGSTON
				COUNTY: ULSTER

	KINGSTON CONNECTIVITY		PIN 8780.49
	BROADWAY STREETSCAPE PROJECT		RTE 32
	CITY OF KINGSTON		
	COUNTY: ULSTER	REGION: 8	
TMC I	INDED THE DIDECTION OF A LICENSED DROCESSIONAL	ENCINEED ADO	HITECT LANDSCA

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

GPP GREENMAN-PEDERSEN, INC.
CONSULTING ENGINEERS

MISCELLANEOUS TABLES

CONTRACT NUMBER DL34880

DRAWING NO. MST-10 SHEET NO. 26

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

CITY OF KINGSTON

THE DE NEW YAR	
STRUM PEDERCE	
No. 131	
POFESSIONAL	

PROPERTY RELEASES									
STATION	SIDE	HOUSE	PROPERTY OWNER(S)	RELEASE	COMMENTS, INCLUDE PURPOSE	DATE			
STATION		NUMBER	PROPERTY OWNER(S)	TYPE	COMMENTS, INCLUDE FURFUSE				
B 14+75	RT	625	KAMRAN KHAN	GRADING	RE-ESTABLISH APPROACHES TO PRIVATE LAND (DRIVEWAYS/WALKWAYS)	7/5/2019			

TABLE OF RIGHT-OF-WAY ACQUISITIONS

LOT

21

32.1

14

14

20

43

19

22

16.1

LIBER PAGE

TAX MAP INFORMATION

3

BLOCK

SECTION

56.092

56.093

56.025

56.093

56.093

56.109

56.025

56.109

56.109

56.109

56.025

56.109

56.025

SIZE OF TOTAL

PARCEL

4344 80 6,404 SF (0.147 AC.) 39 SF (0.001 AC.)

3265 173 12,894 SF (0.296 AC.) 66 SF (0.002 AC.) 3502 362 3,224 SF (0.074 AC.) 13 SF (0.001 AC.)

1646 56 14,419 SF (0.331 AC.) 6 SF (0.001 AC.)

2790 300 12,241 SF (0.281 AC.) 61 SF (0.001 AC.)

 5942
 12
 13,286 SF (0.305 AC.)
 40 SF (0.001 AC.)

 5892
 271
 24,002 SF (0.551 AC.)
 55 SF (0.001 AC.)

1845 161 29,578 SF (0.679 AC.) 241 SF (0.006 AC.) 4493 178 24,873 SF (0.571 AC.) 424 SF (0.010 AC.)

1707 255 9,720 SF (0.223 AC.) 15 SF (0.001 AC.)

1694 89 3,572 SF (0.082 AC.) 20 SF (0.001 AC.)

3490 60 7,928 SF (0.182 AC.) 184 SF (0.004 AC.)

5437 69 4,226 SF (0.097 AC.) 16 SF (0.001 AC.)

SIZE OF PORTION TO

BE ACQUIRED

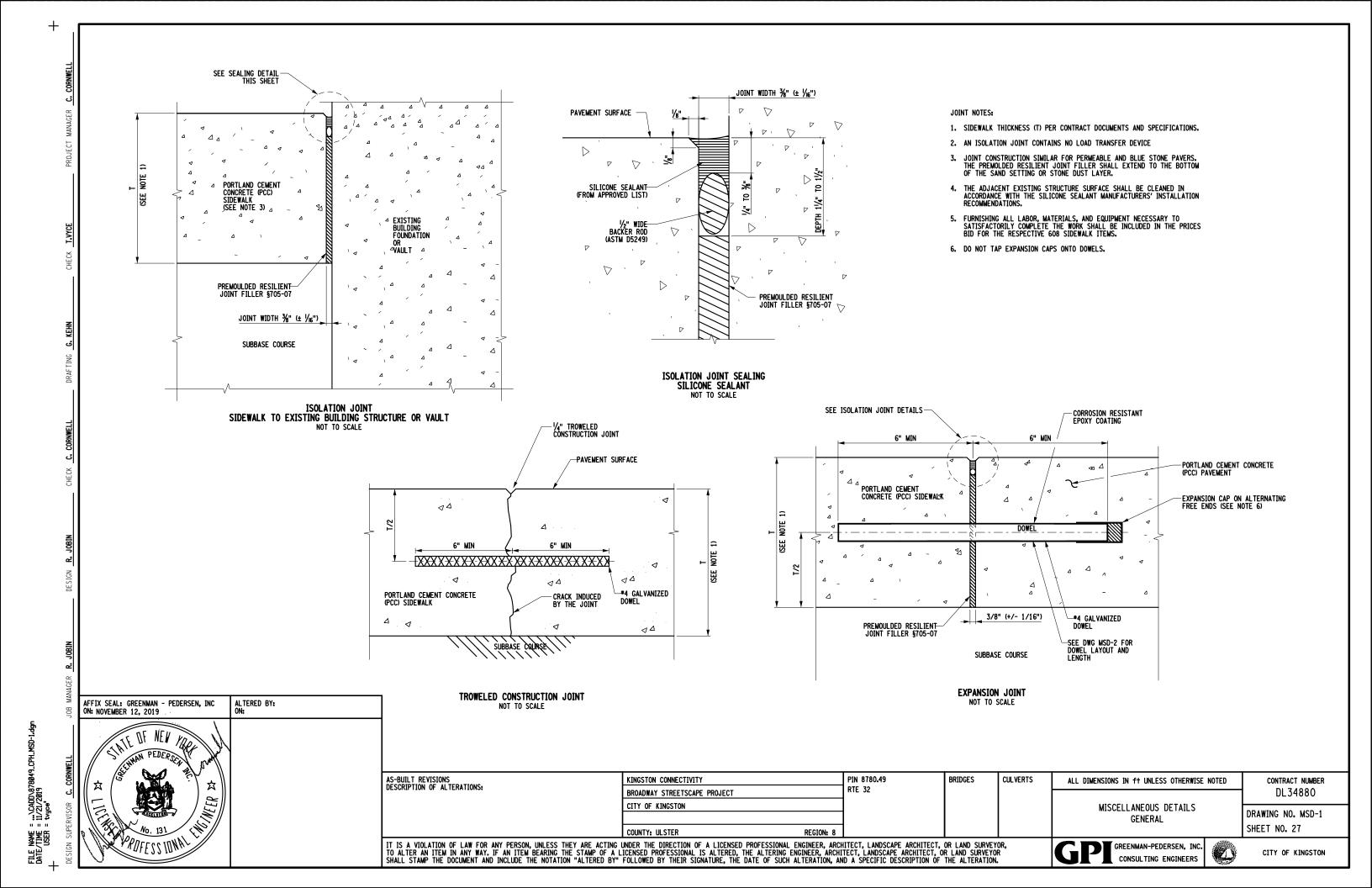
TYPE OF

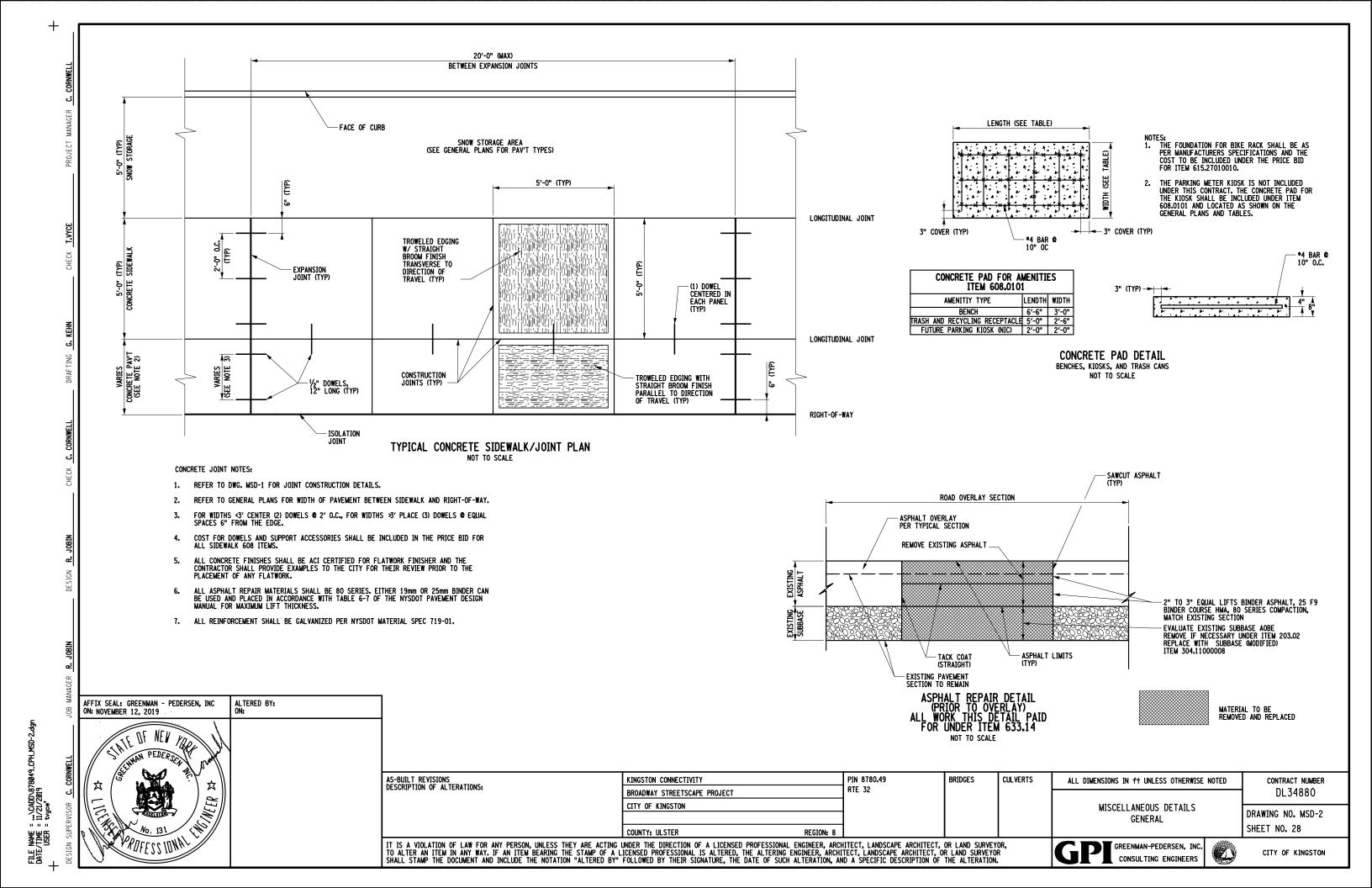
ACQUISITION

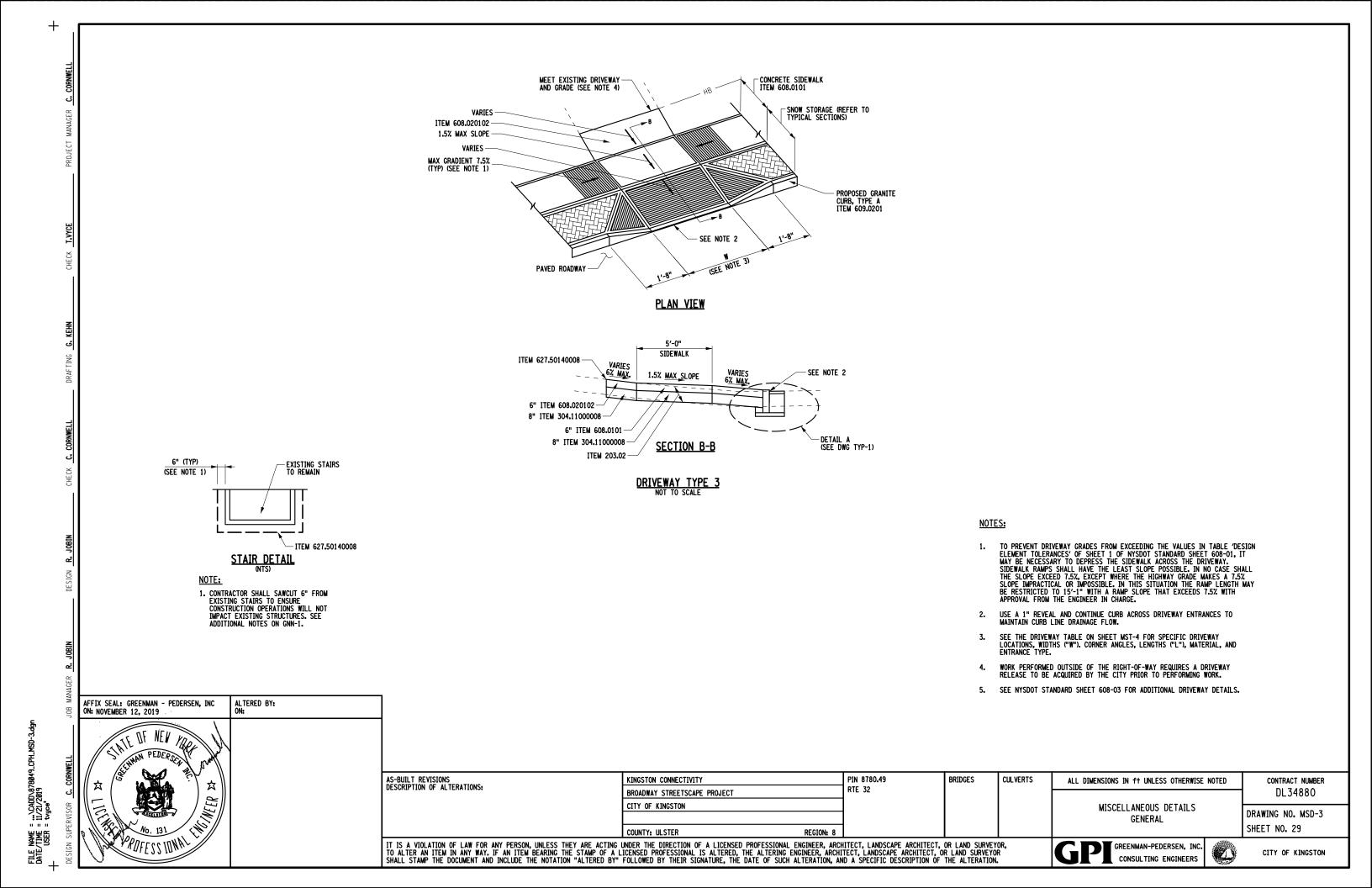
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PE

PE







FILE NAME = ...\CADD\878849_CPH_MSD-4.dgr DATE/TIME = 11/21/2019 USER = tvyce'

ALTERED BY: ON:

AT DRIVEWAYS (REFER TO SIGN LOCATION PLANS)

₹ 0 **₹**

ITEM 645.5102 W/ ITEM 645.81 (1 EACH)

4" WIDE - YELLOW, NORMAL DOTTED EXTENSION LINE, ITEM 685.12

ALL TYPE 'A' SIGN POSTS INSTALLED IN CONCRETE OR PRECAST PAVER AREAS SHALL UTILIZE A PVC CONDUIT

SLEEVE, PAID FOR UNDER ITEM 645.81.

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

CITY OF KINGSTON COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

BROADWAY STREETSCAPE PROJECT

KINGSTON CONNECTIVITY

-4" WIDE - WHITE, NORMAL DOTTED EXTENSION LINES, ITEM 685.11 (EACH SIDE)

PIN 8780.49 RTE 32

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED MISCELLANEOUS DETAILS

GENERAL

CONTRACT NUMBER DL34880

DRAWING NO. MSD-4 SHEET NO. 30

GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

CITY OF KINGSTON

CYCLE TRACK CONFLICT ZONE MARKINGS AT INTERSECTIONS, DRIVEWAYS AND BUS LOADING ZONES
NOT TO SCALE

2'-0" (TYP) ►

CYCLE 1

4'-0"

(SEE NOTE)

LENGTH OF CONFLICT ZONE (REFER TO GENERAL PLANS)

DRIVEWAY (SHOWN) INTERSECTION SIMILAR (REFER TO GENERAL PLANS)

- MIDPOINT OF CONFLICT ZONE (SEE NOTE)

2' x FULL WIDTH - GREEN SURFACE TREATMENT, ITEM 601.01000018 (SPACED AS SHOWN)

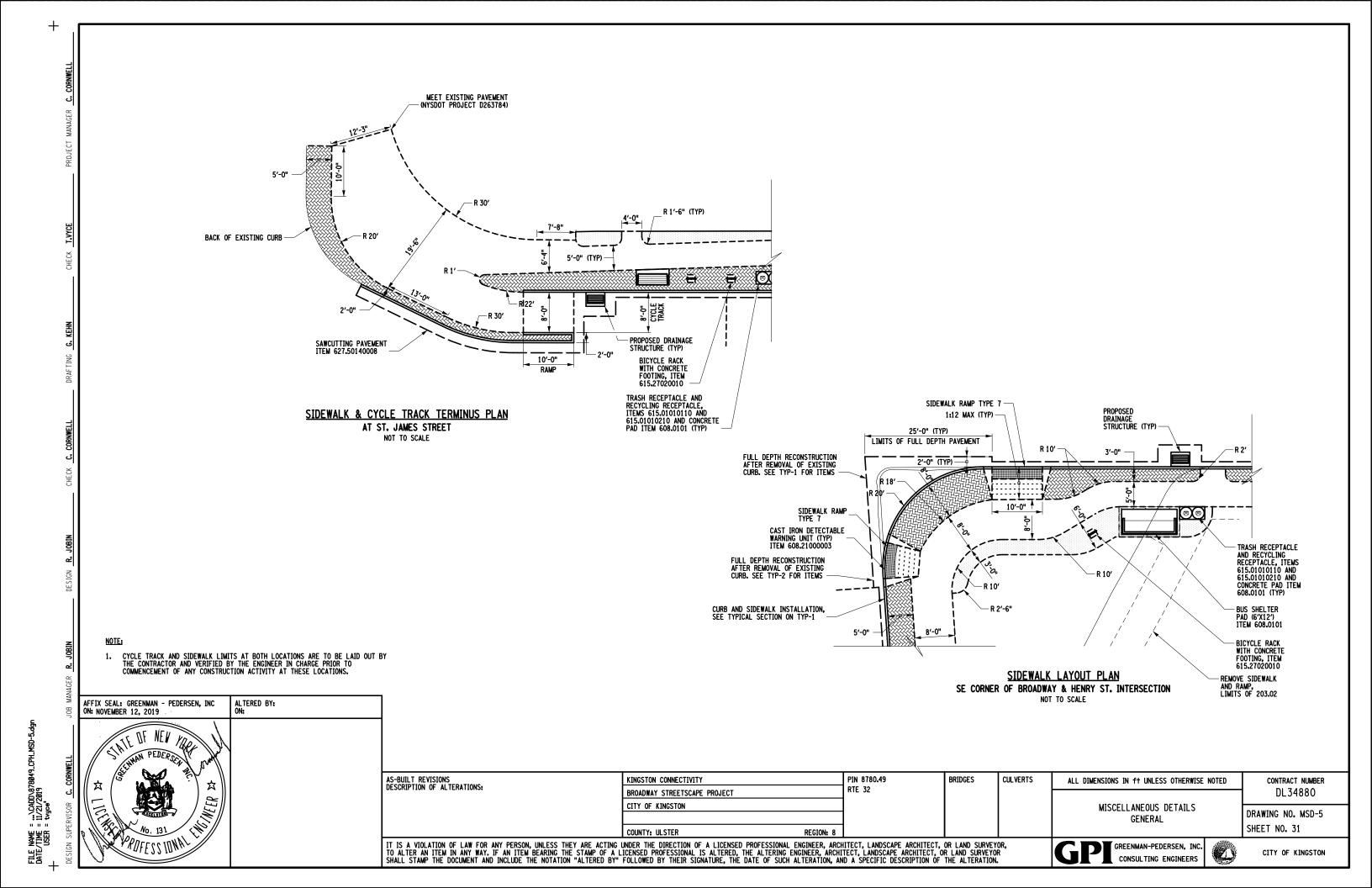
YELLOW CYCLE TRACK CENTER LINE (TYP) ITEM 685.12 CENTER CONFLICT ZONE MARKINGS ON THE MIDPOINT OF THE CONFLICT ZONE. SPACE BETWEEN MARKINGS MAY BE REDUCED BY A MAXIMUM OF 6 INCHES TO PROVIDE THE MAXIMUM NUMBER OF MARKINGS WITHIN THE CONFLICT ZONE.

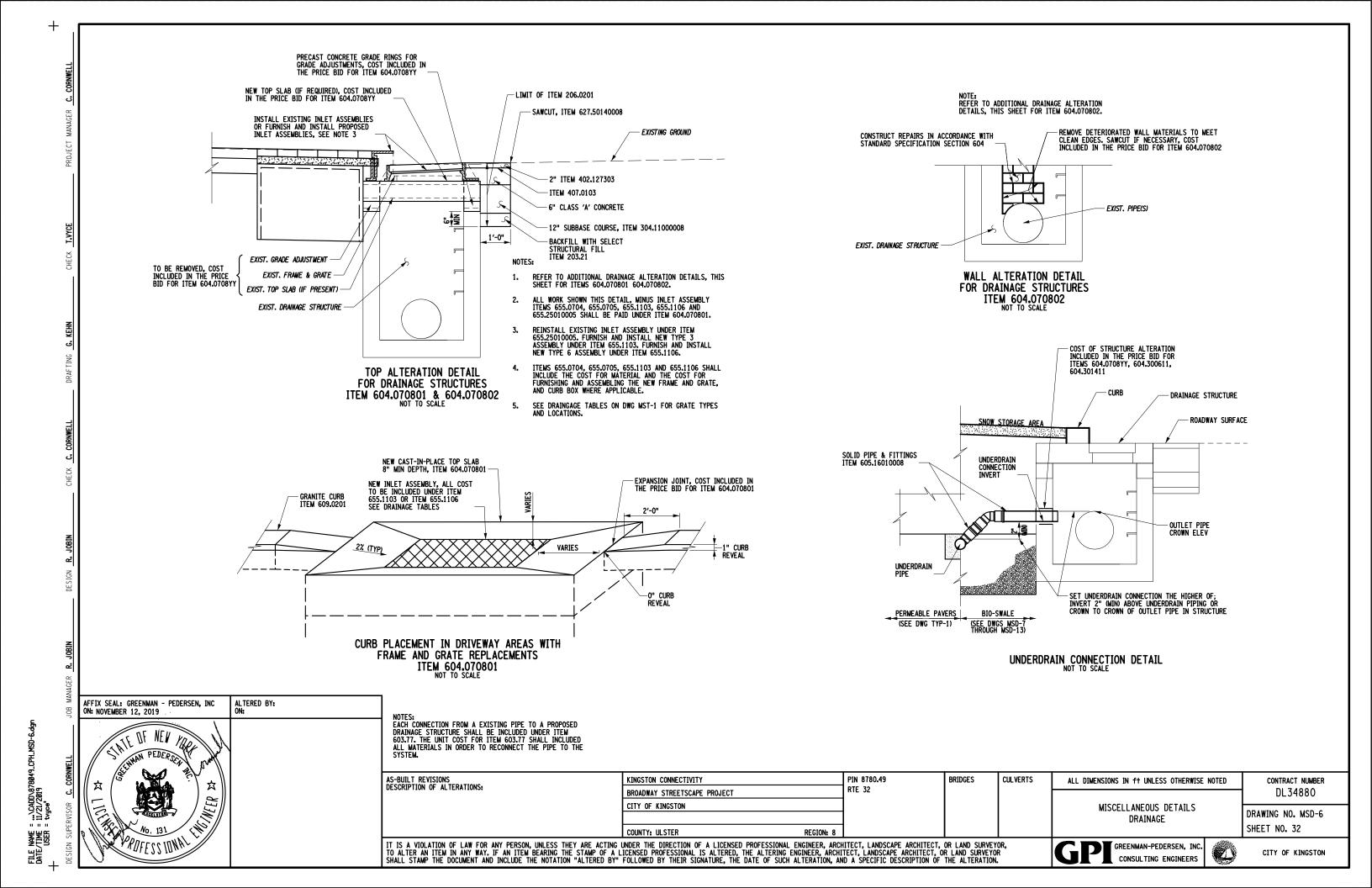
4" WIDE - WHITE, EDGE LINES, ITEM 685.11 (BOX PERIMETER) (1) WHITE BICYCLE SYMBOL, & (1) WHITE (HALF SIZE) TURNING ARROW PER MODULE, ITEM 685.14 MIRROR MODULE AS REQUIRED REFER TO PAVEMENT MARKING PLANS) GREEN SURFACE TREATMENT, ITEM 601.01000018 8'-0"

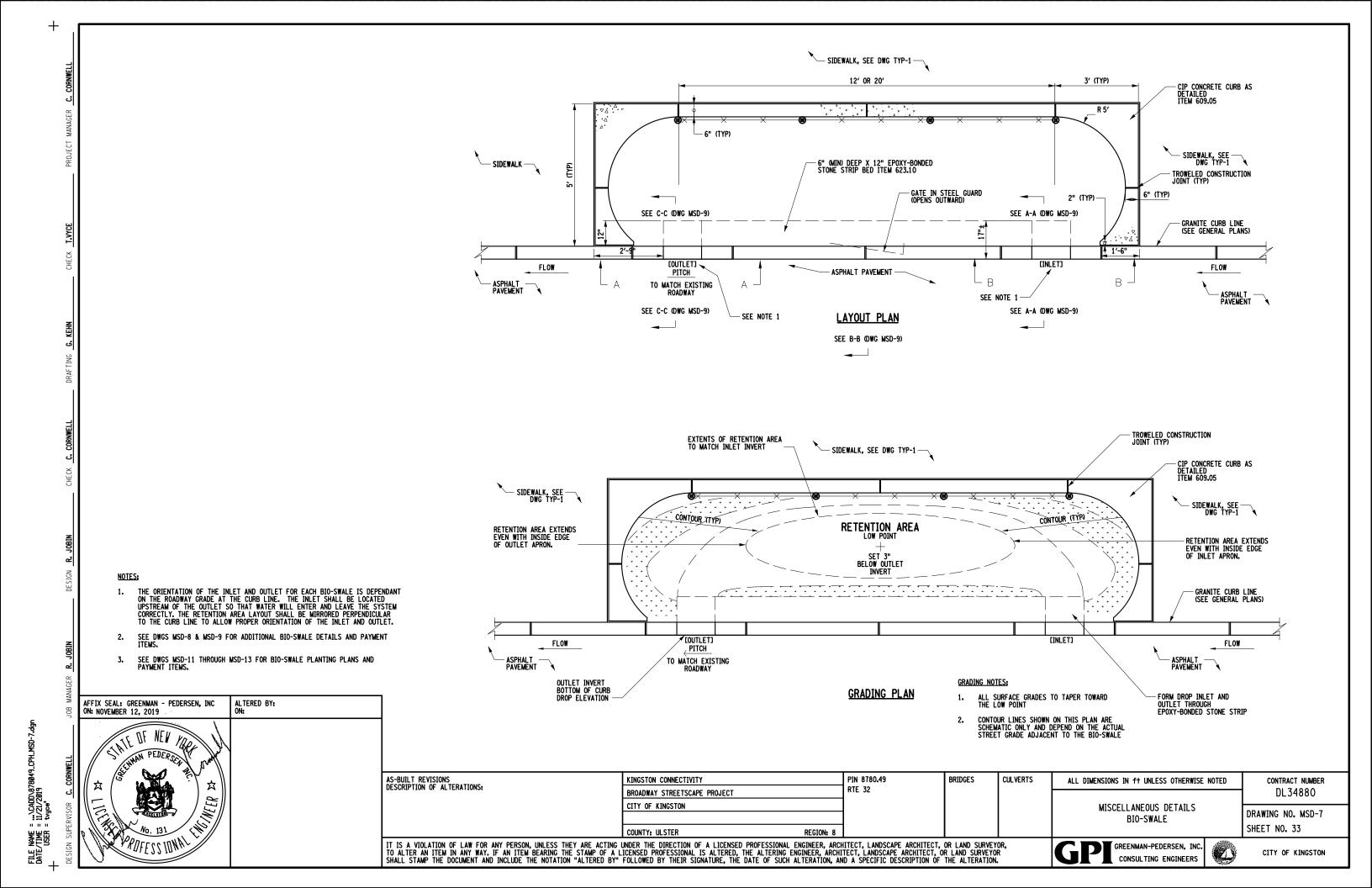
MODULE WIDTH

BIKE BOX MARKINGS

AT SIGNALIZED INTERSECTIONS
NOT TO SCALE







FILE NAME = ...\CADD\878849.CPH_MSD-DATE/TIME = 11/21/2019 USER = tvyce*

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 ALTERED BY: ON:

EXTENT OF NEW CURB VARIES - (REFER TO GENERAL PLANS) PROPOSED PAVEMENT SURFACE TYPICAL CURB JOINT 6" (TYP) INLET OR OUTLET -SLOT CUT IN CURB COST INCLUDED IN PRICE BID FOR ITEM 609.0201 - GRANITE CURB

SECTION AT CURB INLET/OUTLET

NOT TO SCALE

NOTES:

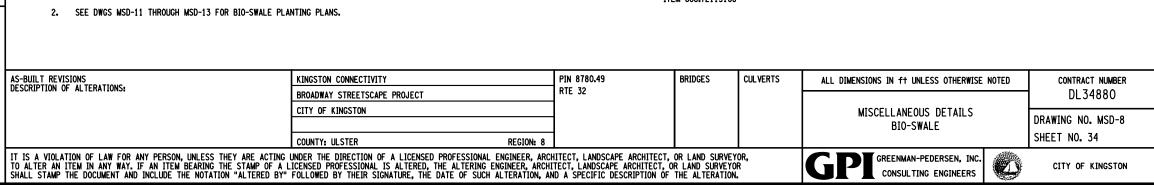
1. SEE DWGS MSD-7, MSD-8 & MSD-10 FOR ADDITIONAL BIO-SWALE DETAILS.

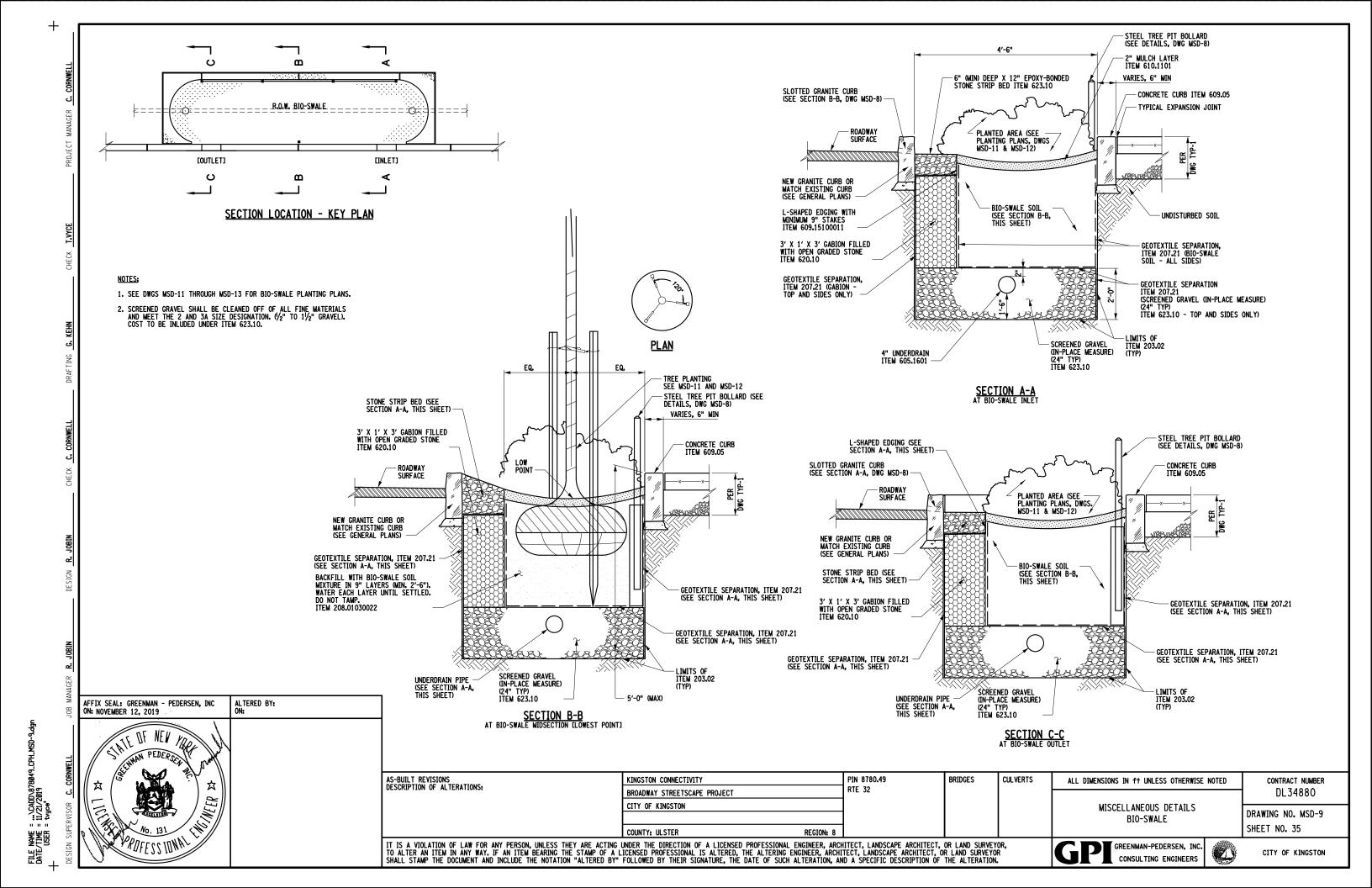
- D-RING Connector (Typ) - 3/8" GALVANIZED LANDSCAPE CHAIN, POWDER COATED BLACK 2'-0" GALVANIZED I-BEAM TYPE POST, POWDER COATED BLACK, ACTUAL SIZE 2.25"x1.75" 4'-0" -1% CROWN ON CONCRETE FOOTING 6" MIN - CLASS D CONCRETE FOOTING 4'-4"x6" MIN. SET IN SONOTUBE (TYP)

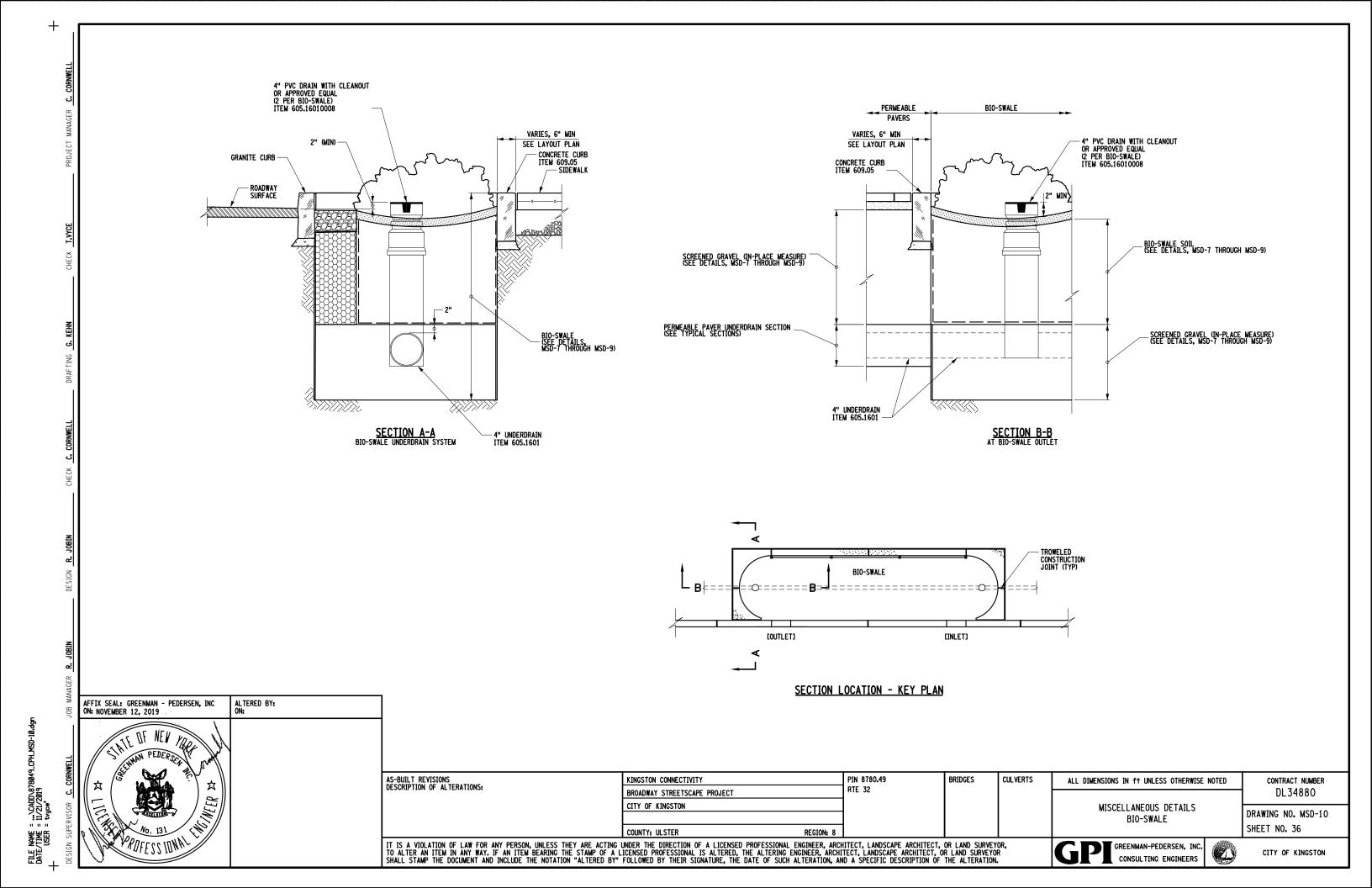
BIO-SWALE BOLLARD NOTES:

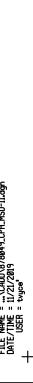
- POST AND CAP SHALL BE SNUG COTTAGE HARDWARE CENTRAL PARK STYLE CAP AND POST OR SIMILAR AS APPROVED BY RESIDENT ENGINEER.
- 2. ALL STEEL SHALL CONFORM TO ASTM-A36 SPECIFICATION.
- 3. ALL STEEL TO BE PAINTED BLACK AND FOLLOW SPECIAL SPECIFICATION FOR ITEM 608.72113108 AND SECTION 657 OF THE STANDARD SPECIFICATIONS.
- 4. BOLLARD SPACING FOR FOUR-BOLLARD BIO-SWALES SHALL BE 6'-8" ON CENTER; SPACING FOR THREE-BOLLARD BIO-SWALES SHALL BE 6'-0" ON CENTER.
- 5. AN ADDITIONAL 10% SHALL BE ADDED TO THE MEASURED LENGTH NEEDED FOR EACH SECTION OF CHAIN TO ALLOW FOR PROPER SAG.
- 6. ALL WORK THIS DETAIL SHALL BE PAID FOR UNDER ITEM 608.72113108.

BIO-SWALE BOLLARDS ITEM 608.72113108









AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 ALTERED BY: ON: FILE NAME = ...\CADD\878849.CPH_MSD-11.dgr DATE/TIME = 11/21/2819 USER = tvyce'

1. ALL BIO-SWALES SHALL HAVE A PORTABLE DRIP IRRIGATION SYSTEM INSTALLED UNDER ITEM 611.17 TO BE LEFT INSTALLED UPON PROJECT COMPLETION.

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

(20) E UNDERDRAIN CLEAN OUT - UNDERDRAIN CLEAN OUT PLANT SCHEDULE - OPTION A

QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING
1	А	TREE	AMELANCHIER CANADENSIS 'AUTUMN BRILLINACE'	SHADBLOW SERVICEBERRY	B&B *3"	611.0171	N/A
6	8	SHRUB	ILEX GLABRA "SHAMROCK"	INKBERRY	4 FOOT/SPREAD	611.0461	2' O.C.
6	C	PERENNIAL	HIBISCUS MOSCHEUTOS 'LUNA PINK'	HARDY HIBISCUS	#2 CONTAINER	611.0751	12" O.C.
18	Ð	GRASSES	CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#Z CONTAINER	611.0751	12" O.C.
20	£	GRASSES	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.

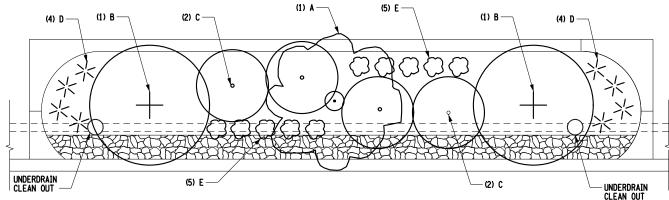
BIORETENTION PLANTING PLAN - OPTION A NOT TO SCALE

	PLANT SCHEDULE - OPTION B										
QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING				
1	А	TREE	NYSSA SYLVATICA	BLACK TUPELO	B&B *3"	611.0171	N/A				
6	8	SHIRUB	ILEX GLABRA 'SHAMROCK'	INKBERRY	4 FOOT/SPREAD	611.0461	2' O.C.				
6	С	PERENNIAL	HIBISCUS MOSCHEUTOS 'LUNA PINK'	HARDY HIBISCUS	#2 CONTAINER	611.0751	12" O.C.				
18	Ð	GRASSES	CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#2 CONTAINER	611.0751	12" O.C.				
20	ε	GRASSES	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.				

BIORETENTION PLANTING PLAN - OPTION B

	PLANT SCHEDULE - OPTION C										
QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING				
1	А	TREE	CELTIS OCCIDENTAILS	HACKBERRY	B&B *3"	611.0171	N/A				
6	1 8	SHRUB	ILEX GLABRA 'SHAMROCK'	INKBERRY	4 FOOT/SPREAD	611.0461	2' O.C.				
6	C	PERENNIAL	HIBISCUS MOSCHEUTOS 'LUNA PINK'	HARDY HIBISCUS	#2 CONTAINER	611.0751	12" O.C.				
18	Ð	GRASSES	CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#2 CONTAINER	611.0751	12" O.C.				
20	ε	GRASSES	PENNISETUM ALOPECUROIDES 'LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.				

BIORETENTION PLANTING PLAN - OPTION C



PLANT SCHEDULE - OPTION D										
ΚEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING				
А	TREE	AMELANCHIER CANADENSIS 'AUTUMN BRILLINACE'	SHADBLOW SERVICEBERRY	8&B *3"	611.0171	N/A				
В	SHRUB	MYRICA PENNSYLVANICA	BAY8ERRY	3 FOOT/SPREAD	611.0452	AS SHOWN				
¢	SHRUB	HYPERICUM DENSIFLORUM	ST. JOHN'S WORT	2 FOOT/SPREAD	611.0442	36" O.C.				
D	GRASSES	CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#2 CONTAINER	611.0751	12" O.C.				
E	GRASSES	PENNISETUM ALOPECUROIDES'LITTLE BUNNY	F	#2 CONTAINER	611.0751	12" O.C.				
	A B C	A TREE B SHRUB C SHRUB D GRASSES	KEY TYPE BOTANICAL NAME A TREE AMELANCHIER CANADENSIS 'AUTUMN BRILLINACE' B SHRUB MYRICA PENNSYLVANICA C SHRUB HYPERICUM DENSIFLORUM	KEY TYPE BOTANICAL NAME COMMON NAME A TREE AMELANCHIER CANADENSIS 'AUTUMN BRILLINACE' SHADBLOW SERVICEBERRY B SHRUB MYRICA PENNSYLVANICA BAYBERRY C SHRUB HYPERICUM DENSIFLORUM ST. JOHN'S WORT D GRASSES CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER' FEATHER REED GRASS	KEY TYPE BOTANICAL NAME COMMON NAME CONTAINER / SIZE A TREE AMELANCHIER CANADENSIS 'AUTUMN BRILLINACE' SHADBLOW SERVICEBERRY B&B +3" B SHRUB MYRICA PENNSYLVANICA BAYBERRY 3FOOT/SPREAD C SHRUB HYPERICUM DENSIFLORUM ST. JOHN'S WORT 2 FOOT/SPREAD D GRASSES CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER' FEATHER REED GRASS #2 CONTAINER	KEY TYPE BOTANICAL NAME COMMON NAME CONTAINER / SIZE ITEM NUMBER A TREE AMELANCHIER CANADENSIS 'AUTUMN BRILLINACE' SHADBLOW SERVICEBERRY B&B *3" 611.0171 B SHRUB MYRICA PENNSYLVANICA BAYBERRY 3 FOOT/SPREAD 611.0452 C SHRUB HYPERICUM DENSIFLORUM ST. JOHN'S WORT 2 FOOT/SPREAD 611.0442 D GRASSES CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER' FEATHER REED GRASS #2 CONTAINER 611.0751				

BIORETENTION PLANTING PLAN - OPTION D NOT TO SCALE

	PLANT SCHEDULE - OPTION E										
QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING				
1	Α	TREE	CELTIS OCCIDENTAILS	HACKBERRY	B&B *3"	611.0171	N/A				
2	В	SHRUB	MYRICA PENNSYLVANICA	BAYBERRY	3 FOOT/SPREAD	611.0452	AS SHOWN				
4	C	SHRUB	HYPERICUM DENSIFLORUM	ST. JOHN'S WORT	2 FOOT/SPREAD	611.0442	36" O.C.				
8	D	GRASSE5	CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#2 CONTAINER	611.0751	12" O.C.				
10	E.	GRASSES	PENN:SETUM ALOPECUROIDES'LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.				

BIORETENTION PLANTING PLAN - OPTION E NOT TO SCALE

PIN 8780.49

RTE 32

KINGSTON CONNECTIVITY

CITY OF KINGSTON

COUNTY: ULSTER

BROADWAY STREETSCAPE PROJECT

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

BRIDGES

CULVERTS

GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS DETAILS

PLANTINGS

CITY OF KINGSTON

DRAWING NO. MSD-11

SHEET NO. 37

CONTRACT NUMBER

DL34880



AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019

ALTERED BY: ON:

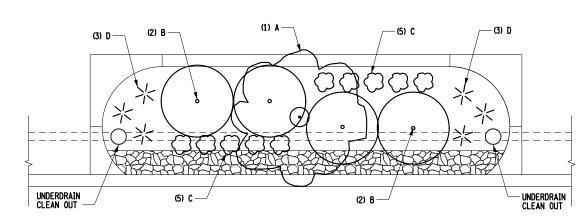
(1) A-(4) E – -(2) C (1) B-(1) B-(5) D – UNDERDRAIN CLEAN OUT (5) D --UNDERDRAIN CLEAN OUT

	PLANT SCHEDULE - OPTION F										
QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING				
1	А	TREE	AMELANCHIER CANADENSIS 'ROBIN HILL PINK'	SHADBLOW SERVICEBERRY	B&B *3"	611.0171	N/A				
2	8	SHRUB	PHYSOCARPUS OPULIFOLIUS	COMMON NINEBARK	3 FOOT/SPREAD	611.0452	AS SHOWN				
4	С	SHRUB	ARONIA ARBUTIFOLIA 'BRILLIANTISSIMA'	RED CHOKEBERRY	4 FOOT/SPREAD	611.0462	36" O.C.				
10	D	GRASSES	CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#2 CONTAINER	611.0751	12" O.C.				
- 8	Ę	GRASSES	PENNISETUMALOPECUROIDES 'LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.				

BIORETENTION PLANTING PLAN - OPTION F NOT TO SCALE

			PLANT SCHEDUL	E - OPTION G			
QTY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING
1	А	TREE	CELTIS OCCIDENTAILS	HACKBERRY	B&B *3"	611.0171	N/A
2	8	SHRUB	PHYSOCARPUS OPULIFOLIUS	COMMON NINEBARK	3 FOOT/SPREAD	611.0452	AS SHOWN
4	С	SHRUB	ARONIA ARBUTIFOLIA 'BRILLIANTISSIMA'	RED CHOKEBERRY	4 FOOT/SPREAD	611.0462	36" O.C.
10	ס	GRASSES	CALAMAGROSTIS x ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#2 CONTAINER	611.0751	12" O.C.
8	E	GRASSES	PENNISETUMALOPECUROIDES 'LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.

BIORETENTION PLANTING PLAN - OPTION G NOT TO SCALE

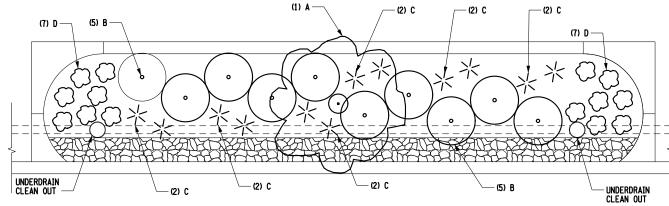


	PLANT SCHEDULE - OPTION J									
QIY.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING			
1	А	TREE	AMELANCHIER CANADENSIS 'ROBIN HILL PINK'	SHADBLOW SERVICEBERRY	B&B *3"	611.0171	N/A			
4	В	SHRUB	ARONIA ARBUTIFOLIA 'BRILLIANTISSIMA'	RED CHOKEBERRY	4 FOOT/SPREAD	611.0462	36" O.C.			
10	£	GRASSES	PENNISETUM ALOPECUROIDES' LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.			
6	D	PERENNIA	HIBISCUS MOSCHEUTOS 'LUNA PINK'	HARDY HIBISCUS	#2 CONTAINER	611.0751	12" O.C.			

BIORETENTION PLANTING PLAN - OPTION J

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

NOTE: 1. ALL BIO-SWALES SHALL HAVE A PORTABLE DRIP INSTALLED UNDER ITEM 611.17 TO BE LEFT INSTALLE COMPLETION.	
--	--

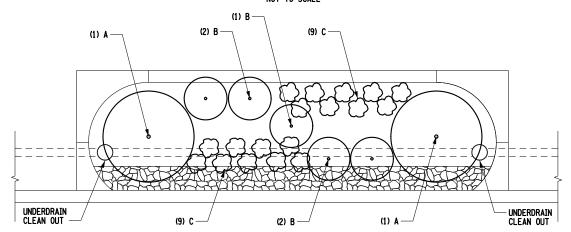


							CLEAN OUT
			PLANT SCHEDULE	- OPT!ON H			
QTY.	κĘΥ	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING
1	А	TREE	AMELANCHIER CANADENSIS 'AUTUMN BRILLINACE'	SHADBLOW SERVICEBERRY	B&B *3"	611.0171	N/A
10		PERENNIAL	SYMPHYTRICHUM NOVAE-ANGLIAE	NEW ENGLAND ASTER	3" POT	611.0751	24" O.C.
12	С	PERENNIAL	HIB:SCUS MOSCHEUTOS 'LUNA PINK'	HARDY HIBISCUS	#2 CONTAINER	611.0751	12" O.C.
14	Ð	GRASSES	PENNISETUM ALOPECUROIDES' LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.

BIORETENTION PLANTING PLAN - OPTION H NOT TO SCALE

	PŁANT SCHEDULE · OPTION I									
QTY.	KEY				CONTAINER / SIZE	ITEM NUMBER	SPACING			
1	А	TREE	CELTIS OCCIDENTAILS	HACKBERRY	B&B *3"	611.0171	N/A			
10				NEW ENGLAND ASTER	3" POT	611.0751	24" O.C.			
12	С	PERENNIAL	HIBISCUS MOSCHEUTOS 'LUNA PINK'	HARDY HIBISCUS	#2 CONTAINER	611.0751	12" O.C.			
14	D	GRASSES	PENNISETUM ALOPECUROIDES' LITTLE BUNNY'	FOUNTAIN GRASS	#2 CONTAINER	611.0751	12" O.C.			

BIORETENTION PLANTING PLAN - OPTION I



	PLANT SCHEDULE - OPTION P									
Q1Y.	KEY	TYPE	BOTANICAL NAME	COMMON NAME	CONTAINER / SIZE	ITEM NUMBER	SPACING			
2	А	SHRUB	ITEA VIRGINICA "HENRY'S GARNET"	VIRGINIA SWEETSPIRE	#3 CONTAINER	611.0461	48" O.C.			
5	В	SHRUB	XANTHORHIZA SIMPLICISSIMA	YELLOWROOT	#1 CONTAINER	611.0442	36" O.C.			
18	C	PERENNIAL	MEEHANIA CORDATA	CREEPING MINT	FLATS	611.0751	12" O.C.			

CULVERTS

BIORETENTION PLANTING PLAN - OPTION P NOT TO SCALE

BRIDGES

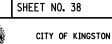
PIN 8780.49

RTE 32



ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

MISCELLANEOUS DETAILS



CONTRACT NUMBER

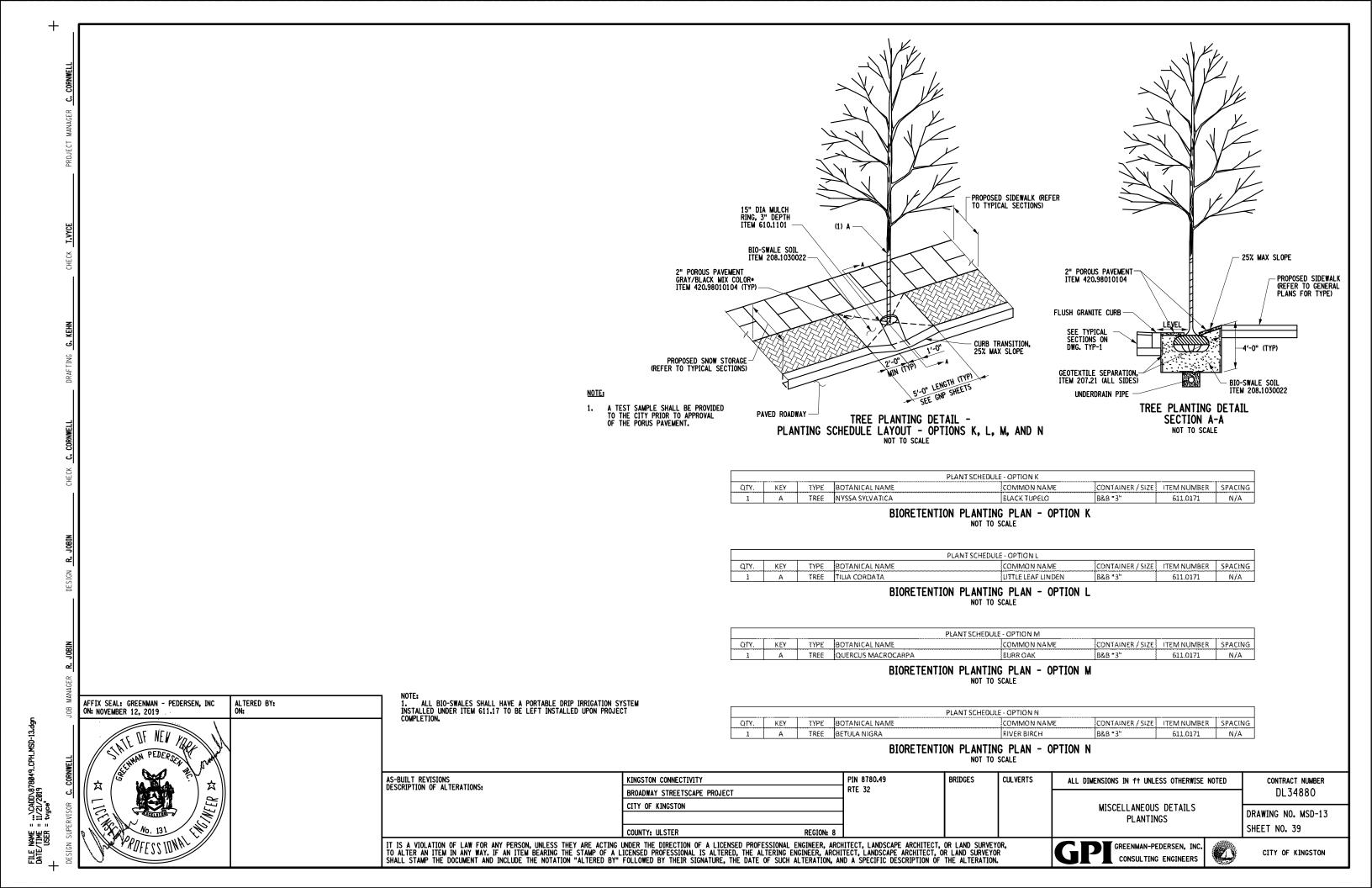
DL34880

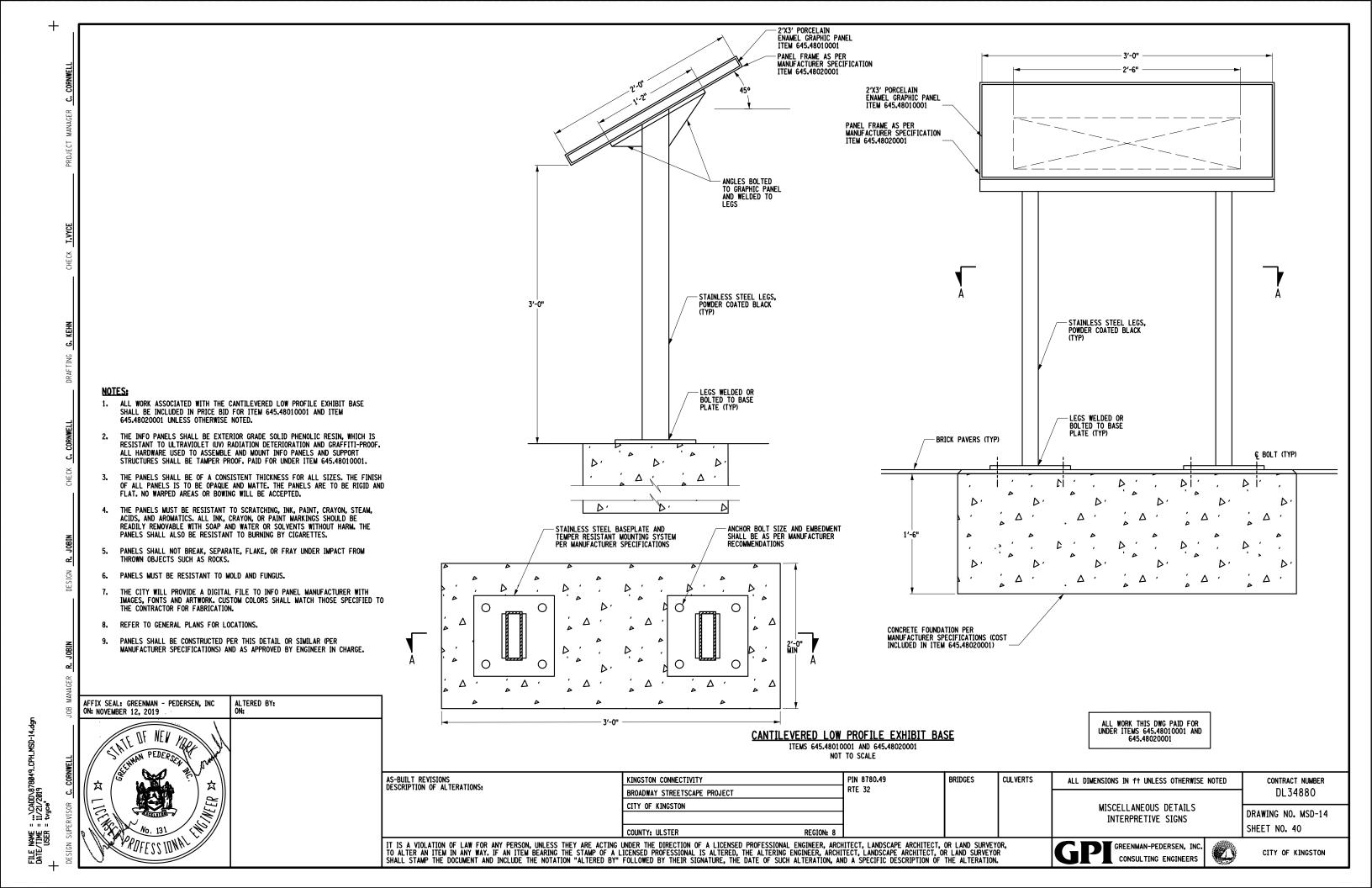
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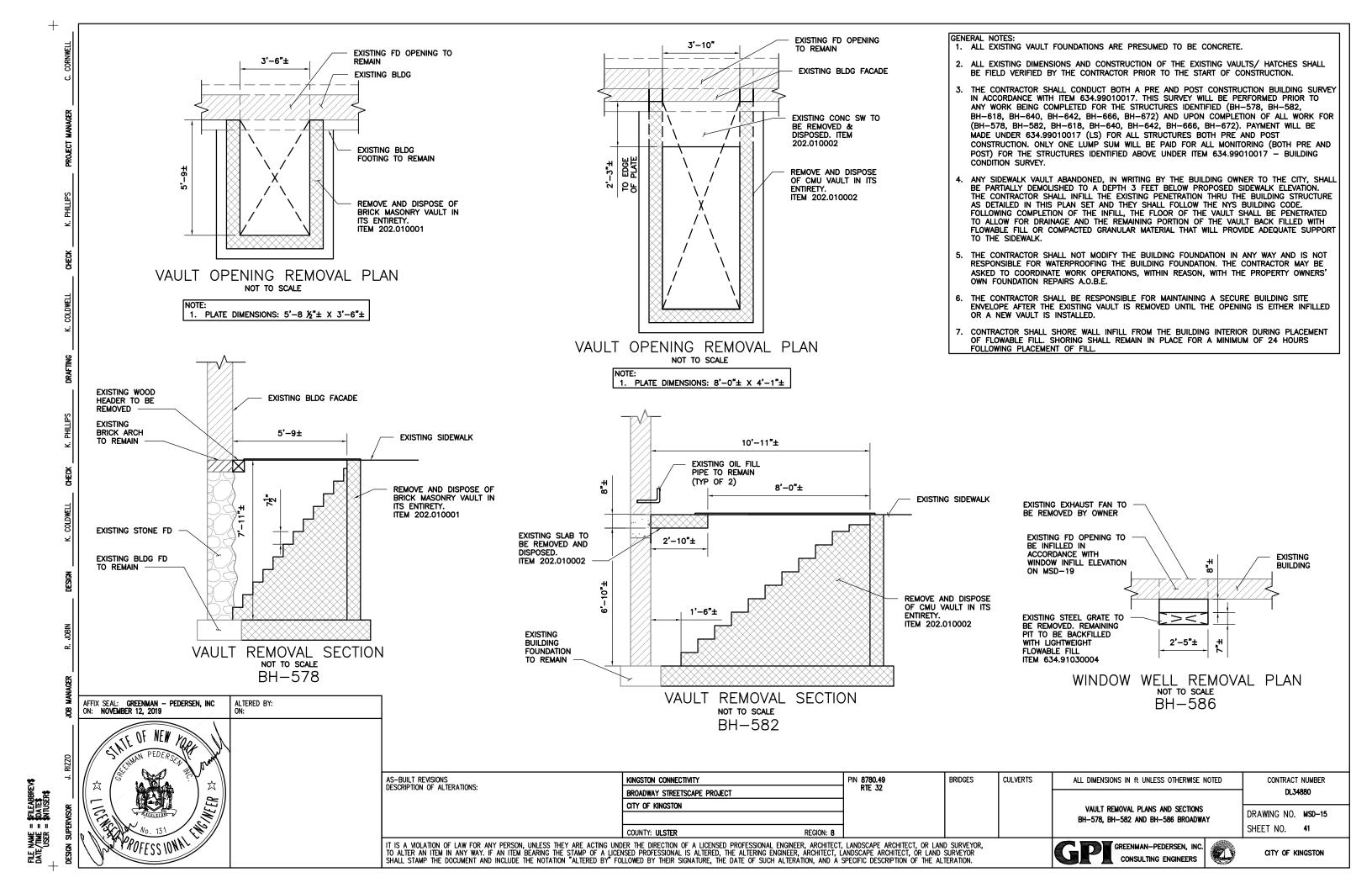
KINGSTON CONNECTIVITY

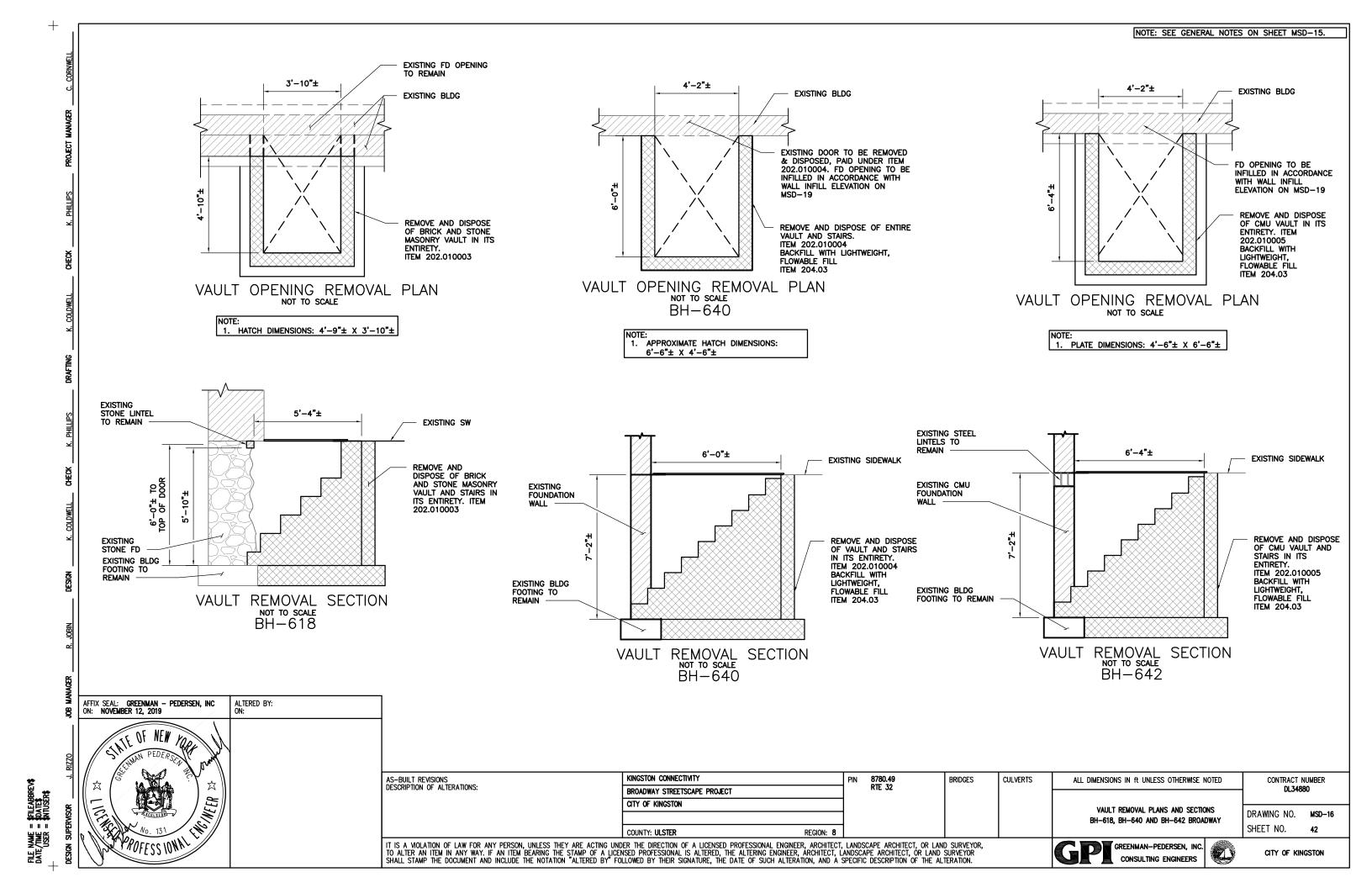
CITY OF KINGSTON

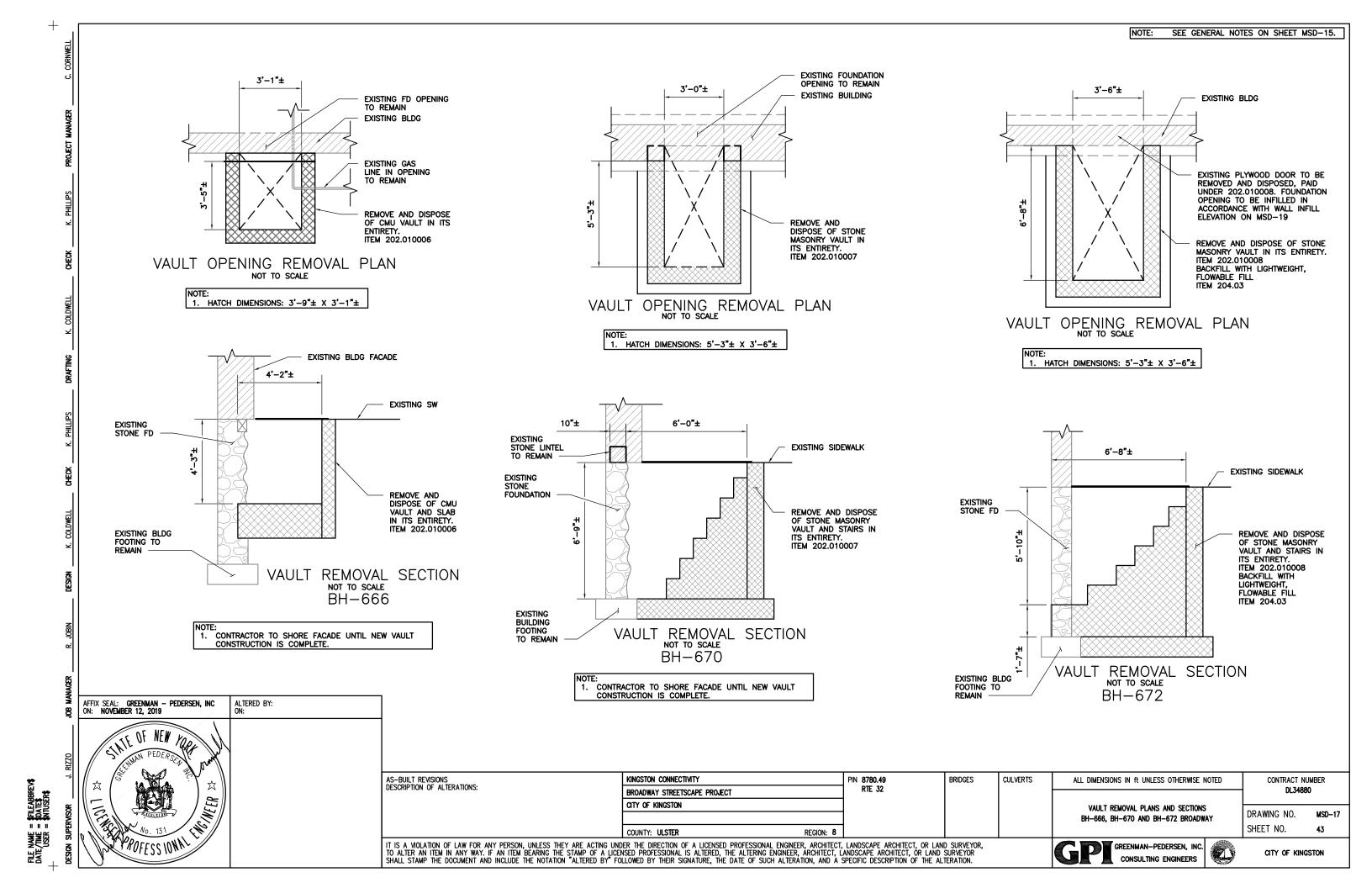
BROADWAY STREETSCAPE PROJECT

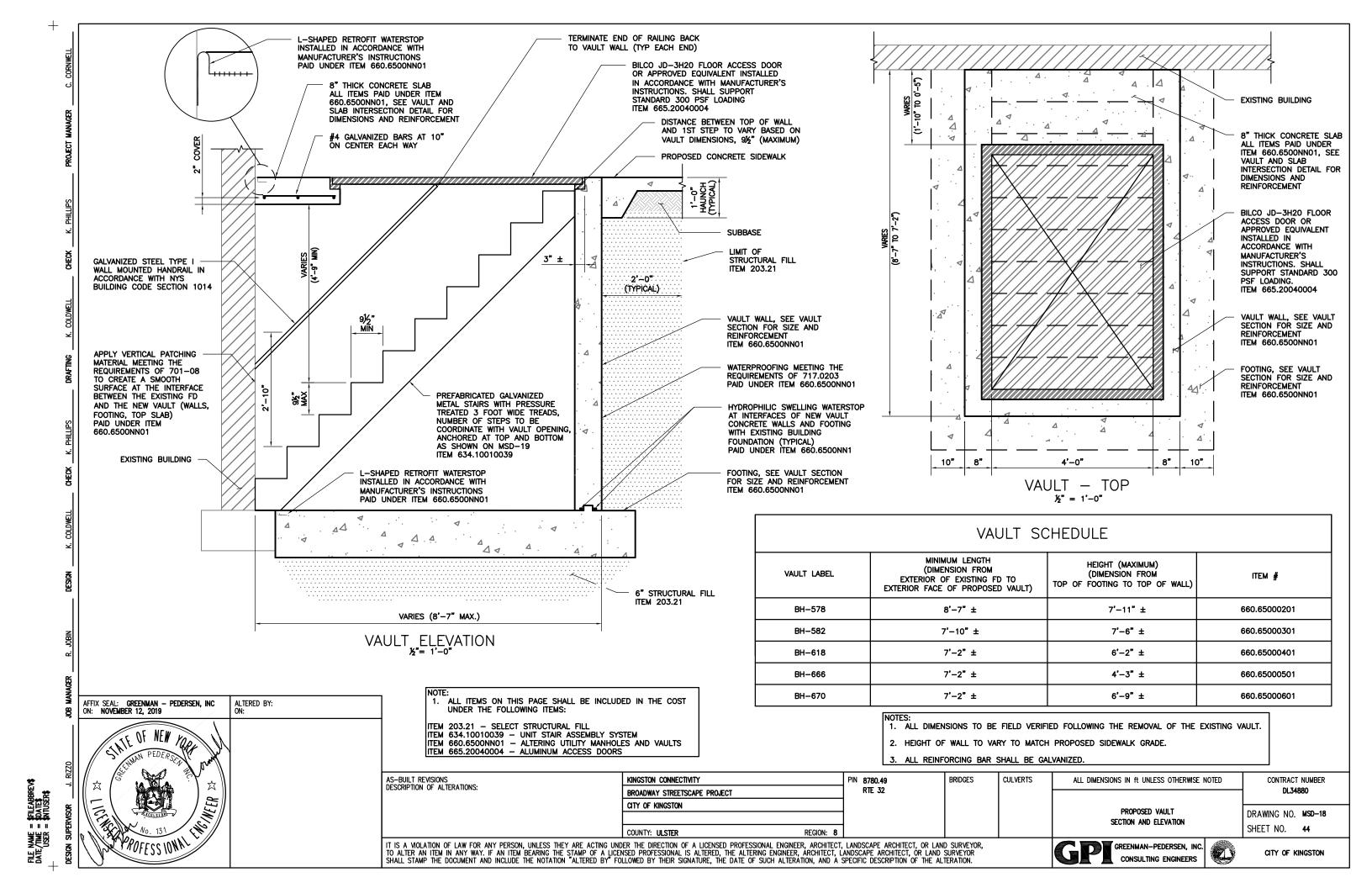


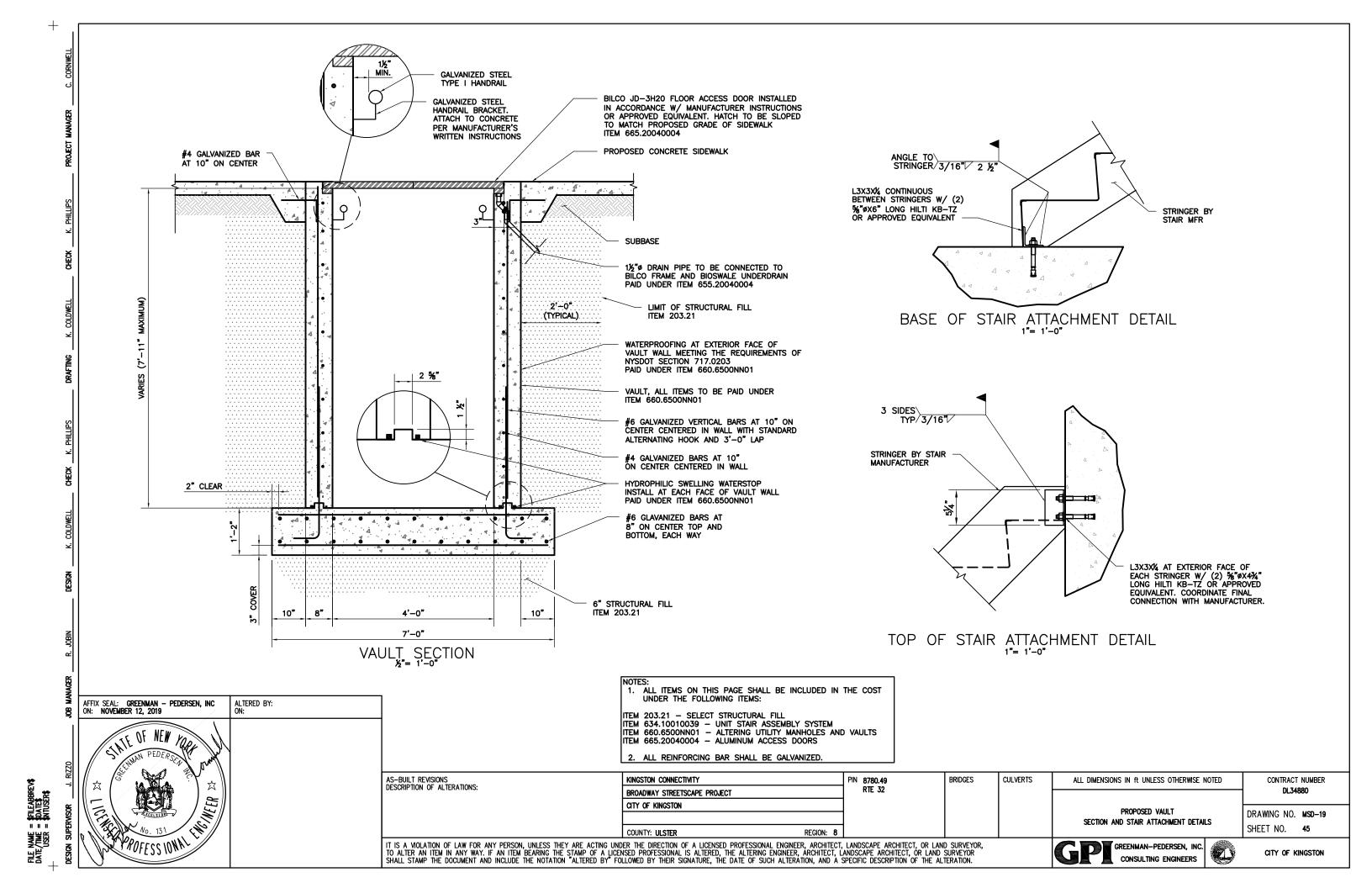


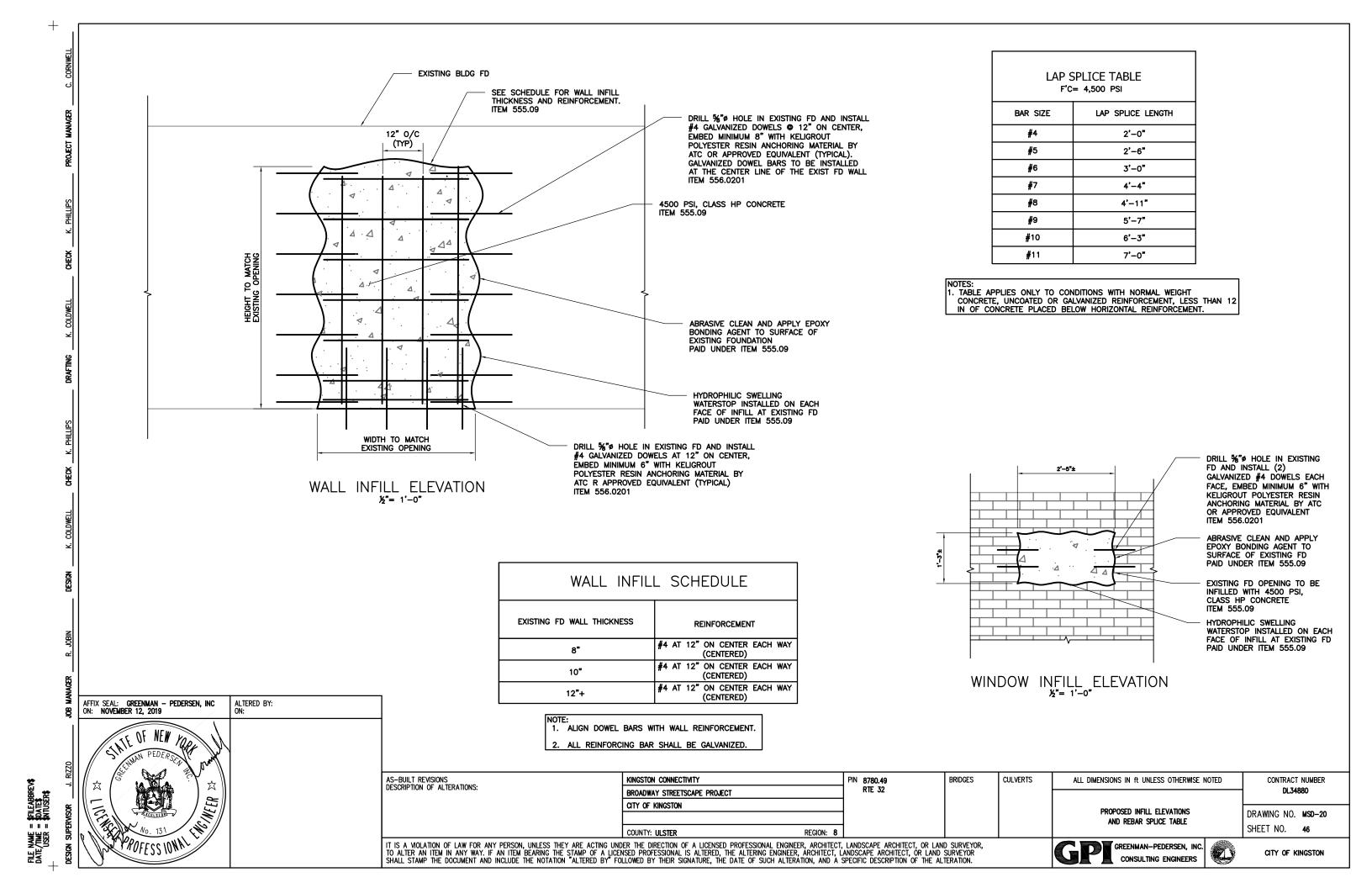




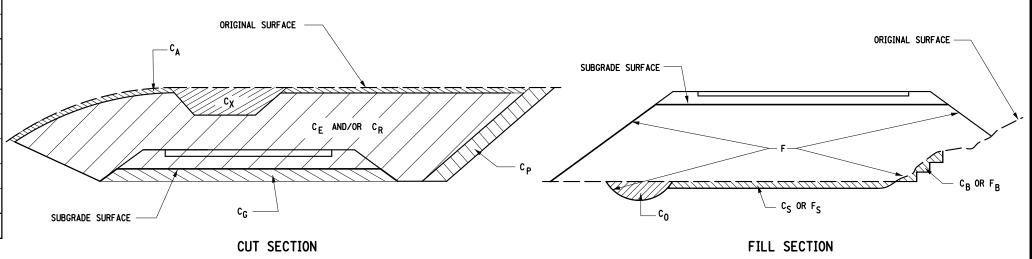








SUMMARY OF EARTHWORK ()TEMS 203.02 AND 203.03 ONLY)								
SOURCE -		EXCAVATION	1TEM 203.02	ITEM 203.03				
200/02	3 .	C _R	Tu	C _T	F _T			
ITEM 608.0101 - CONCRETE SIDEWALKS	0.0	0.0	1496.0	1496.0	0.0			
ITEM 608.130000039 - PERMEABLE PAVERS	0.0	0.0	1854.0	1854.0	0.0			
ITEM 608.40010008 - BLUESTONE ON STONE DUST	0.0	0.0	45.3	45.3	0.0			
ITEM 609.0201 - STONE CURB, TYPE A	0.0	0.0	2237.2	2237.2	0.0			
BIORETENTION AREAS	0.0	0.0	1072.0	1072.6	0.0			
HENRY STREET SLIP RAMP REMOVAL	0.0	0.0	41.0	41.0	0.0			
STAIRCASE REMOVAL	0.0	0.0	0.0	0.0	23.0			
TOTALS	0.0	0.0	6745.5	6745.5	23.0			



DEFINITIONS:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

- $\boldsymbol{c}_{\boldsymbol{B}}$ excavation for required benching, (both longitudinal and transverse).
- $\mathbf{C}_{\mathbf{G}}$ EXCAVATION FOR SUBGRADE IMPROVEMENT.
- CP EXCAVATION FROM CUT SLOPE NECESSARY TO PLACE SLOPE PROTECTION.
- c_{E} Portion of cut assumed to be earth suitable for embankment construction, excluding c_{G} and c_{P} .
- T_E $(C_B + C_G + C_P + C_E)$ Total Earth excavation assumed suitable for embankment construction.
- C_{A} EXCAVATION OF TOPSOIL (UNSUITABLE MATERIAL) IN CUT.
- ${\tt C}_{\sf S}$ excavation of topsoil (unsuitable material) under embankment.
- C_{X} excavation of unsuitable material in cut: swamp or dump
- ${\tt C_0}$ EXCAVATION OF UNSUITABLE MATERIAL BENEATH EMBANKMENT: SWAMP OR DUMP

KINGSTON CONNECTIVITY

- T_U $(C_A$ + C_S + C_X + C_0) Total excavation assumed unsuitable for embankment construction.
- \mathbf{c}_{R} Portion of cut assumed to be rock, including \mathbf{c}_{G} if applicable.
- $C_T (T_E + T_U + C_R)$ TOTAL EXCAVATION.

DEFINITIONS:

- ${\sf F}_{\sf B}$ FILL REQUIRED TO REPLACE BENCHES.
- $\boldsymbol{\mathsf{F}}_{\mathsf{S}}$ FILL REQUIRED TO REPLACE TOPSOIL REMOVED BENEATH EMBANKMENTS.
- F FILL REQUIRED TO COMPLETE EMBANKMENT TO SUBGRADE SURFACE AND SIDE-SLOPES AFTER FOUNDATION IS PREPARED.
- ${\sf F}_{\sf T}$ $({\sf F}_{\sf B}$ + ${\sf F}_{\sf S}$ + F) TOTAL FILL REQUIRED.
- $\rm T_A$ (T_E \times $\rm F_E$ + $\rm ^C_R\times F_R)$ THE VOLUME WHICH THE SUITABLE EXCAVATED MATERIAL COULD OCCUPY IN EMBANKMENT.
- ${\sf F_E}$ Shrinkage factor for Earth
- $\mathbf{F}_{\mathbf{R}}$ SWELL FACTOR FOR ROCK

CULVERTS

THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THESE TABLES ARE ESTIMATED, AND ARE PROVIDED FOR THE PURPOSE OF PREPARING AN ESTIMATE. THEY ARE NOT TO BE CONSTRUED AS BEING EXACT. THEY ARE INTENDED TO QUANTIFY AND QUALIFY THE NATURE OF THE WORK TO BE PERFORMED. SIGNIFICANT DIFFERENCE FROM THIS REPRESENTATION, WHEN ENCOUNTERED DURING THE ACTUAL WORK, WILL BE HANDLED ACCORDING TO THE SPECIFICATIONS GOVERNING THIS PROJECT.

203.02 UNCLASSIFIED EXCAVATION AND DISPOSAL

203.03 EMBANKMENT IN PLACE

DECODITION OF ALTERATIONS.	NAMES TO A SOURCE STATE	4 ' *** * · · · · · · · · · · · · · · · ·	51125020	1 002 12.1110	MEE DIMENSIONS IN 11 ONEESS OTHERWISE NO
DESCRIPTION OF ALTERATIONS:	BROADWAY STREETSCAPE PROJECT	RTE 32			
	CITY OF KINGSTON				
					EARTHWORK SUMMARY SHEETS
	COUNTY: ULSTER REGION: 8				
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A L SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	ICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARC	HITECT, LANDSCAPE ARCHITECT, (R LAND SURVEYO	R	GPI GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

PIN 8780.49

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

SHEET NO. 47

CONTRACT NUMBER

DL34880

DRAWING NO. ESS-1

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

CITY OF KINGSTON

CITY OF KINGSTON			
COUNTY: ULSTER	REGION: 8		
UNDER THE DIRECTION OF A LICENSED PROF		LANDSCAPE ARCHITECT, C	

ROCK

 \mathbf{C}_{R}

0.0

FOR DEFINITIONS AND NOTES SEE DWG. ESS-1

CA

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6745.5

1496.0

1854.0

45.3

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UNSUITABLE EXCAVATION

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6745.5

1496.0

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6745.5

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

SUITABLE EXCAVTION

C₽

0.0

 $\mathbf{C}_{\mathbf{E}}$

0.0

T_E

0.0

CB

0.0

 $\mathbf{C}_{\mathbf{G}}$

0.0

LOCATION

(STATION TO STATION)

8 5+30 TO B 37+76

ITEM 608.0101 - CONCRETE SIDEWALKS

ITEM 609.0201 - STONE CURB, TYPE A

HENRY STREET SLIP RAMP REMOVAL

BIORETENTION AREAS

STAIRCASE REMOVAL

ITEM 608.130000039 - PERMEABLE PAVERS

ITEM 608.40010008 - BLUESTONE ON STONE DUST

TOTALS

SUBDIVISION NO.

1A

3B

1C

1D

KINGSTON CONNECTIVITY	PIN 8780.49
BROADWAY STREETSCAPE PROJECT	RTE 32
CITY OF KINGSTON	
COUNTY: ULSTER REGION: 8	

BRIDGES

CULVERTS

TOTAL

EXCAVATION

C_T

6745.5

1496.0

1854.0

45.3

2237.2

1072.0

41.0

6745.5

F_B

0.0

0.0

EMBANKMENT

23.0

23.0

Fī

23.0

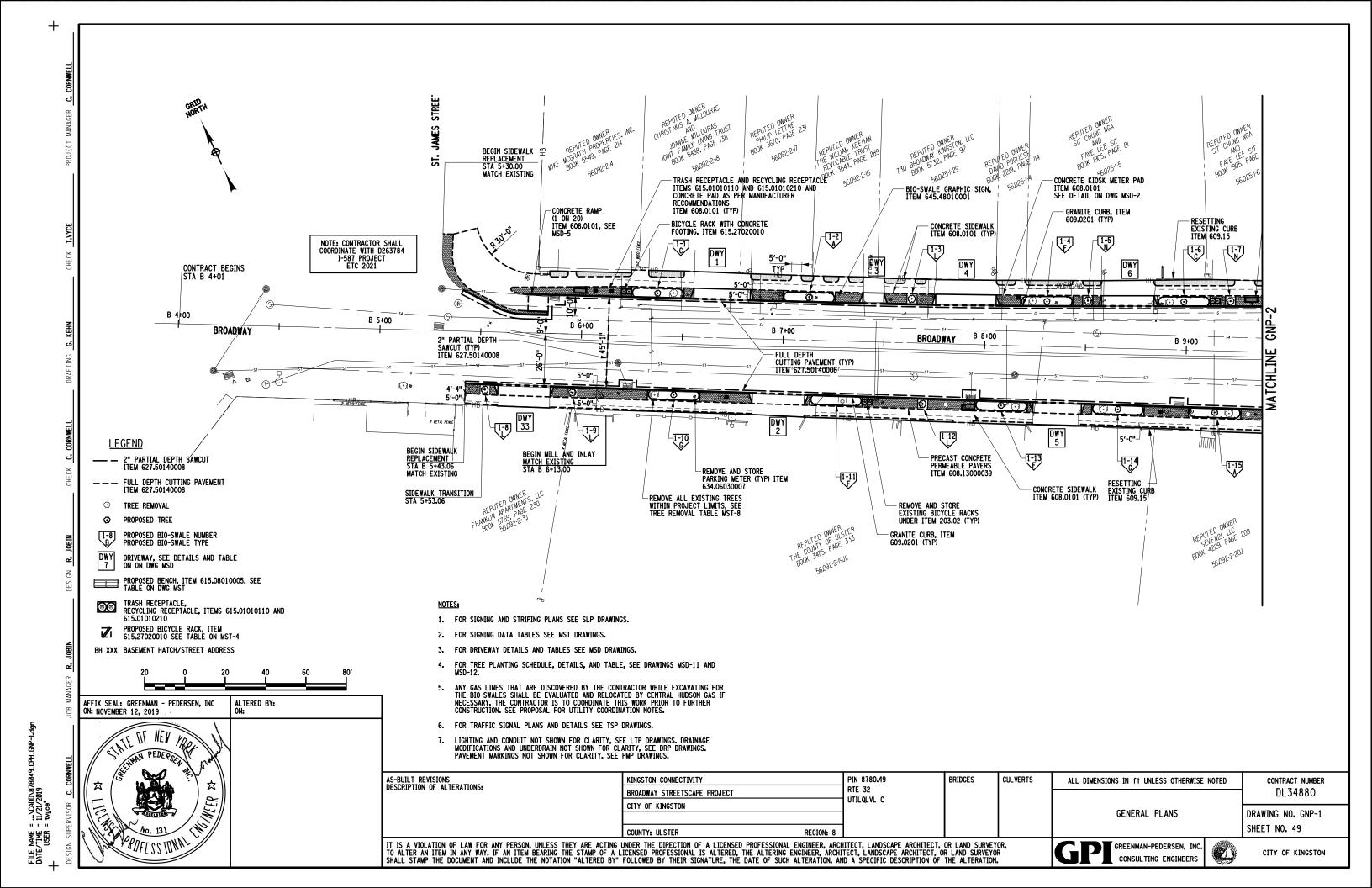
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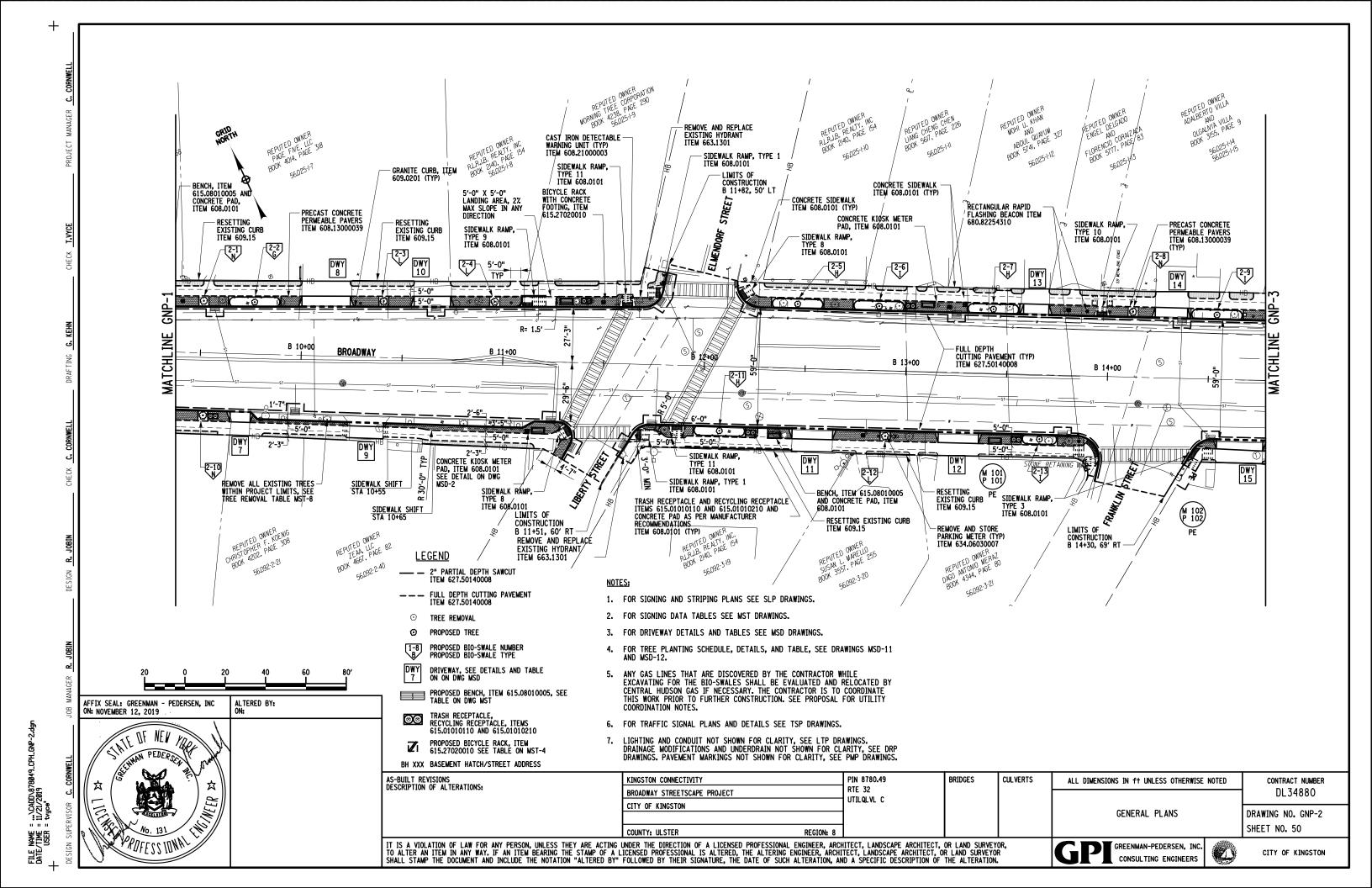
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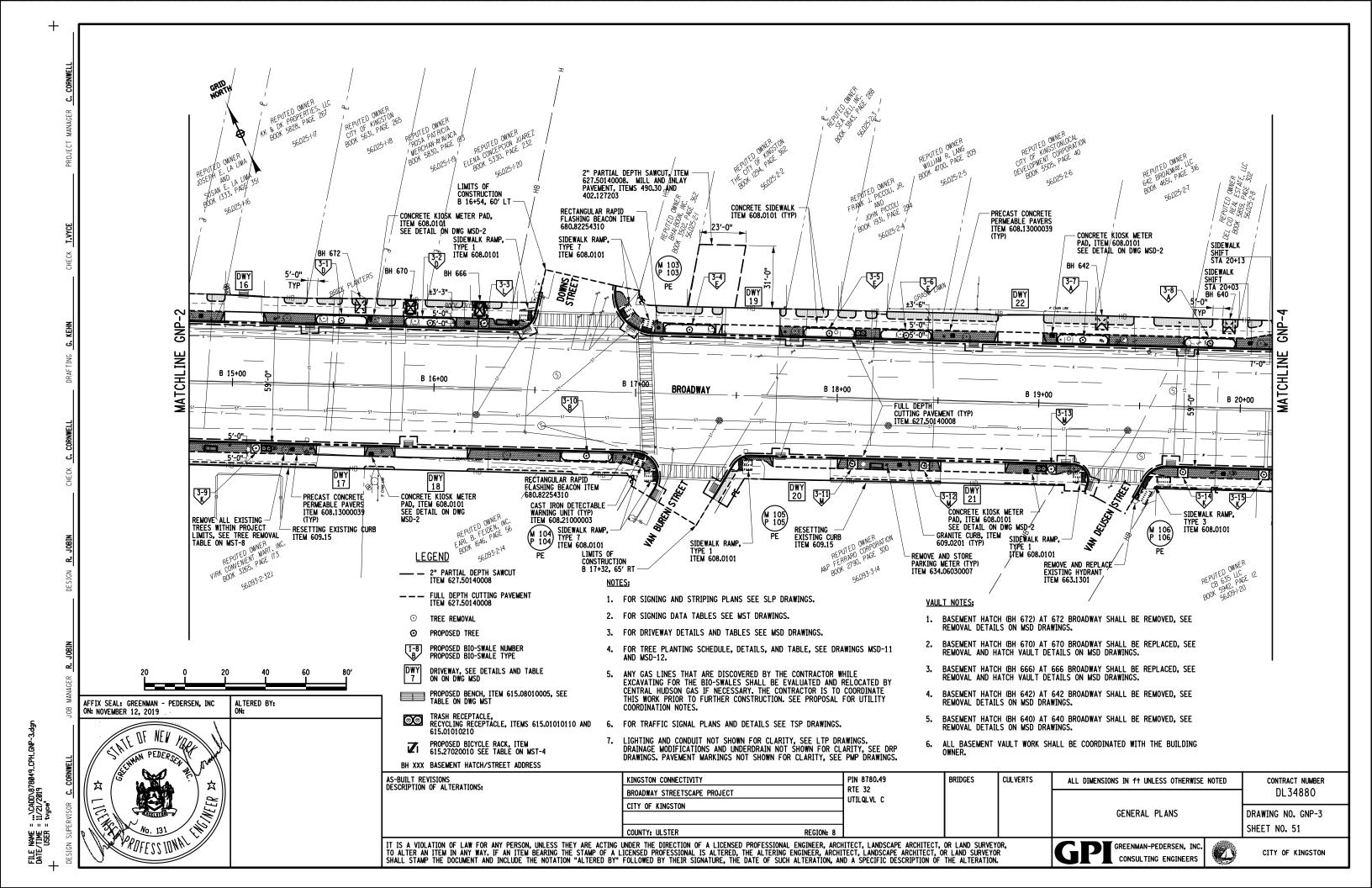
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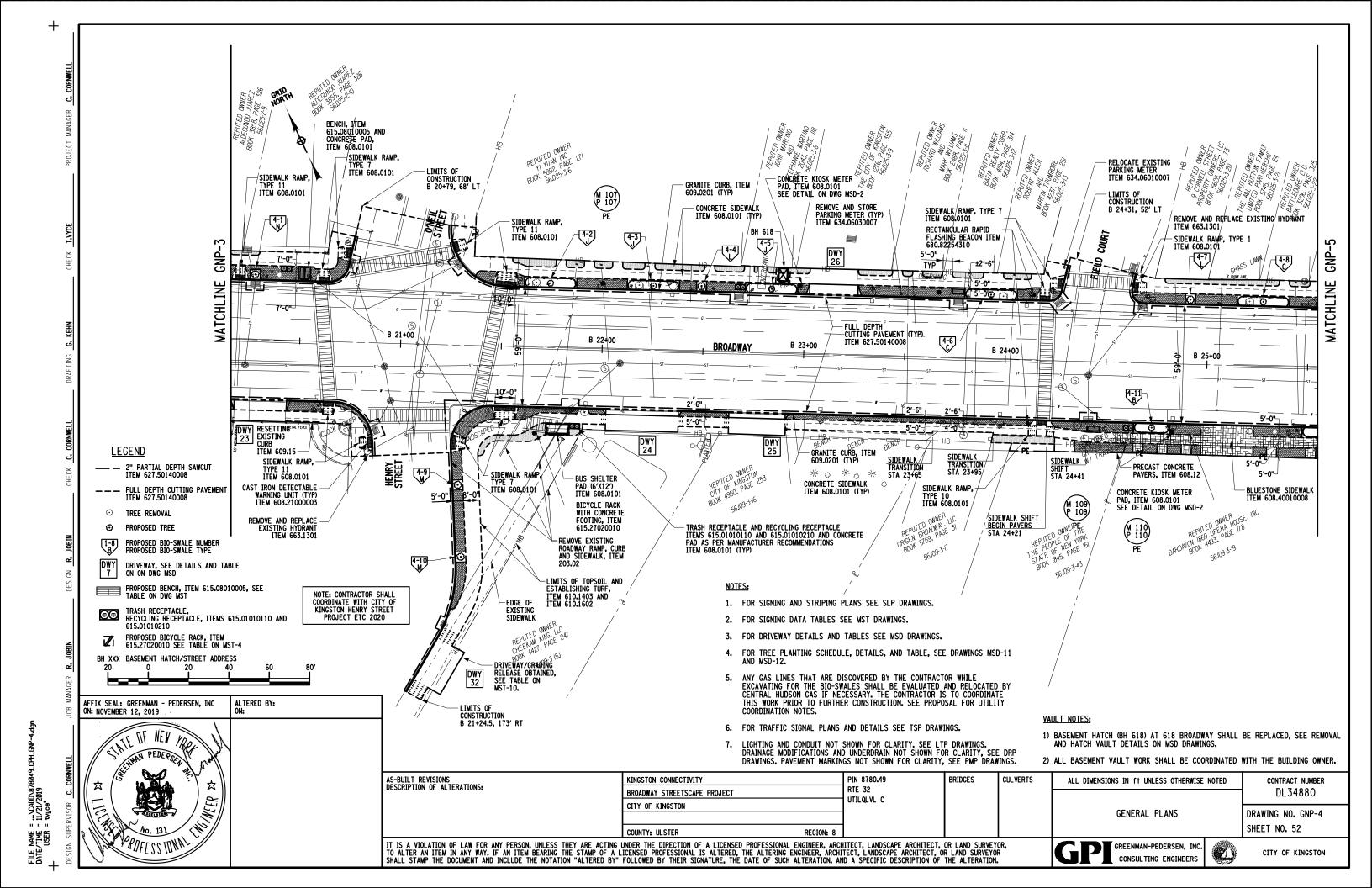
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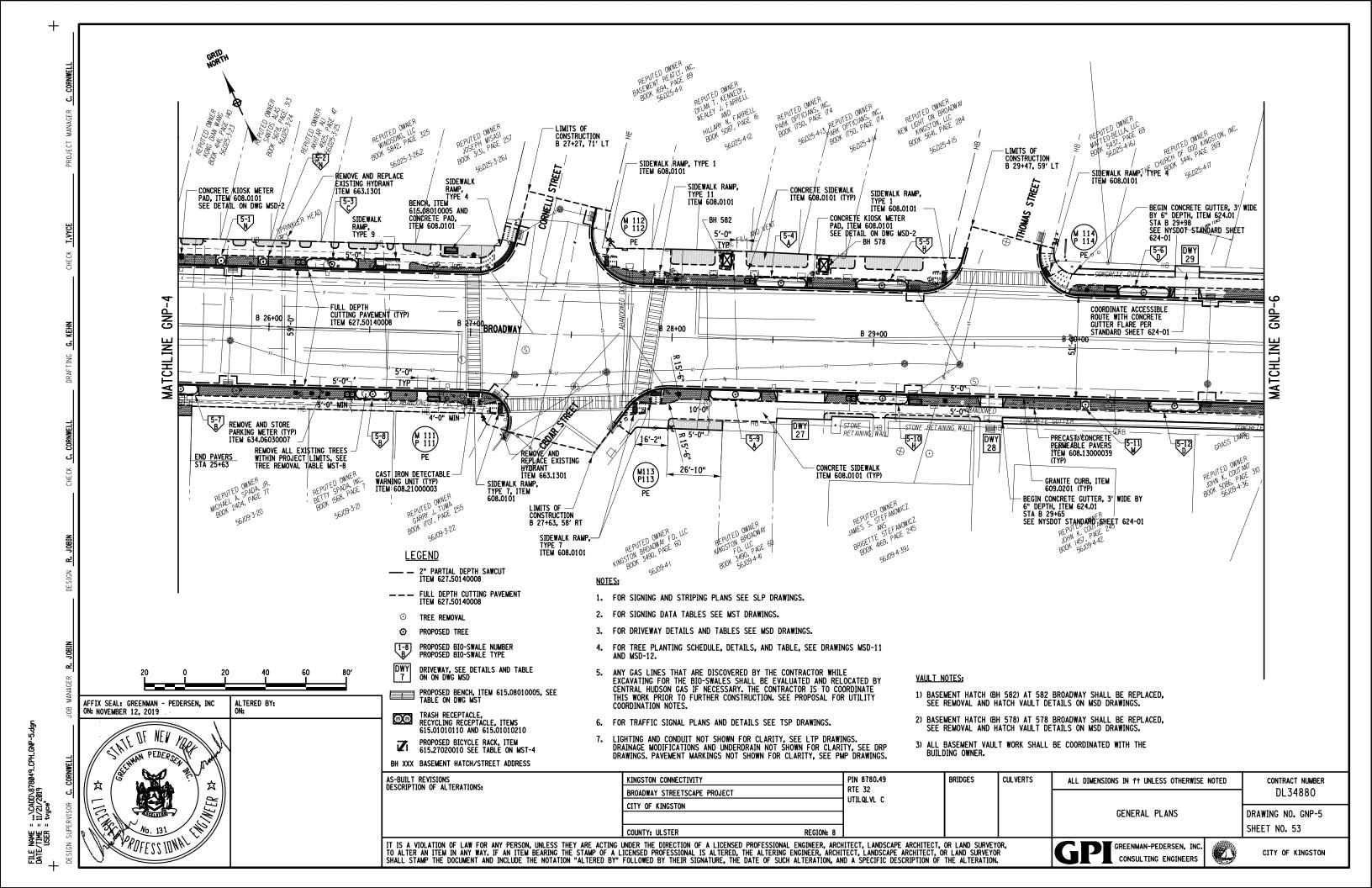
EARTHWORK SUMMARY SHEETS SHEET NO. 48

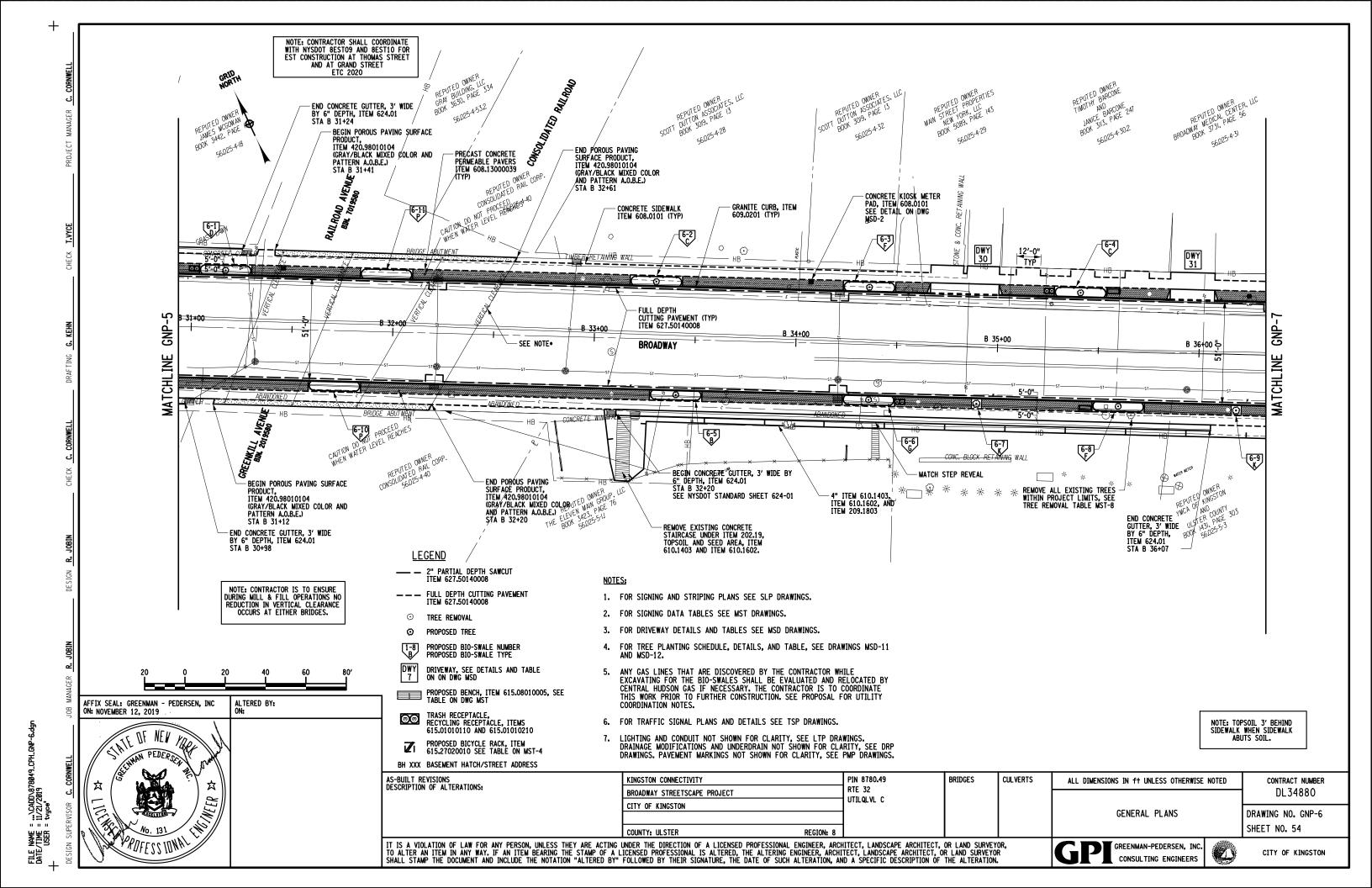


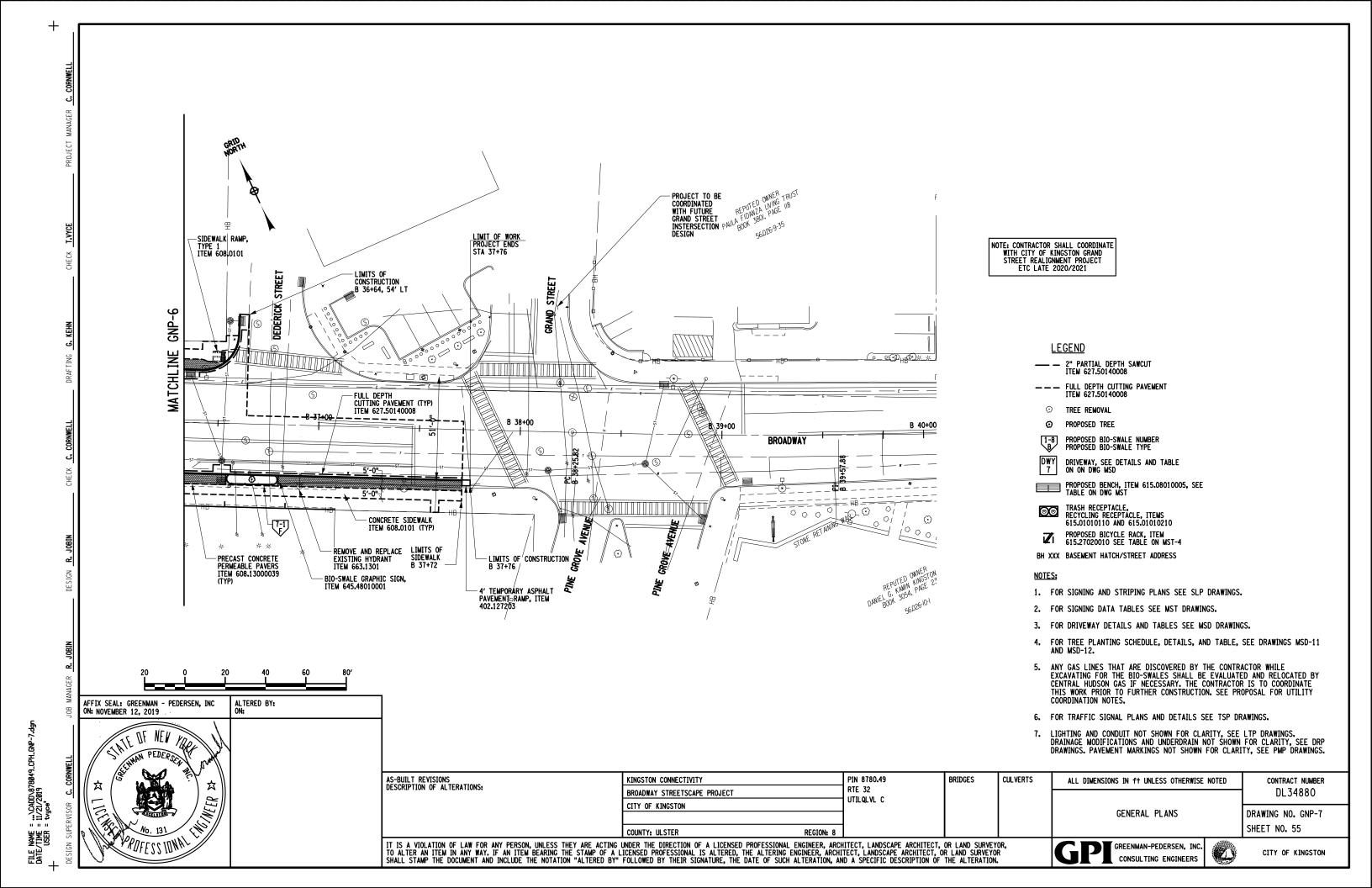


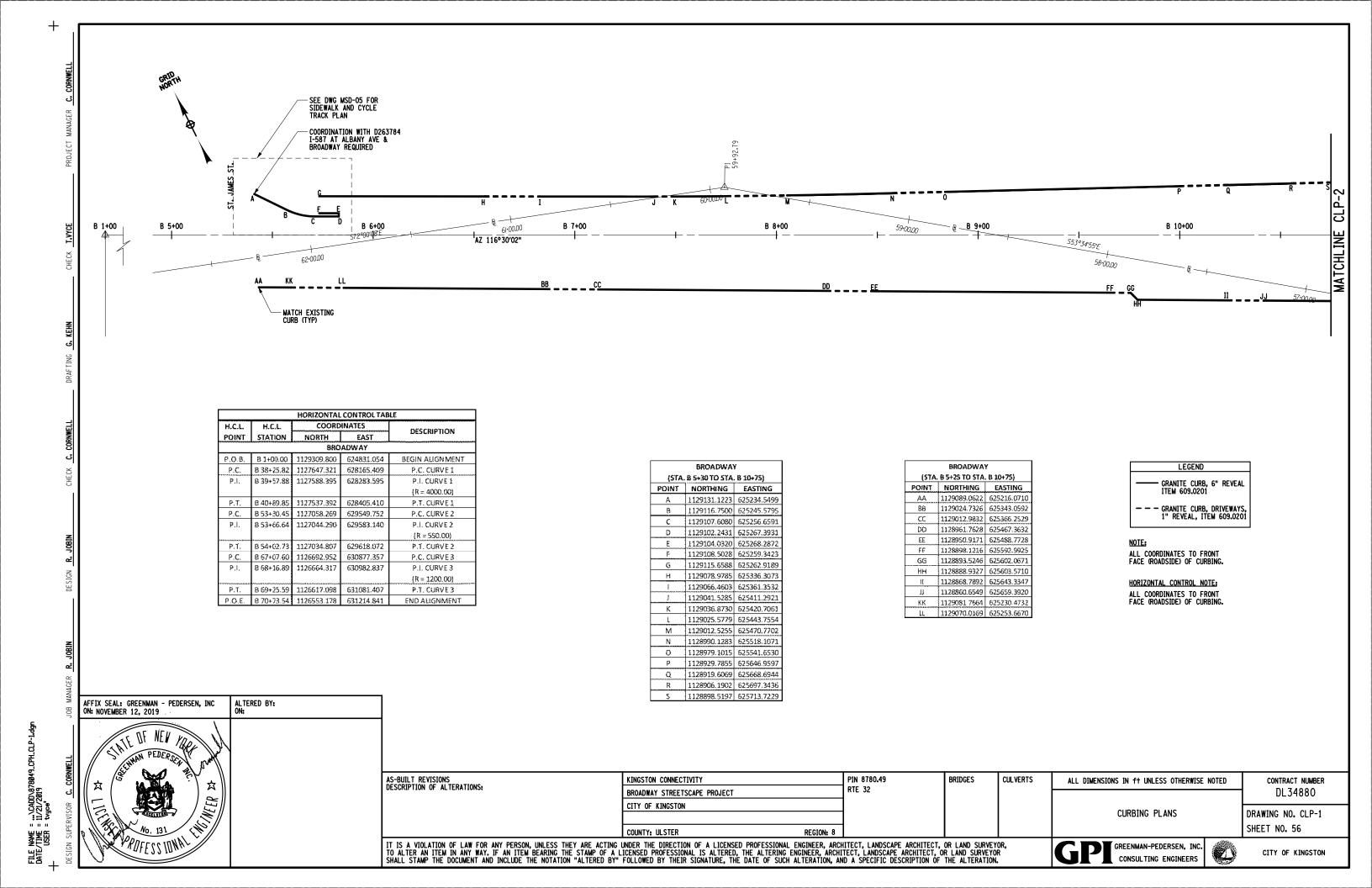


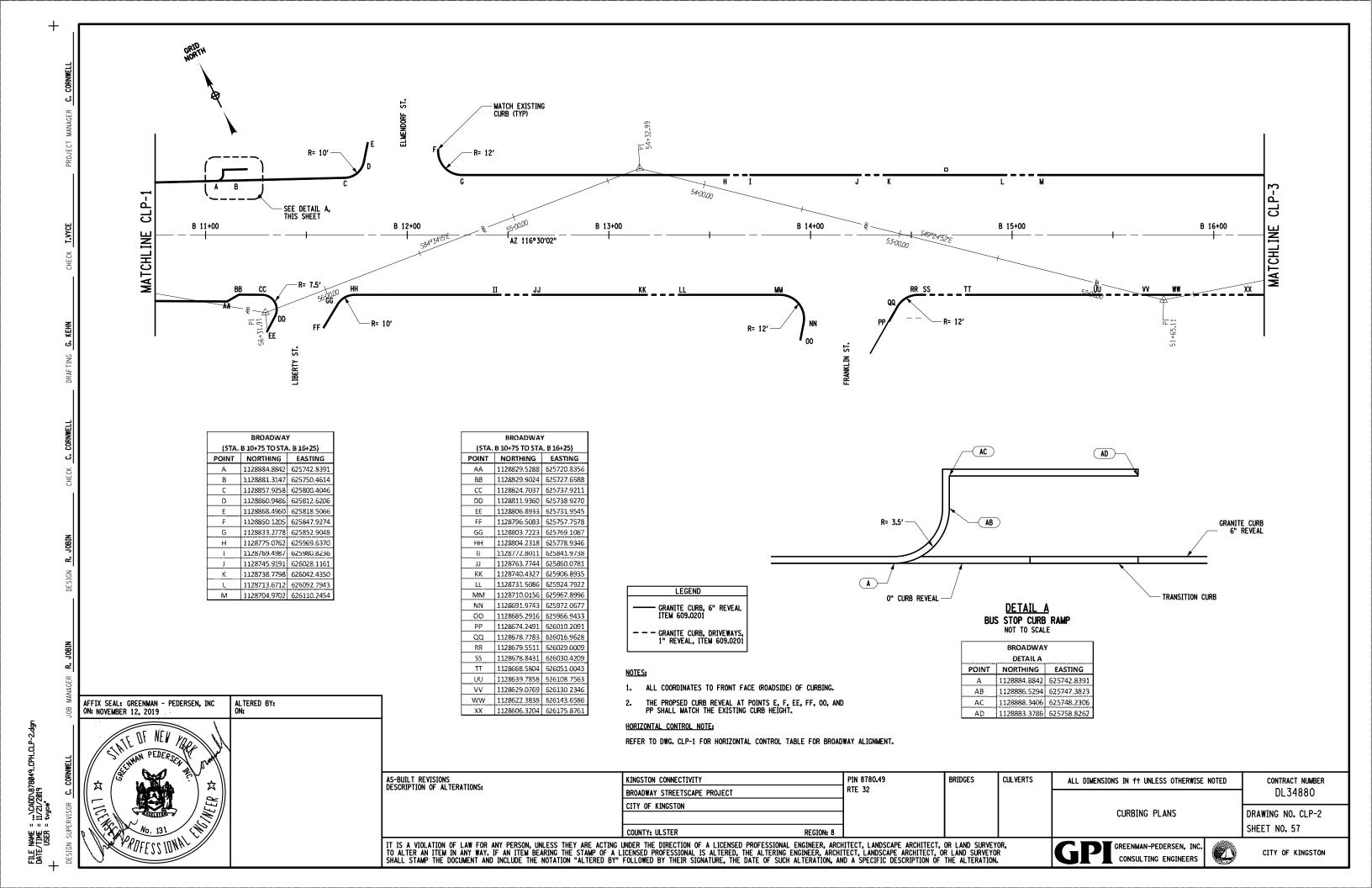


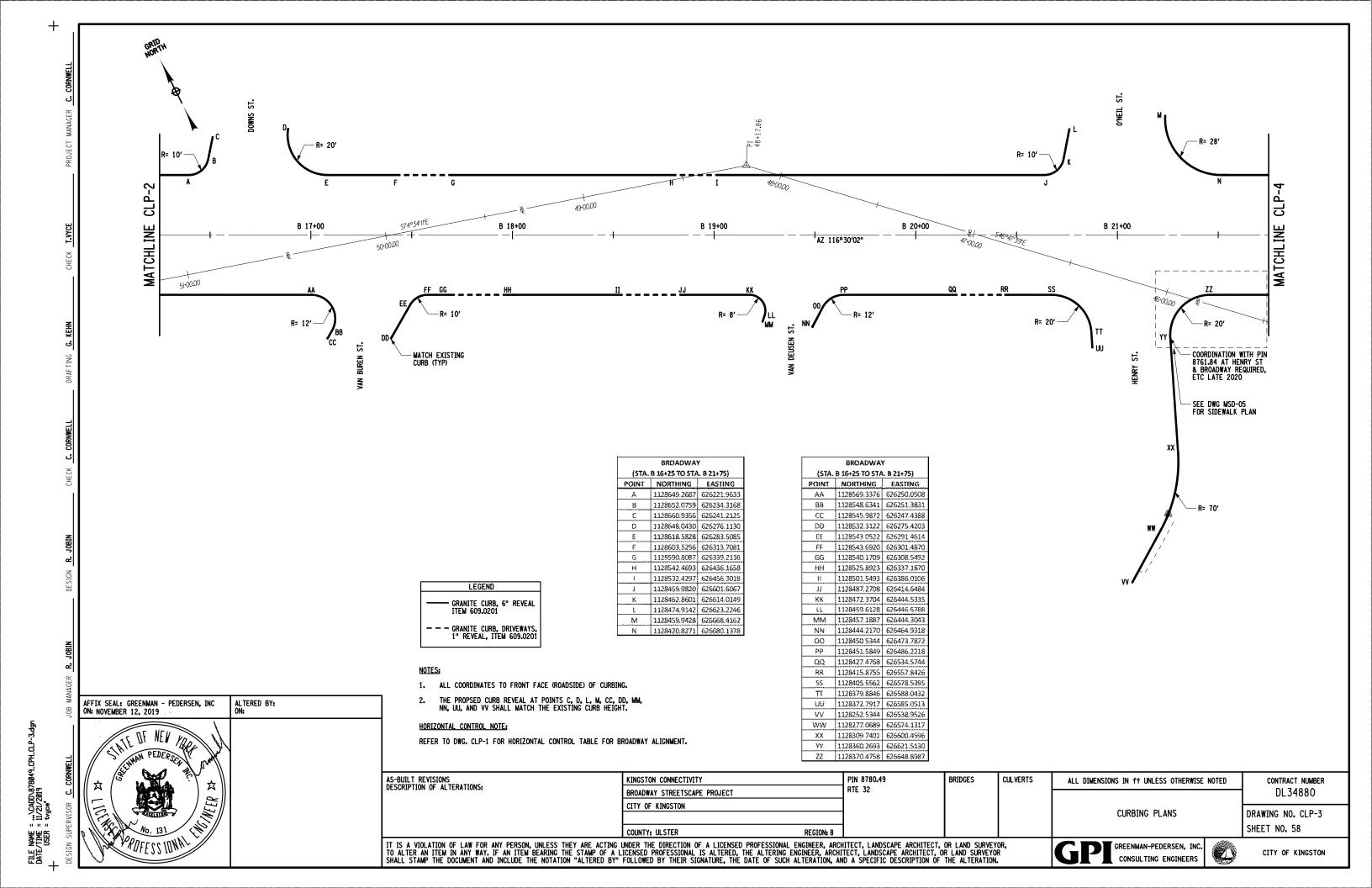


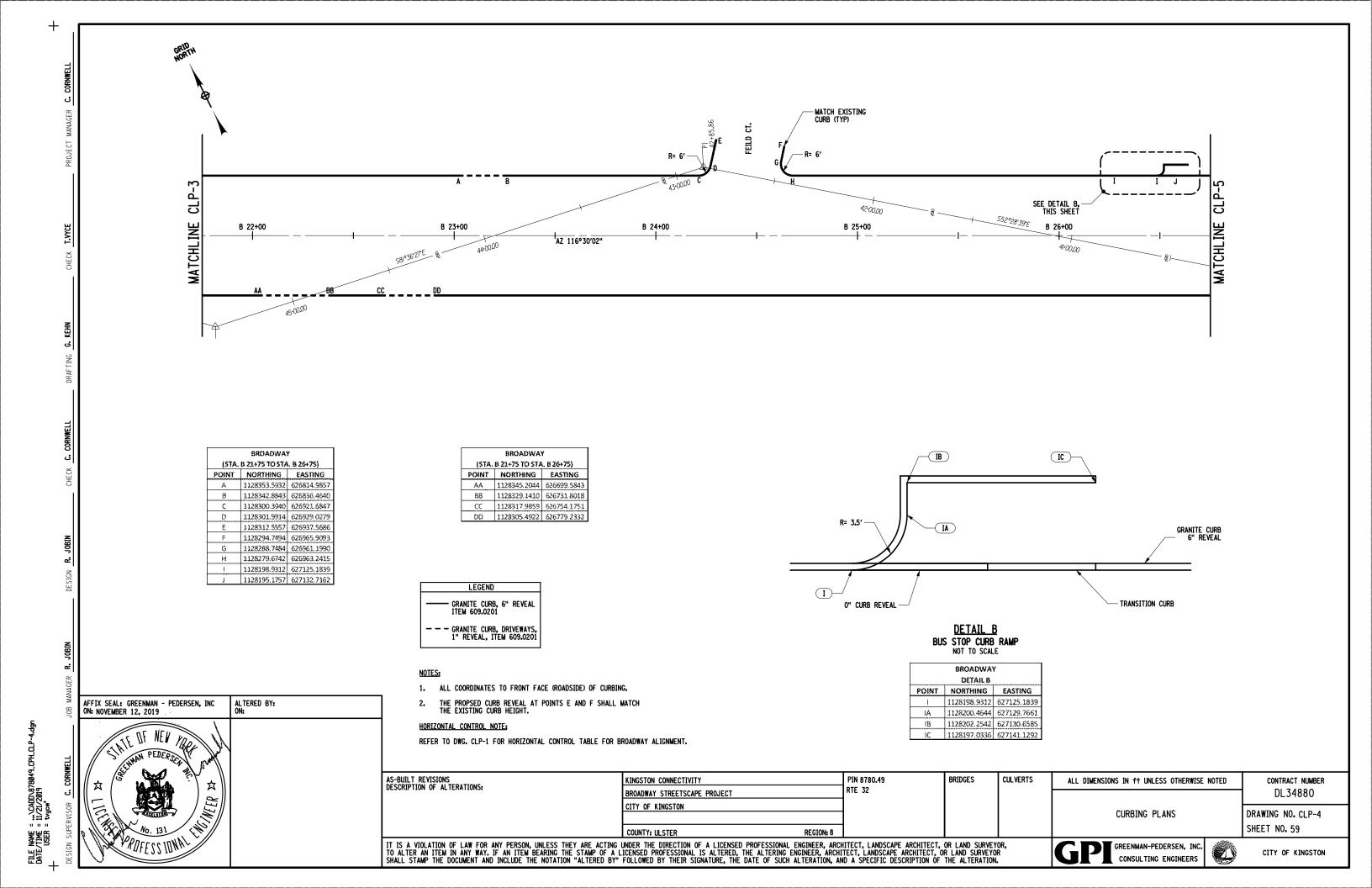


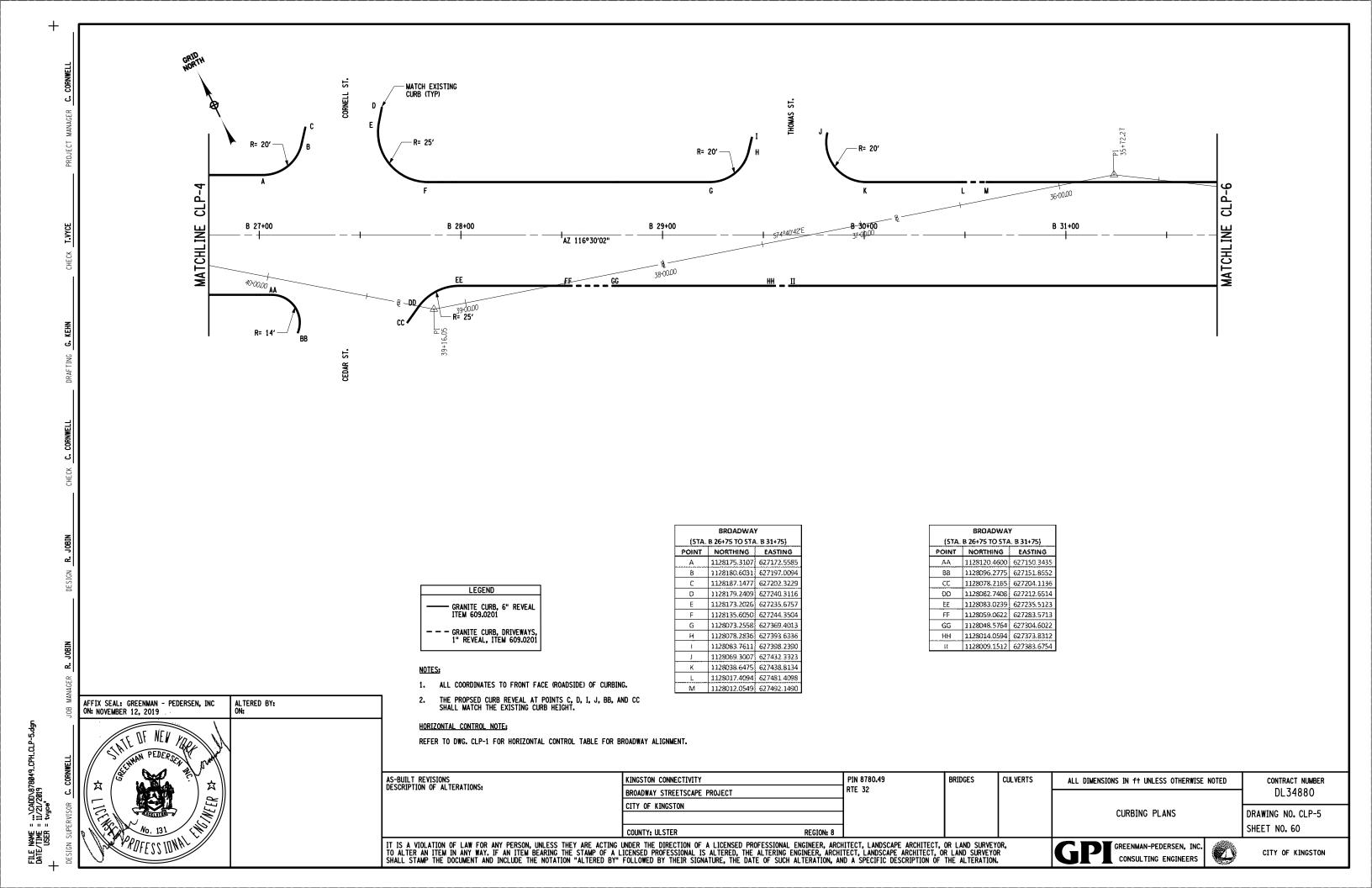


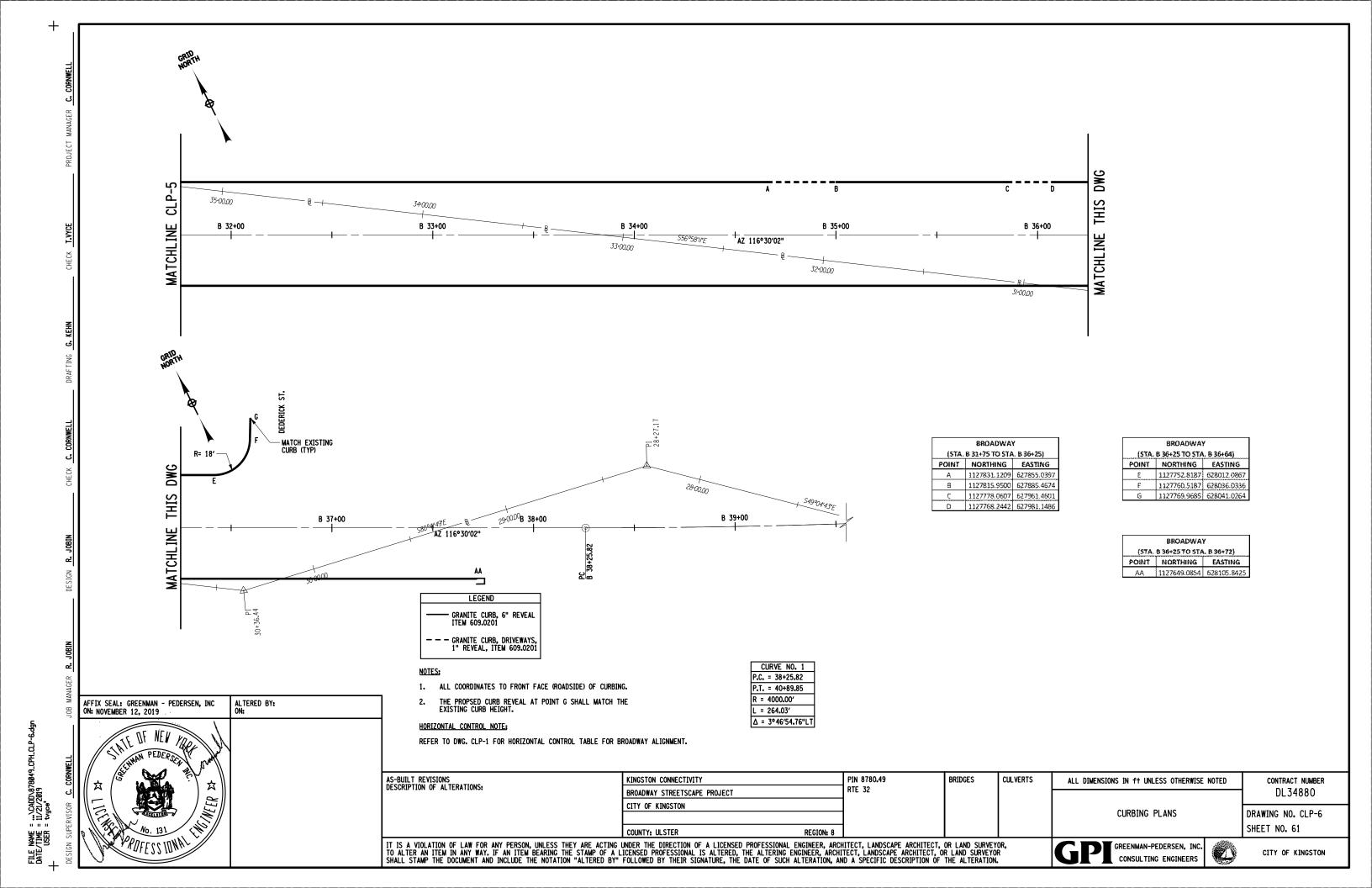












PESSON SUPERVISOR C. CORNWELL MANAGER R. JOBIN DESIGN RANGE R. JOBIN DESIGN RANGER R. JOBIN DESIGN RANGE R. JOBIN DESIGN RANGER R. JOBIN DESIGN RANGE R. J

SIGN DATA TABLE SIGN DATA TABLE PAYMENT AREA PAYMENT AREA DESIGNATION DESIGNATION LOCATION TEXT **TEM** LOCATION ITEM TEXT & COLOR & COLOR AREA AREA TOTAL PAYMENT TOTAL PAYMENT SEE NOTE 2) (SEE NOTE 2) AREA 1-2, 1-4, 1-5, 2-4, 2-8, 3-2, PARKING 24" x 24" 1.5 SF NYP2-3R 3-5, 3-6, 3-11, 3-15, 4-1, D9-11B 5-15, 5-16 645.5101 PAY AT 645.5101 (MOD) 4-2, 4-7, 4-8, 5-4, 5-11, WHITE-GREEN STATION 4.0 SF 8.0 SP 5-12, 6-1, 6-6, 6-7, 6-8 1.5 SF 31.5 SF 1-9, 2-1, 2-10, 2-11, 2-12, 21" x 15" 12" x 8" 0.7 \$9 2-14, 2-15, 3-9, 3-10, M6-1L 645.5101 3-14, 3-17, 3-18, 4-5, 5-16 BLUE-WHITE 4-10, 4-11, 5-2, 5-3, 5-6, NYP3-2 2.2 SF 6-3, 6-4, 6-5, 6-10, 6-12, 9AM-6PM (MOD) 645.5101 6-13, 1-2, 1-4, 1-5, 2-4, 2-MON-SAT WHITE-GREEN 8, 3-2, 3-5, 3-6, 3-11, 3- 45 31.5 SF 2.2 SF 21" x 15' 15, 4-1, 4-2, 4-7, 4-8, 5-4, 5-11, 5-15 645.5202 BLUE-WHITE 5-12, 6-1, 6-6, 6-7, 6-8 2.2 SF 30" x 30" 6.3 SF NO PARKING R1-1 2-9, 3-12, 4-6, 7-1 645.5202 NYP1-2D 1-10, 1-11, 3-20, 4-15, ANY 645.5101 25.2 SF WHITE-RED 5-8, 6-15, 6-16, 6-17 1.5 SF 12.05F 4.0 SF (I) PARKING R3-2 645.5202 5-9 NYP1-2L ANY 1-1, 1-12 645.5101 4.0 SF 4.0 SF WHITE-RED TIME 1.5 SF 3.0 SF 3.0 SF NO PARKING 12" x 18 1.5 SF 2-21 R6-1L 645.5102 ONE WAY NYP1-2R ANY 2-16 645.5101 3.0 SF 3.0 SF WHITE-RED TIME 1.5 SF 36" x 12" 3.0 SP NO PARKING 32" x 18" ONE WAY R6-1R 2-21 645.5102 NYP1-9L HERE TO 7-2 645.5101 3.0 SF WHITE-RED CORNER 1.5 SF 1.5 SE RESERVE 1.5 SF PARKING PARKING R7-8L WHITE-4-8, 4-10 645.5101 F HERE TO GREEN/BLUE 645.5101 5-10 3.0 \$8 CORNER 1.5 SF 1.5 SF RESERVED 1.5 \$8 1-9, 2-1, 2-10, 2-11, 2-12, **PARKING** METER 87-8N 2-14, 2-15, 3-9, 3-10, 12" x 18 1.5 SF PARKING WHITE-3-24, 3-25 645.5101 Ġ 3-14, 3-17, 3-18, 4-5, 645.5101 GREEN/BLUE PAY AT 3.0 SF 4-10, 4-11, 5-2, 5-3, 5-6, STATION 6-3, 6-4, 6-5, 6-10, 6-12, 1.5 SF 36.0 SF 6.13 24" x 18" 3.0 SF R7-20 1-7, 2-5, 2-13, 3-3, 3-8, CUSTOM 3-13, 3-16, 4-3, 4-9, 5-1, 645.5102 WHITE-GREEN 5-5, 6-2, 6-9 3.0 \$F 39.0 \$F AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 ALTERED BY: ON: 12" x 18 1.5 SF R9-23aL LEFT TURN 2-18, 4-14, 5-14 645.5101 WHITE-BLACK MUST USE SELWAN PEDERSEN 1.5 SF 4.5 SF TURN BOX AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 8780.49 KINGSTON CONNECTIVITY RTE 32 BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON

COUNTY: ULSTER

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

SIGN DATA TABLE								
DESIGNATION	LOCATION		ТЕХТ	fTEM	SIZE	PAYMENT AREA (SEE NOTE 2)		
& COLOR					AREA	TOTAL PAYMENT		
		_			(SEE NOTE 2)	AREA		
R9-23aR	. 2-17, 3-19, 5-13 _F		RIGHT TURN	645.5101	12" × 18"	1.5 SF		
WHITE-BLACK		3	MUST USE TURN BOX		1.5 SF	4.5 SF		
R10-11	2-19 r		NO TURN	645.5102	24" × 30"	5.0 SF		
WHITE-BLACK/RED		1	ON RED		5.0 SF	5.0 SF		
R10-15 (MOD)	1-3, 1-6, 1-8, 2-2, 2-3, 2-6, 2-7, 3-1, 3-4, 3-7, 4-4, 5-7,		TURNING VEHICLES	645.5102	30" x 30"	6.3 SF		
WHITE-BLACK/RED	6-11.6-14	14	▼ 10 %	301,110%	6.3 SF	88.2 SF		
W11-2 HI-VIZ	2-20, 3-21, 3-22, 3-23, 4-			645.5202	30" x 30"	6.3 SF		
YELLOW-BLACK	12, 4-13				6.3 SF	37.85F		
W16-9P HI-VIZ	2-20, 3-21, 3-22, 3-23, 4-		AHEAD	645.5202	24" x 12"	2.0 SF		
YELLOW-BLACK	12, 4-13	6	ARLAD	043.3202	2.0 SF	12.05F		
CUSTOM	1-13		CYCLE TRACK	645.5101	12" x 18"	1.5 SF		
COSTOR	4-4-7	1	Ø	043.3101	1.5 SF	1.5 SF		
CUSTOM	1-13 s		A	645 5101	12" x 18"	1.5 SF		
COSTOIVI	1-13	1	SLOW X	645.5101	1.5 SF	1.5 SF		

NOTES:

BRIDGES

CULVERTS

- 1. SIGN LOCATIONS AS SHOWN ON PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL INSTALL NEW SIGNS AND RELOCATED EXISTING SIGNS IN ACCORDANCE WITH THE MUTCD AND NYS SUPPLEMENT.
- THE AREA AND PAYMENT AREA FOR SIGNS ARE FROM THE APPLICABLE STANDARD SHEETS OR SIGN FACE LAYOUTS.
- 3. ADDITIONAL SIGNS AT SIGNALS SHOWN ON TSP-DWGS.
- 4. FOR SIGNS NYP2-3R (MOD) AND NYP2-3L (MOD), USE LAYOUT FOR SIGN NYP2-3. TEXT HEIGHT OF "PAY AT STATION" SHALL BE 2"-C.
- 5. ALL TYPE 'A' SIGN POSTS INSTALLED IN CONCRETE OR PRECAST PAVER AREAS SHALL UTILIZE A PVC CONDUIT SLEEVE, COST TO BE INCLUDED UNDER ITEM 645.81.

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

SIGN DATA SHEET

FOR ALL CUSTOM SIGNS, THE RESIDENT ENGINEER SHALL PROVIDE JPEG FILES TO THE CONTRACTOR FOR SIGN FACE LAYOUT.

GP GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

DRAWING NO. SDS-1 SHEET NO. 62

CITY OF KINGSTON

CONTRACT NUMBER

DL34880

AFFIX SEAL: GREENMAN - PEDERSEN, INC ON: NOVEMBER 12, 2019 ALTERED BY: ON:

LOCATION NUMBER	STATION SIDE		SIDE DESCRIPTION		ITEM 647.41	ITEM 647.51	1TEM 647.61	17E	
1-1	£ 5+88 ±	ł.T	NO PARKING ANY TIME			1			
1-2	B 6+52 ±	i.T	20 MIN PARKING		į	1		-	
	8 7+34 ±	1.7	RT 32 + SLIGHT RIGHT + NO PARKING		ĺ				
1-3	8 /+34 <u>r</u>	ĹΤ	IN DRIVEWAY	1		1			
	2.7.03		HOSPITAL + *WEEKENO SHUTTLE						
1-4	B 7+92 ±	RT	PICK UP HERE	1	1				
2-1	B 10+28 ±	£Τ	JCT 1-587 + JCT RT 28	1					
2-2	B 10+78 ±	ŁT	KINGSTON 1777 TRAIL		1				
2-3	B 11+66 ±	ŁT	FIRE HYDRANT	1					
2-4	B 11+72 ±	ŁΤ	ELMENDORF ST + BROADWAY	1	į				
2-5	B 12+89 ±	ŁŦ	NO PARKING HERE TO CORNER			1			
3.6	5.14.30		*THE KINGSTON LIBRARY +						
2-6	B 14+78±	ŁŤ	EMERGENCY SNOW ROUTE	1	1				
2-7	B 14+39±	RT	STOP			1		l	
2-8	B 13+82 ±	RT	KINGSTON 1777 TRAIL		1				
			BROADWAY + LIBERTY ST + SNOW						
2-9	B 11+81 ±	RT	EMERGENCY ROUTE + *BROADWAY	2	1		İ		
			THEATER PARKING						
3-1	B 16+40±	LT	BROADWAY + DOWNS ST	1					
3-2	B 17+14±	LT	BUS STOP		1	• • • • • • • • • • • • • • • • • • • •			
3-3	8 19÷10 ±	LT	RESERVED PARKING			1			
3-4	8 19÷31±	LT	RESERVED PARKING			1			
3-5	8 19+52 ±	RT	STOP + DO NOT ENTER	1		1			
		1	DO NOT ENTER + ONE WAY (x2) +		-				
3-6	8 19+18±	RT	BROADAY + VAN DEUSEN ST	2	{				
			SNOW EMERGENCY ROUTE + ONE						
3-7	B 17+55 ±	RT	WAY + BROADWAY + VAN BUREN ST	2					
3-8	B 17+11 ±	RT	ONE WAY	1					
4-3	8 21+54 ±	LT	BROADWAY + NEIL ST	1	[İ	
	*********	1	SOUTH RT 32 LEFT + NORTH RT 32		-				
4-2	B 21÷92 ±	LT	STRAIGHT + RT 213 + TO THRUWAY	2			1		
4-3	B 24+29 ±	LT	STOP			1			
4-4	B 24+66 ±	LT	FIRE HYDRANT	1			***************************************	·	
		1	YIELD TO PEDESTRIAN IN						
4-5	8 24+76 ±	LT	CROSSWALK			1			
4-6	B 25÷06±	LŦ	JCT RT 32 + JCT RT 213	2					
		1	YIELD TO PEDESTRIAN IN						
4-7	B 23+94 ±	RT	CROSSWAŁK			1			
		†	HERITAGE AREA + KINGSTON						
4-8	B 21+92 ±	RT	VISITOR CENTER + STRAIGHT		2				
4-9	8 21÷88 ±	RT	ONE WAY (x2) + STOP			2			
		1	RT 32 + LEFT + SENATE HOUSE + LEFT						
4-10	8 21+33 ±	RT	+ WATERFRONT + BIKE 32 +	1	3		1		
-		1	STRAIGHT	_	_				
4-11	B 20+84 t	RT	FIRE HYDRANT	1					
			BROADWAY + HENRY ST + KINGSTON		į			<u> </u>	
4-32	B 20+60 ±	RT	1777 TRAIL + BUS STOP	1	2			[
4-13	8 20+52 ±	RT	RT 32 + RIGHT + RT213 + RIGHT	1					
5-1	8 26÷54 ±	LT	BUS STOP + NO PARKING ANY TIME	·- 	1	1		Ι	
5-2	8 29+29 ±	LT	KINGSTON 1777 TRAIL		1				
5-3	8 29÷90 ±	LT	NO LEFT TURN			1			
5-4	B 29+97 ±	LT	BROADWAY + THOMAS ST	1				İ	
5-5	B 30+38±	RT	NO PARKING BETWEEN SIGNS		-	1			
5-6	B 29+80 ±	RT	NO PARKING BETWEEN SIGNS			1		 	
5-7	B 29+77 ±	RT	NO LEFT TURN			1			
5-8	B 29+10±	RT	BUS ROUTE		1				
J.0	0.50510.7	1 11	BRIDGE AHEAD CLEARANCE 12'-10" +		*				
5-9	B 28+02 ±	RT	DISSOC MECAD CECATAMACE 12-10 T	1	}	1	I		

SIGN REMOVAL/RELOCATE TABLE

			SIGN REMOVAL/RELOCATE TABLE	(CONT.)				
LOCATION STATION SI		SIDE	DESCRIPTION	17EM 647.31	*ITEM 647.41	ITEM 647.51	ITEM 647.61	17EM 645.81
5-10	B 27+76 ±	RT	BROADWAY + CEDAR ST	1				
6-1	B 32+15±	ŧт	CAUTION DO NOT PROCEED WHEN WATER LEVEL REACHES				1	
6-2	8 34+55 t	i.T	NO PARKING BETWEEN SIGNS			1		
6-3	B 35+03 ±	1.T	NO PARKING BETWEEN SIGNS			1	1	
6-4	B 36+10±	ŁΤ	NO PARKING FROM HERE TO CORNER			1		
6.5	B 34+11 ±	RT	NO PARKING BETWEEN SIGNS		1			
6-6	B 33+66 ±	RT	SNOW EMERGENCY ROUTE + KINGSTON 1777 TRAIL	1	1			1
6-7	6 32+04 ž	RT	CAUTION DO NOT PROCEED WHEN WATER LEVEL REACHES				1	
7-1	8 36+43 ž	ŧΪ	BROADWAY + DEDRICK ST + KINGSTON 1777 TRAIL + NO PARKING	1	1	1		3
7-2	B 36+46 ±	1.T	BRIDGE AHEAD CLEARANCE 12'-10"	1				3
7-3	B 36+57±	ŁT	STOP			1		
			TOTALS	31	18	25	2	25

REFLECTIVE SIGN POST STRIPS							
SIGN STRUCTURE	ITEM 645.81020003 (EA)						
2-9	2						
2- 20	2						
3-12	2						
3-21	2						
3-22	2						
3-23	2						
4-6	2						
4-12	2						
4-13	2						
7-1							
TOTAL:	20 EA						

SIGNING SUMMARY TABLE					
ITEM NUMBER	QUANTITIY				
645.5101	147 SF				
645.5102	138 S F				
645.5202	81 SF				
645.81	165 EA				
645.81020003	20 £A				
647.31	31 EA				
647.41	18 EA				
647.51	25 EA				
647.61	2 EA				

SIGN STRUCTURE	STATION*	OFFSET	# OF POST FOOTINGS	POST SPACING	PANEL WIÐTH	COMMENTS	(TEM) 645.830102** (EA)
161	B 14+90	LŤ	2	2'-0"	44"		2
162	B 13+51	RT	2	2'-0"	44"	ALL SIGN EDGES	2
314	B 37+35	RT	1	-	24"	SHALL BE 2'-0°	1
315	B 17+73	LT	2	2'-0'	42"	FROM FACE OF	2
161A	B 26+35	RT	2	2'-0"	44"	CURB	2
1618	B 27+81	ĻΤ	2	2'-0"	44"		2
						TOTAL:	11

PROVIDED AND INSTALLED BY OTHERS.

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

PIN 8780.49 RTE 32 KINGSTON CONNECTIVITY BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON COUNTY: ULSTER REGION: 8

BRIDGES CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

SIGN DATA SHEET

CONTRACT NUMBER

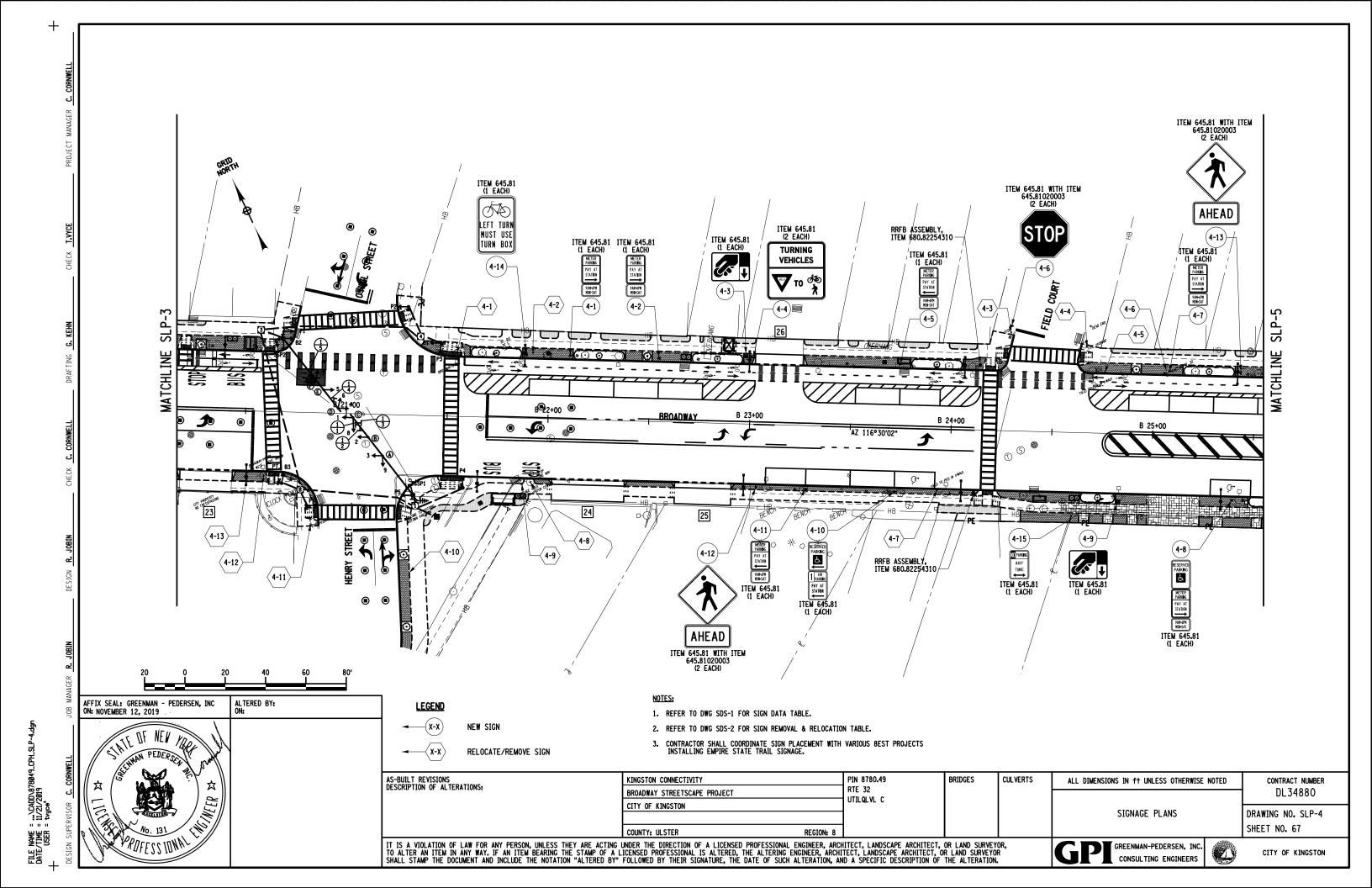
DRAWING NO. SDS-2 SHEET NO. 63

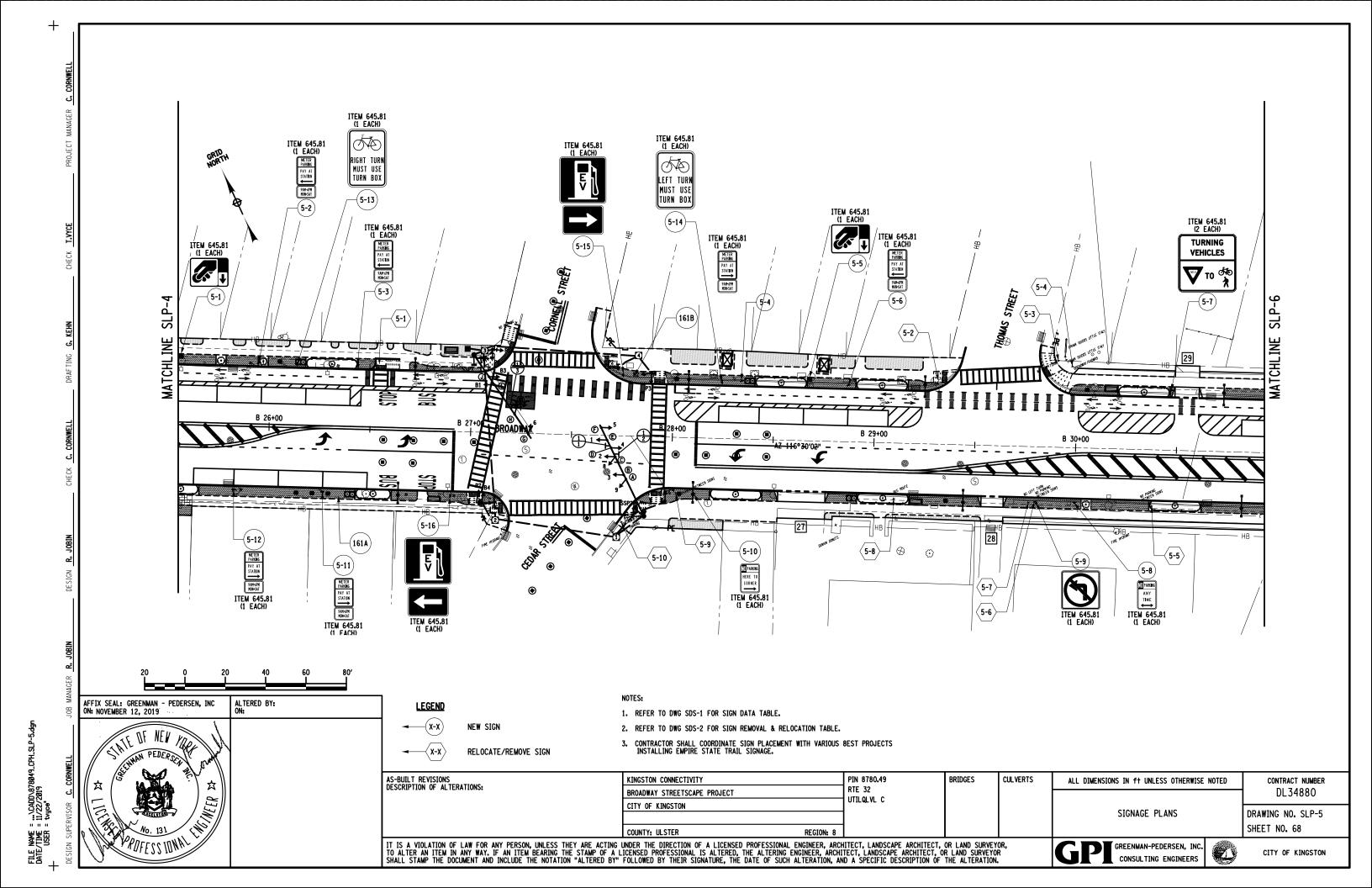
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

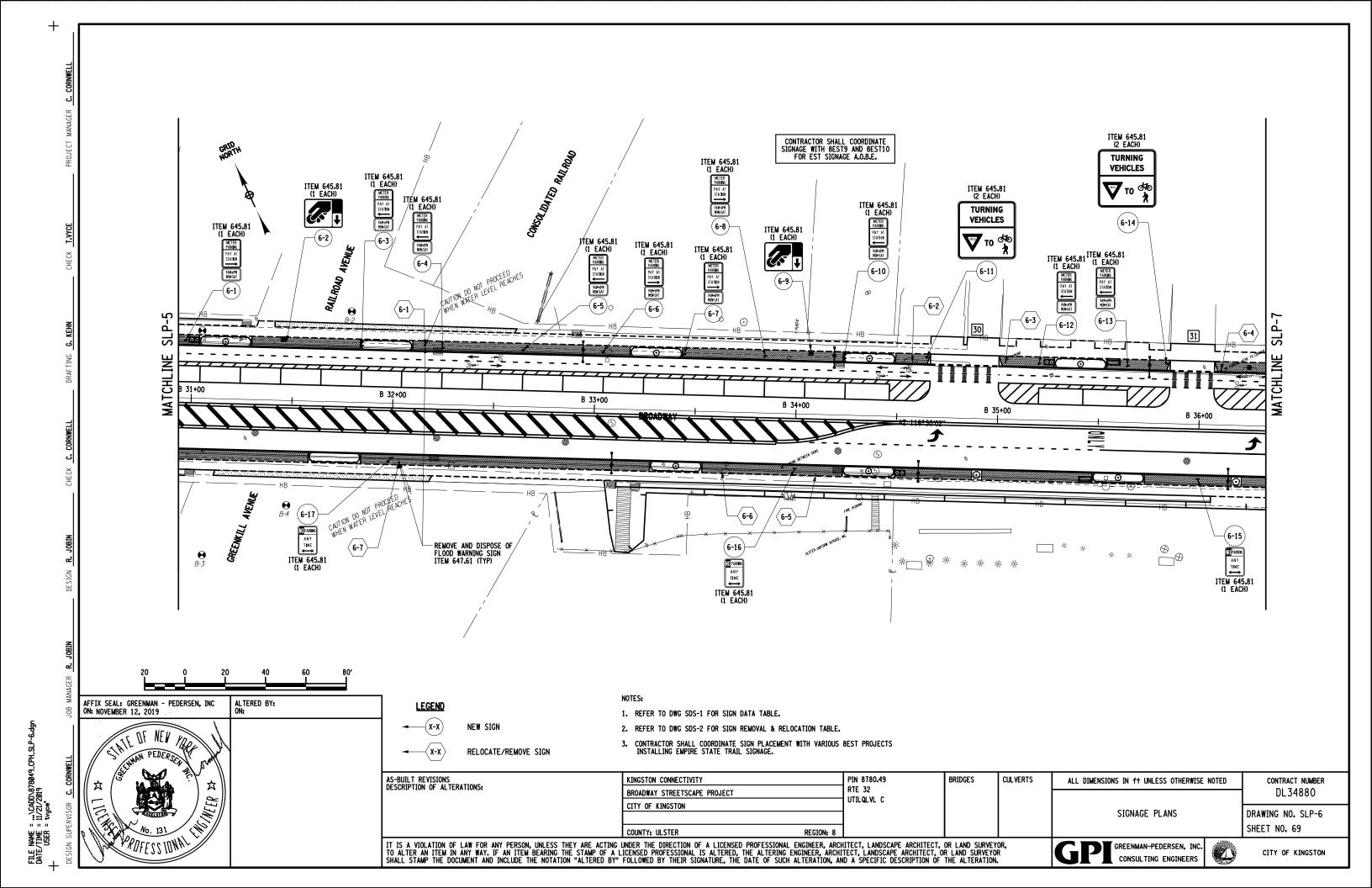
GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

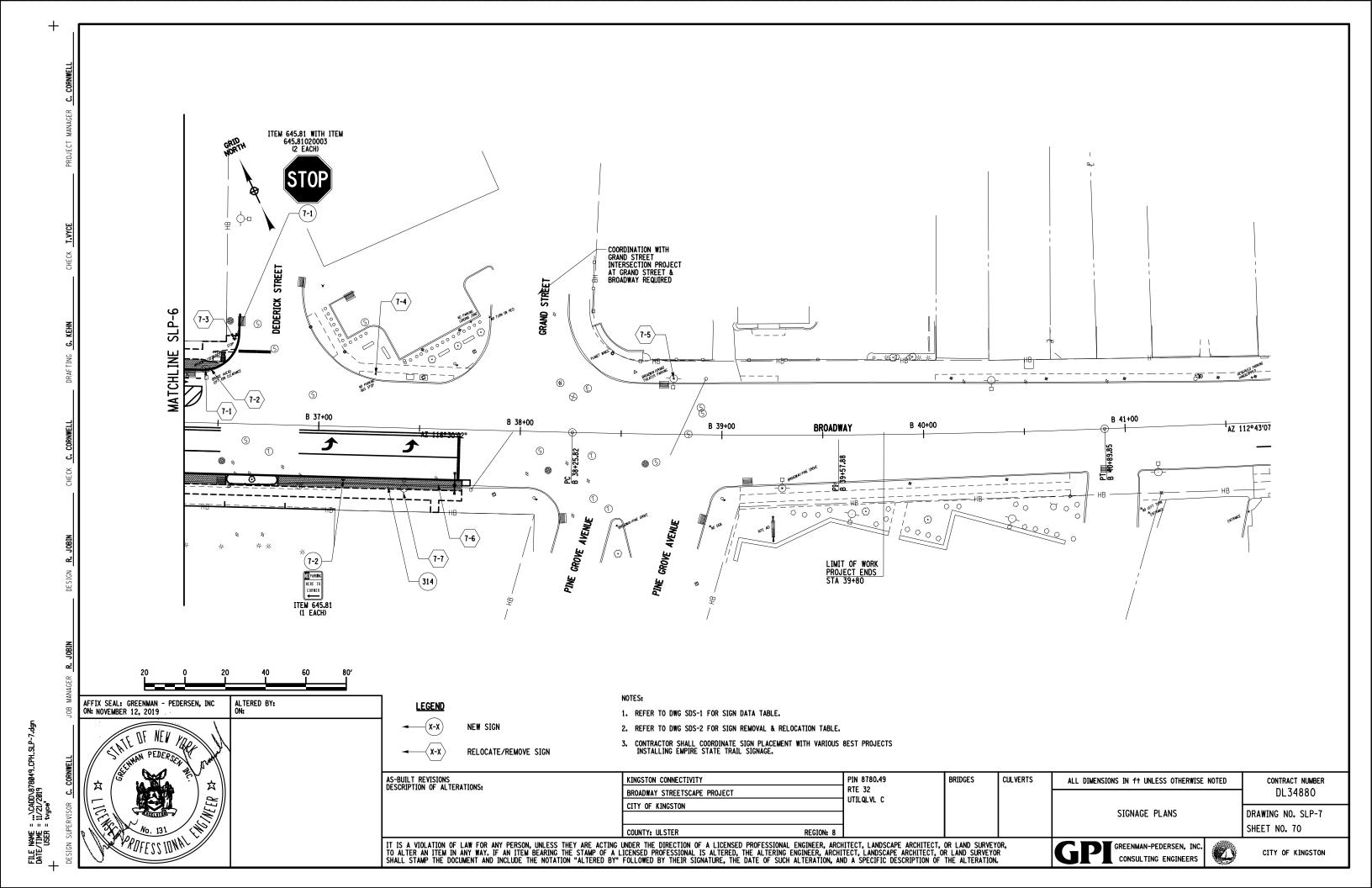
CITY OF KINGSTON

DL34880









AFFIX SEAL: GREENMAN - PEDERSEN, INC

STATE OF NEW MAR

SETHAN PEDERSON

 $\mathcal{E}_{\mathcal{I}}$

ON: NOVEMBER 12, 2019

TRAFFIC SIGNAL NOTES:

- ALL TRAFFIC SIGNAL EQUIPMENT SHALL BE INSTALLED IN ACCORDANCE WITH THE CURRENT NYSDOT STANDARD SPECIFICATION AND APPLICABLE NEW YORK STATE STANDARD SHEETS, EXCEPT AS MODIFIED BELOW OR IN THE CONTRACT
- UNLESS OTHERWISE NOTED, THE CITY OF KINGSTON SHALL HAVE MAINTENANCE JURISDICTION OVER ALL SIGNALS IN THIS CONTRACT UPON COMPLETION OF THEIR INSTALLATION AND OFFICIAL ACCEPTANCE.
- THE CONTRACTOR SHALL NOTIFY THE CITY OF KINGSTON ENGINEERING DEPARTMENT (845-334-3967) 48 HOURS IN ADVANCE OF ANY SCHEDULED
- UNDER NO CONDITION SHALL THE CONTRACTOR MAKE THEIR OWN SERVICE CONNECTIONS. ALL SERVICE CONNECTIONS SHALL BE MADE BY CENTRAL HUDSON GAS AND ELECTRIC WEST, (845) 334-3575.
- IF, AT ANY TIME DURING THE INSTALLATION OF THE TRAFFIC SIGNAL POLES, OR SIGNAL SPAN WIRES AND MESSENGER CABLES, IT IS DISCOVERED THAT THE MINIMUM CLEARANCES FROM PRIMARY/SECONDARY POWER CONDUCTORS AS REQUIRED BY SECTION 23 OF THE OF THE NATIONAL ELECTRICAL SAFETY CODE (ANSI STANDARD C2) AND LOCAL UTILITY CODES CAN NOT BE ACHIEVED, THE UTILITY COMPANY OWNING SUCH CONDUCTORS SHALL BE NOTIFIED IN WRITING OF THE PROBLEM. NO FURTHER WORK SHALL BE DONE UNTIL SAID POWER LINES HAVE BEEN RELOCATED TO PROVIDE THE PROPER CLEARANCES.
- THE LOCATIONS OF ALL UNDERGROUND OBJECTS HAVE NOT BEEN SHOWN ON THE PLANS. THE CONTRACTOR SHALL SATISFY THEMSELVES OF EXISTING CONDITIONS, AND SUPPORT AND PROTECT ALL FACILITIES ENCOUNTERED IN THE TRENCHING AND EXCAVATING OPERATIONS.
- WHERE UNFORESEEN UNDERGROUND UTILITIES MAKE IT NECESSARY TO RELOCATE SIGNAL POLES MORE THAN 5 FEET FROM THEIR ORIGINAL LOCATION AS SHOWN ON THE PLANS, THE CITY OF KINGSTON SHALL BE MEDIATELY NOTIFIED AND ALTERNATE LOCATIONS WILL BE SUPPLIED BY
- A CARD SHALL BE PLACED ON THE INSIDE DOOR OF EACH CONTROLLER CABINET LISTING ALL THE COMPONENT PARTS BY SERIAL NUMBER, THE DATE OF INSTALLATION AND DATE OF OFFICIAL OPERATION.
- TERMINAL BLOCKS PROVIDED IN THE CONTROLLER CABINETS SHALL BE OF THE BINDER HEAD SCREW TYPE AND SHALL BE MOUNTED WITH THE SCREW HEADS ON THE FRONT FACE OF THE BLOCKS FACING THE OPPOSITE WALL OF
- WHERE MICROCOMPUTER EQUIPMENT IS TO BE INSTALLED, THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING THAT EQUIPMENT IN THE NEW SYSTEM UPON THE COMPLETION OF THE TESTS SET FORTH IN SECTION 680-3.32 OF THE STANDARD SPECIFICATIONS. IF DURING THE PERIOD BETWEEN THE START OF THE MAINTENANCE AND THE COMPLETION OF THE CONTRACT, A MATERIAL OR WORKMANSHIP DEFECT IS FOUND OUTSIDE THE MICROCOMPUTER EQUIPMENT SUPPLIED, THE CONTRACTOR WILL BE DESPANSIBLE FOR COMPETCING SICH DEFECT AT THER OWN FYPENSE. RESPONSIBLE FOR CORRECTING SUCH DEFECT AT THEIR OWN EXPENSE
- BALANCE ADJUSTERS AND SWIVEL BALANCERS WILL BE INSTALLED AT EACH SIGNAL HEAD ASSEMBLY. ASSOCIATED PIPE EXTENSIONS SHALL NOT EXCEED 30", UNLESS ORDERED BY THE CITY.
- THE COST OF THE CONDUIT WITHIN THE POLE FOUNDATION SHALL BE INCLUDED IN THE PRICE BID FOR THE FOUNDATION. THE COST OF THE CONDUITS WITHIN A GROUND-MOUNTED CONTROLLER BASE SHALL BE INCLUDED IN THE PRICE BID FOR THE CONTROLLER BASE. ALL POLE BASES SHALL HAVE ONE UNUSED CONDUIT IN THE BASE CAPPED FOR FUTURE USE. THIS SHALL BE IN ADDITION TO THE CONDUITS NECESSARY FOR THE OPERATION OF THE WORKING SIGNAL SYSTEM.
- NO SPLICES SHALL BE ALLOWED IN THE SIGNAL CONTROL CABLE WITH THE EXCEPTION OF THE LEAD-IN CABLE TO THE INDUCTANCE LOOP CABLE.
- UPON COMPLETION OF THE WORK AT THE INTERSECTION, THE CITY OF KINGSTON MAINTENANCE PERSONNEL WILL PERFORM AN INSPECTION OF THE SIGNAL SYSTEM. AT THE TIME OF THE INSPECTION, CONTRACTOR SHALL PROVIDE ACCESS TO ALL PULLBOXES FOR INSPECTION PURPOSES. THE CONTRACTOR WILL BE NOTIFIED OF ANY DEFECTS FOUND DURING THE INSPECTION AND WILL MAKE NECESSARY CORRECTIONS BEFORE THE INTERSECTION WILL BE ACCEPTED.
- POWER SHALL BE SUPPLIED TO THE SIGNALS VIA 1 NPS METAL STEEL CONDUIT PAID FOR UNDER ITEM 680.520103 CONDUIT, METAL STEEL ZINC COATED, 1 NPS. THE PRICE BID UNDER THIS ITEM SHALL INCLUDE THE COST OF INSTALLING THE CONDUIT AS SHOWN IN THE PLANS. 15.

ALTERED BY:

- 16. VEHICLE SENSORS SHALL BE CENTERED IN THEIR RESPECTIVE LANES AS DETAILED ON DWG. TSD-1. THE CONTRACTOR SHALL COORDINATE WITH CITY OF KINGSTON ENGINEERING DEPARTMENT PERSONNEL FOR LAYOUT APPROVAL
- TEN *2 AND TEN POLICE DOOR KEYS SHALL BE SUPPLIED WITH THE CONTROLLER CABINET.
- 18. FOR NEW ANCHOR BASE POLE INSTALLATIONS:
- THE CONCRETE POLE FOUNDATION SHALL CURE FOR A MINIMUM OF 14 DAYS BEFORE THE SIGNAL POLE IS ERECTED, AND SHALL CURE FOR A MINIMUM OF 28 DAYS BEFORE THE SPAN WIRE AND TRAFFIC SIGNAL HEADS ARE
- THE COST OF THE ANCHOR BOLTS AND THE STEEL REINFORCING SHALL BE INCLUDED IN THE PRICE BID UNDER ITEM 680.5001-POLE EXCAVATION AND
- 19. CONSTRUCTION SIGNS SHALL BE INSTALLED ON ALL LEGS OF AN INTERSECTION WHILE THE CONTRACTOR IS WORKING AT THAT INTERSECTION. ADDITIONAL CONSTRUCTION SIGNS SHALL BE INSTALLED AS REQUIRED BY THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, COST SHALL BE INCLUDED WITH THE PROPERTY OF THE IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
- 20. UNLESS OTHERWISE NOTED, ITEM 680.510501-PULLBOXES, RECTANGULAR, 18"x26", REINFORCED CONCRETE, SHALL BE USED IN ALL SIGNAL
- 21. ALL ANCHOR BASE TRAFFIC SIGNAL POLES AND PEDESTRIAN SIGNAL POLES SHALL BE EQUIPPED WITH ANCHOR BOLT COVERS.
- ALL HOLES FOR LB CONDUITS, CONDUITS, EYEBOLTS, AND POLE MOUNTED SIGNAL AND/OR PEDESTRIAN HEADS SHALL BE DRILLED. THE USE OF CUTTING TORCHES SHALL NOT BE PERMITTED UNLESS WRITTEN PERMISSION IS GRANTED IN ADVANCE BY THE CITY OF KINGSTON.
- 23. IN AREAS WHERE SIGNAL POLES ARE INSTALLED OR WHERE SIGNAL EQUIPMENT IS REMOVED, ANY DISTURBED GROUND SHALL BE RESTORED TO A CONDITION MATCHING ADJACENT AREA. PAYMENT FOR WORK AND MATERIALS IS TO BE INCLUDED IN THE BID PRICE AS APPLICABLE.
- 24. IN THE EVENT THE UNSUITABLE MATERIALS SUCH AS SILTS, SOFT CLAY OR ORGANIC SOILS ARE ENCOUNTERED IN THE EXCAVATION FOR A POLE FOUNDATION, THE CITY OF KINGSTON ENGINEERING DEPARTMENT SHALL BE
- WHERE SOUND BEDROCK IS ENCOUNTERED DURING POLE EXCAVATION OPERATIONS AND THE DEPTH OF THE SOIL OVER THE ROCK IS ONE-THIRD OR LESS THAN THE "MINIMUM EMBEDMENT" LENGTH GIVEN IN THE TABLE ON STANDARD SHEET 680-01, THEN THE FOOTING LENGTH MAY BE DECREASED SO AS TO PROVIDE AN EMBEDMENT INTO THE ROCK EQUAL TO THE FOOTING DIAMETER PLUS 2 FT. FOR DEEPER OVERBURDENS, THE FOOTING LENGTH MAY BE DECREASED TO PROVIDE EMBEDMENT INTO ROCK EQUAL TO THE FOOTING DIAMETER. IF THE RESULTING DEPTH IS LESS THAN THE LENGTH OF THE ANCHOR BOLTS, THE CONTRACTOR SHALL EXCAVATE TO THE LENGTH THE ANCHOR BOLTS PLUS 6 INCHES.
- 26. PRIOR TO INSTALLATION THE CONTRACTOR SHALL VERIFY THE SUITABILITY OF EACH LOCATION WHERE EACH TRAFFIC SIGNAL POLE AND SPAN WIRE ASSEMBLY IS TO BE LOCATED AT THE INTERSECTION.
- 27. ALL THIMBLE BOLTS USED FOR TRAFFIC SIGNAL SPAN WIRE AND/ OR ALL HIMBLE BOL'S USED FOR TRAFFIL SIGNAL SPAN WIRE AND/ OR MESSENGER CABLE ATTACHMENTS, AS SHOWN ON STANDARD SHEETS 680-06, 680-07, AND 680-16 SHALL BE 3/4" AND SHALL HAVE A BREAKING STRENGTH EQUAL TO THAT OF ATTACHED SPAN WIRE AS SHOWN IN NOTE 6 OF STANDARD SHEET 680-680-07. THIMBLE BOLTS ARE TO BE USED IN PLACE OF THE EYEBOLTS SPECIFIED ON THE STANDARD SHEET.
- 28. WHERE SIGNAL CABLE IS INSTALLED ON SPAN WIRE OR MESSENGER CABLE, IT SHALL BE SUPPORTED AT INTERVALS OF NOT GREATER THAN 15" BY STAINLESS STEEL CABLE RINGS.
- 29. ON ALL SIGNAL HEAD ASSEMBLIES ALL FEMALE THREADED CONNECTING HARDWARE SHALL BE MANUFACTURED WITH TWO SET SCREWS AND ALL THREADED PIPE NIPPLES SHALL HAVE TAPERED THREADS.
- THE BOTTOM OF ALL TRAFFIC SIGNAL HEADS SHALL BE LOCATED A MINIMUM OF 16'-6" ABOVE THE PAVEMENT SURFACE.
- ONCE TRAFFIC SIGNAL HEAD ASSEMBLIES HAVE BEEN INSTALLED AND ADJUSTED IN THE FIELD TO THE SATISFACTION OF CITY MAINTENANCE PERSONNEL, THE CONTRACTOR SHALL APPLY A BEAD OF SILICONE SEALANT AROUND THE SERRATED LOCK RING AT ALL LOCATIONS WHERE VERTICAL PIPE NIPPLES ARE ATTACHED TO THE TOP SURFACE OF BOTH TRAFFIC SIGNAL SECTIONS AND PIPE CROSS BODIES, TO PREVENT WATER INFILTRATION INTO
- ALL POLES SHALL BE EQUIPPED WITH A GROUNDING TERMINAL ACCESSIBLE THROUGH THE HAND HOLE IN THE POLE PER SECTION 724-03 OF THE STANDARD SPECIFICATIONS, THIS TERMINAL SHALL BE LOCATED ON THE OPPOSITE SIDE OF THE POLE FROM THE HAND HOLE.

- 33. ALL TRAFFIC SIGNAL POLES SHALL BE INSTALLED SO THAT THE POLES HAVE A MINIMUM RAKE OF ONE QUARTER THE DIAMETER OF THE SIGNAL POLE MEASURED AT ITS BASE AFTER LOADING.
- 34. THE CONTRACTOR SHALL SALVAGE, AND DELIVER TO THE CITY, THE FOLLOWING EQUIPMENT REMOVED UNDER ITEM 680.79XX0008. CONTRACTOR SHALL COORDINATE DELIVERY LOCATION WITH THE CITY
 - A) TRAFFIC SIGNAL POLES, CONTROLLER CABINETS, TRAFFIC SIGNAL CONTROLLERS AND ALL ASSOCIATED EQUIPMENT, TRAFFIC SIGNAL HEADS, AND PULLBOX FRAMES AND COVERS.
- ALL SIGNAL POLES TO BE REMOVED UNDER ITEM 680.79XX0008 SHALL BE REMOVED AS DETAILED BELOW UNLESS OTHERWISE ORDERED BY THE
- EMBEDDED POLES SHALL BE CUT OFF TO A MINIMUM OF 6 INCHES
- ANCHOR BASE POLES SHALL BE REMOVED FROM THEIR BASE AND ANCHOR BOLTS SHALL BE CUT OFF TO A MINIMUM DEPTH OF 6 INCHES
- FOR BOTH TYPES OF POLES, THE FOUNDATION SHALL BE CHIPPED DOWN TO A MINIMUM DEPTH OF 6 INCHES BELOW GRADE.
- ALL HOLES IN THE BASE SHALL BE FILLED WITH CONCRETE AND THE AREA SHALL BE RESTORED TO A CONDITION MATCHING ADJACENT AREAS. PAYMENT FOR WORK AND MATERIALS IS TO BE INCLUDED IN THE PRICE
- ALL PULLBOXES TO BE REMOVED UNDER ITEM 680.79XX0008 SHALL BE REMOVED AS FOLLOWS: PULLBOX COVER AND FRAME SHALL BE REMOVED AND RETURNED TO THE CITY OF KINGSTON. THE PULLBOX SHALL BE FILLED WITH SUITABLE FILL MATERIAL MEETING THE REQUIREMENTS OF SECTION 733-08; EMBANKMENT IN PLACE OF THE NYSDOT STANDARD SPECIFICATIONS. THE AREA SHALL BE RESTORED TO A CONDITION
- 37. THE CONTRACTOR'S ATTENTION IS CALLED TO THE FACT THAT THE INSTALLATION OF ALL PEDESTRIAN CROSSING CONTROLS SHALL BE IN COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT AS OUTLINED COMPLIANCE WITH THE AMERICANS WITH DISABILITIES ACT AS OUTLIN IN THE PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY, JULY 2011 (PROWAG). PEDESTRIAN PEDESTALS SHALL BE LOCATED ADJACENT TO THE SIDEWALK SUCH THAT A 30"×48" LEVEL SIDEWALK SURFACE IS LOCATED WITHIN 10 INCHES OF EACH PEDESTRIAN PUSHBUTTON PER FEDERAL GUIDELINES. PUSHBUTTONS SHALL BE APS COMPLIANT WITH AUDIBLE AND TACTILE FEEDBACK.
- 38. WHEN CONDUIT EXCAVATION AND POLE EXCAVATION IS REQUIRED IN SIDEWALK AREAS, CONTRACTOR SHALL REPLACE THE ENTIRE SIDEWALK SLAB TO THE EXPANSION JOINT, SAWCUT AND REPAIR OF SIDEWALK FOR CONDUIT INSTALLATION AND POLE INSTALLATION IS UNACCEPTABLE.
 COST FOR SIDEWALK REPLACEMENT SHALL BE PAID FOR AS PART OF ITEM 206.03 - CONDUIT EXCAVATION AND BACKFILL.
- 39. IT SHALL BE THE CONTRACTORS RESPONSIBILITY, IN CONJUNCTION WITH THE SUPPLIER/MANUFACTURER, TO DETERMINE THE APPROPRIATE MAST ARM POLE STRENGTH AND FOOTING MOMENT FOR THE LOADING SHOWN IN ARM POLE STRENGTH AND FOOTING MOMENT FOR THE LUADING SHOWN IN THE PLANS. THE FOOTING DATA SHALL BE INCLUEDE IN THE SHOP DRAWING THE CONTRACTOR SUBMITS FOR APPROVAL BY THE ENGINEER. IN NO CASE SHALL THE FOUNDATION FOR ANY MAST ARM POLE BE SMALLER THAN A K-8 PER NYSDOT STANDARD SHEET 680-01. ALL MAST ARM FOUNDATIONS ARE TO BE SQUARE.
- 40. MAST ARM MOUNTING BRACKETS SHALL BE PELCO ASTRO-BRAC OR APPROVED EQUAL.
- 41. ALL MAST ARM POLES, SIGNAL POLES AND PEDESTRIAN POLES SHALL BE FLUTED WITH DECORATIVE BASE IN A STYLE APPROVED BY THE CITY ENGINEER. POLES SHALL BE COLORED BLACK AT THE FACTORY.
- SIGNAL CONTROLLERS PROVIDED UNDER ITEM NUMBERS 680.80149305 AND 680.93200101 SHALL BE ECONOLITE COBALT OR APPROVED EQUAL, CAPABLE OF COORDINATED SIGNAL OPERATIONS WITHIN THE CORRIDOR. ALL TRAFFIC SIGNALS ASSOCIATED WITH THIS PROJECT (SEE DWGS.
 TSP-1 THRU TSP-4) SHALL BE INTERCONNECTED WIRELESSLY, WITH THE
 CONTROLLER AT GRAND STREET SERVING AS THE SYNCING MASTER
 CONTROLLER FOR THE SYSTEM.

PIN 8780.49

RTE 32

	SIGNAL PLAN LEGEND							
PROPOSED	EXISTING	DESCRIPTION						
•	0	SIGNAL POLE						
—	—— <u> </u>	GROUND MOUNTED CONTROLLER						
		GROUND MOUNTED CONTROLLER						
•=	œ	POLE MOUNTED CONTROLLER						
		SPAN WIRE ASSEMBLY						
—s—	—- s—	MESSENGER CABLE INSTALLATION						
		CONDUIT						
•	3	PULLBOX (TRAFFIC SIGNALS & NUMBER)						
••	⊶	TRAFFIC SIGNAL HEAD - 1 WAY						
←→	4-0>	TRAFFIC SIGNAL HEAD - 2 WAY						
+1 +	↓ \$→	TRAFFIC SIGNAL HEAD - 3 WAY						
+++	↔	TRAFFIC SIGNAL HEAD - 4 WAY						
•-+>	0-00	PROGRAMMED SIGNAL HEAD - 1 WAY						
î.,	â O-DD	PROGRAMMED SIGNAL HEAD - 2 WAY						
3	3	SIGNAL HEAD ASSEMBLY NUMBER						
		INDUCTANCE LOOP & ALUMINUM JUNCTION BOX						
4	4	SIGN FACE NUMBER						
ø	Ø	PHASE						
		REFLECTORIZED PAV'T STRIPES						
þ	٥	TRAFFIC SIGN (GROUND MOUNTED)						
7	Т	OVERHEAD TRAFFIC SIGN						
+	<u> </u>	MICROWAVE DETECTOR						
		WIRELESS VEHICLE DETECTION SYSTEM SENSOR						
	\boxtimes	WIRELESS MICRO-RADAR BICYCLE SENSOR						
SSP 💻	SSP 🗀	WIRELESS VEHICLE DETECTION ANTENNA						
	++++	DIRECTIONAL ANTENNA FOR WIRELESS INTERCONNECT						
3 3		SIGN CALLOUT LOCATION NUMBER TEXT NUMBER						

ABBREVIATIONS

2SC = 2 CONDUCTOR SHIELDED LEAD-IN CABLE 3SC = 3 CONDUCTOR SHIELDED CABLE 4SC = 4 CONDUCTOR SHIELDED CABLE SCC = SHIELDED COMMUNICATIONS CABLE
3C = 3 CONDUCTOR SIGNAL CABLE

5C = 5 CONDUCTOR SIGNAL CABLE 1OC = 10 CONDUCTOR SIGNAL CABLE 15C = 15 CONDUCTOR SIGNAL CABLE 19C = 19 CONDUCTOR SIGNAL CABLE

ECC = ETHERNET COMMUNICATIONS CABLE = RED = YELLOW = GREEN = ORANGE = WHITE = BLACK

FR = FLASHING RED FY = FLASHING YELLOW
P.B. = PUSH BUTTON

O.H. = OVERHEAD N.T.S= NOT TO SCALE NFS = NATIONAL PIPELINE STANDARD
PE = PREEMPTION
OVLP= OVERLAP

NOTES

PED = PEDESTRIAN

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

KINGSTON CONNECTIVITY BROADWAY STREETSCAPE PROJECT CITY OF KINGSTON

BRIDGES

CULVERTS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED TRAFFIC SIGNAL

CONTRACT NUMBER DL34880

DRAWING NO. TSN-1 SHEET NO. 71

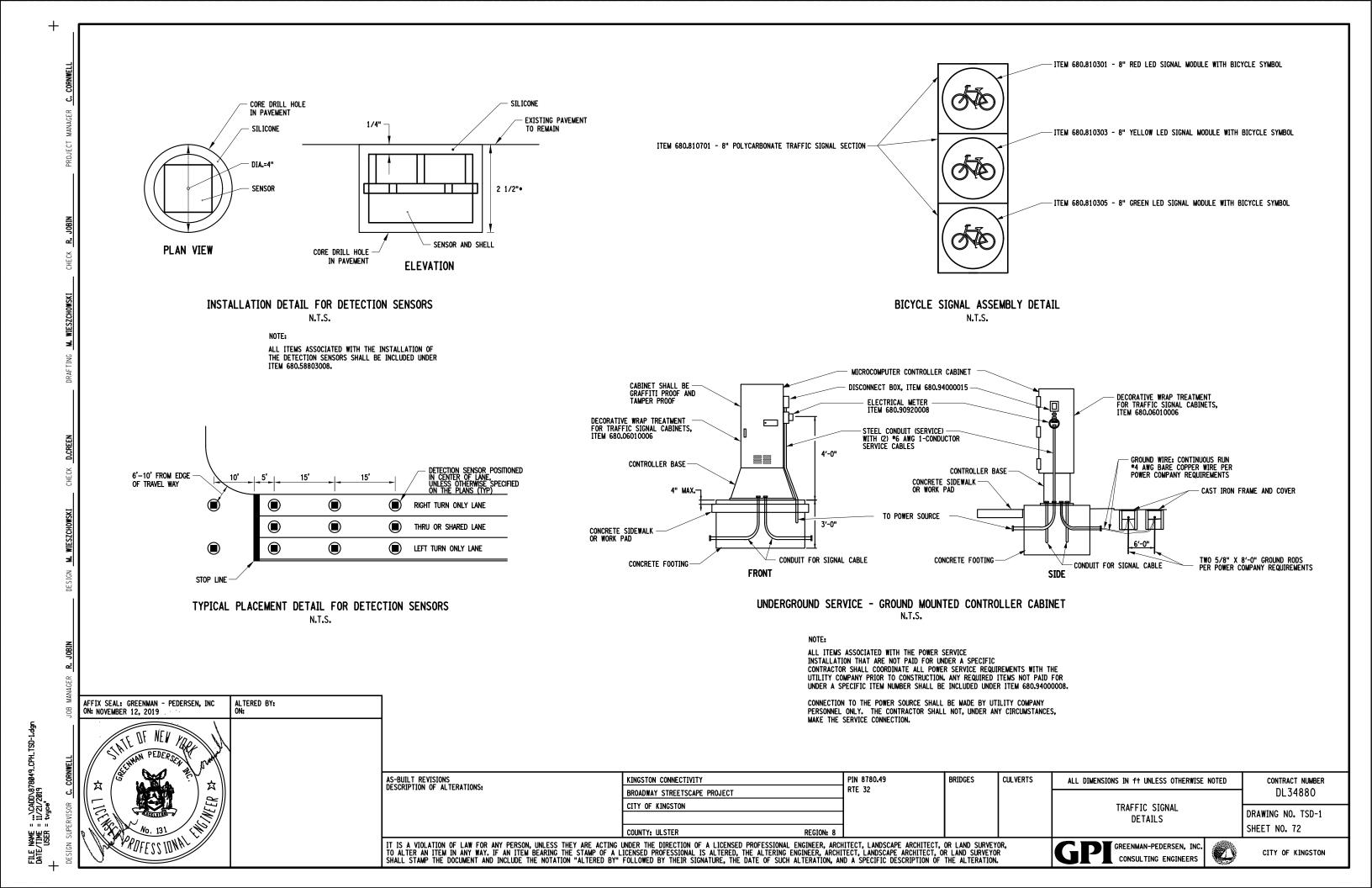
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

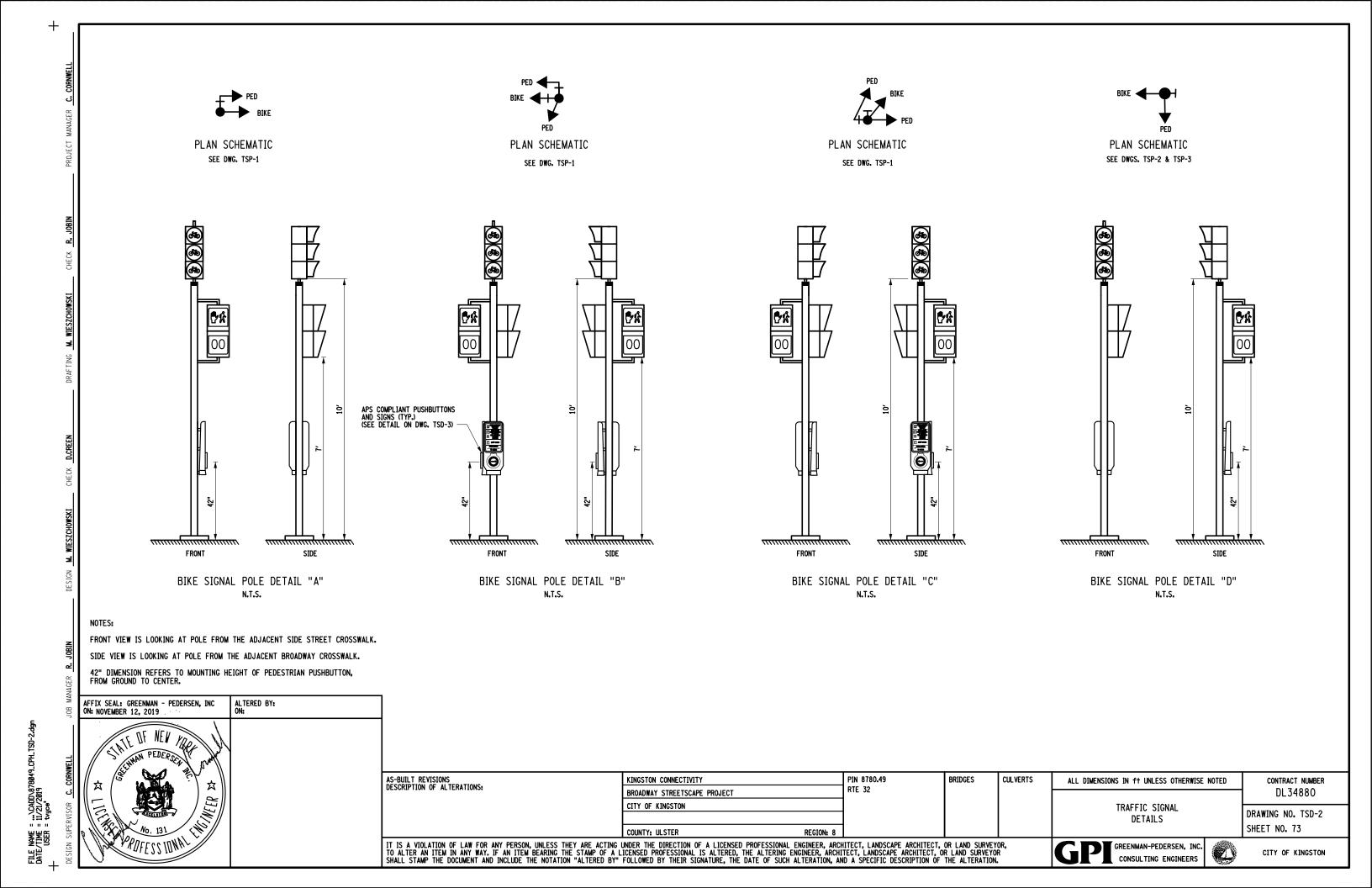
COUNTY: ULSTER





CITY OF KINGSTON





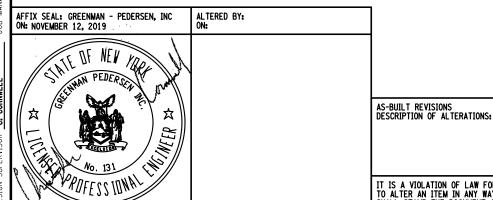


FILE NAME = ...\CADD\878849_CPH_TSD-3.c DATE/TIME = 11/21/2019 USER = tvyce*

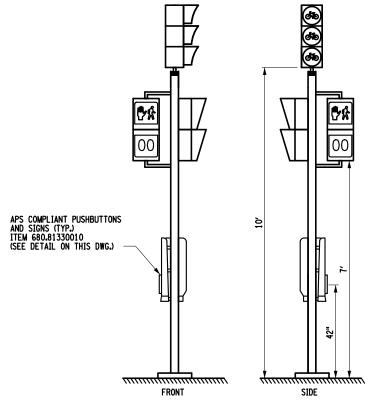
FRONT VIEW IS LOOKING AT POLE FROM THE ADJACENT SIDE STREET CROSSWALK. SIDE VIEW IS LOOKING AT POLE FROM THE ADJACENT BROADWAY CROSSWALK. 42" DIMENSION REFERS TO MOUNTING HEIGHT OF PEDESTRIAN PUSHBUTTON, FROM GROUND TO CENTER.

BIKE SIGNAL POLE DETAIL "E"

N.T.S.



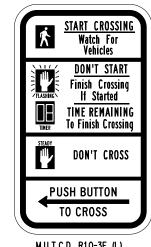


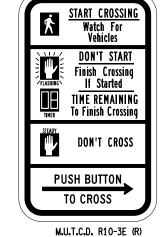


BIKE SIGNAL POLE DETAIL "F" N.T.S.



TYPICAL APS COMPLIANT PUSHBUTTON ITEM 680.81330010 N.T.S.





M.U.T.C.D. R10-3E (L)

BRIDGES

PEDESTRIAN SIGNAL SIGN DETAIL N.T.S.

PIN 8780.49

RTE 32

THE PRICE FOR THE PEDESTRIAN SIGNAL SIGN SHALL BE INCLUDED IN THE BID PRICE FOR 680.81330010.

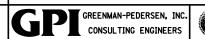
CULVERTS

COUNTY: ULSTER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

KINGSTON CONNECTIVITY

CITY OF KINGSTON

BROADWAY STREETSCAPE PROJECT



ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

TRAFFIC SIGNAL

DETAILS

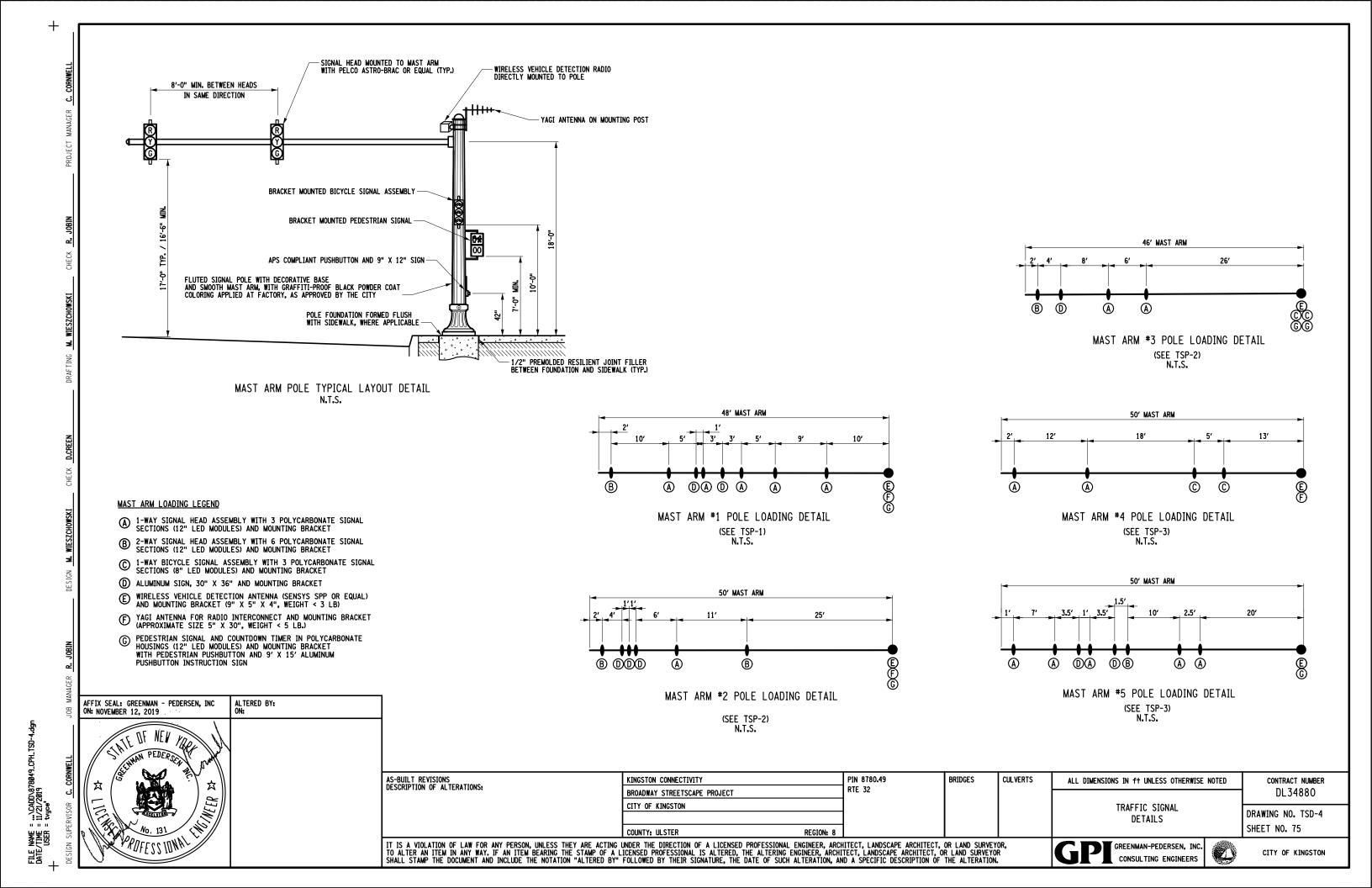
CITY OF KINGSTON

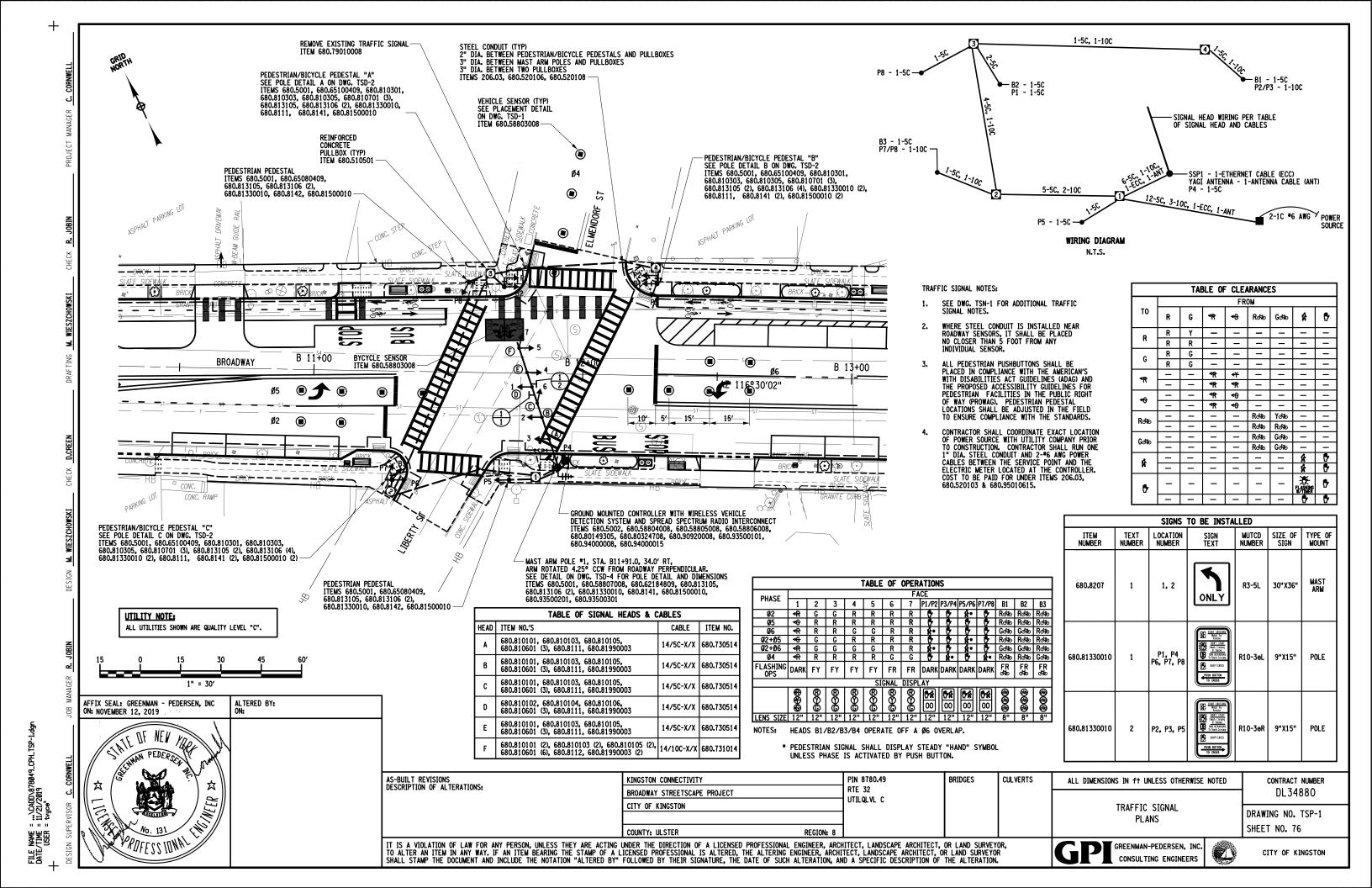
DRAWING NO. TSD-3

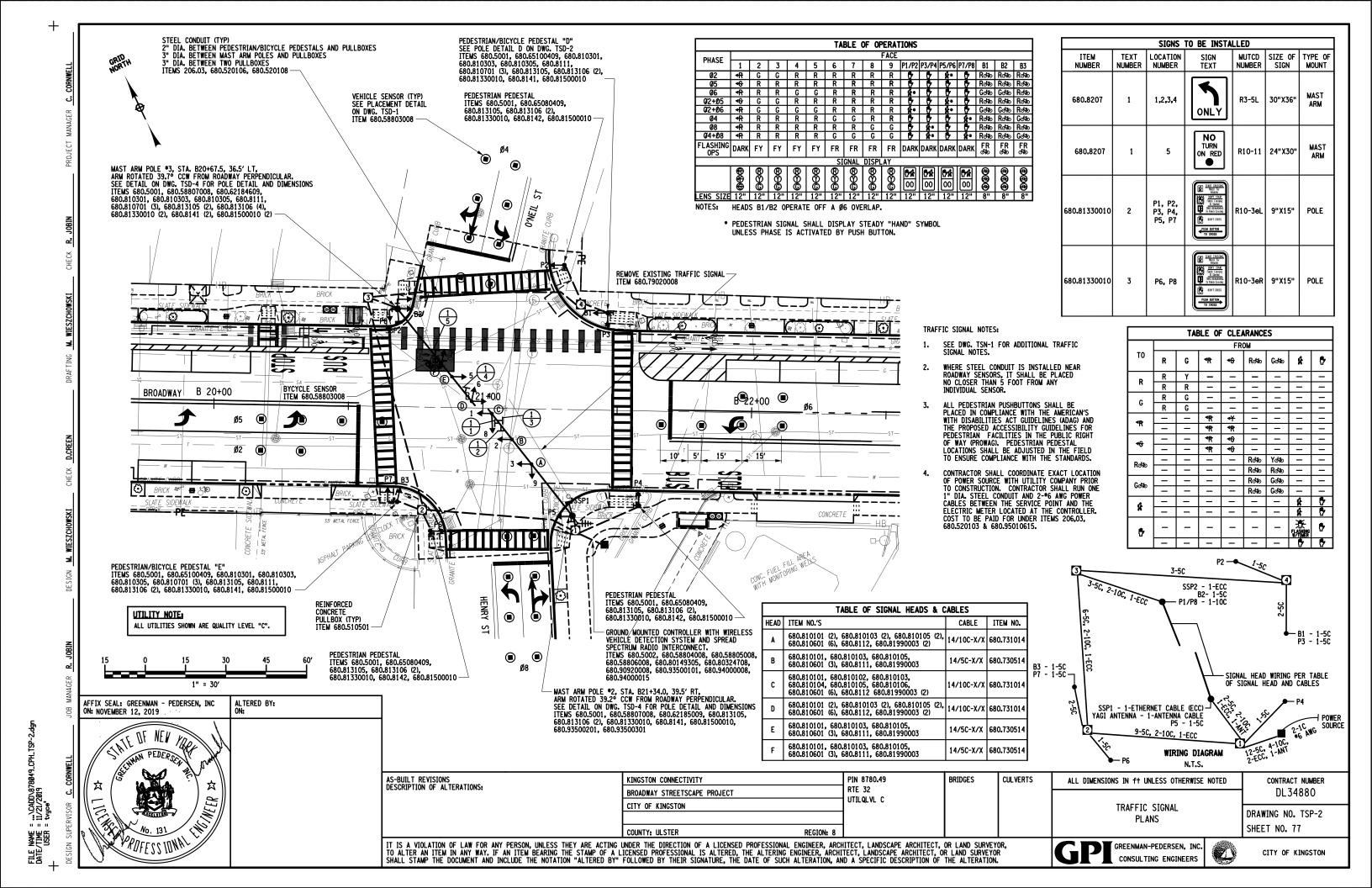
SHEET NO. 74

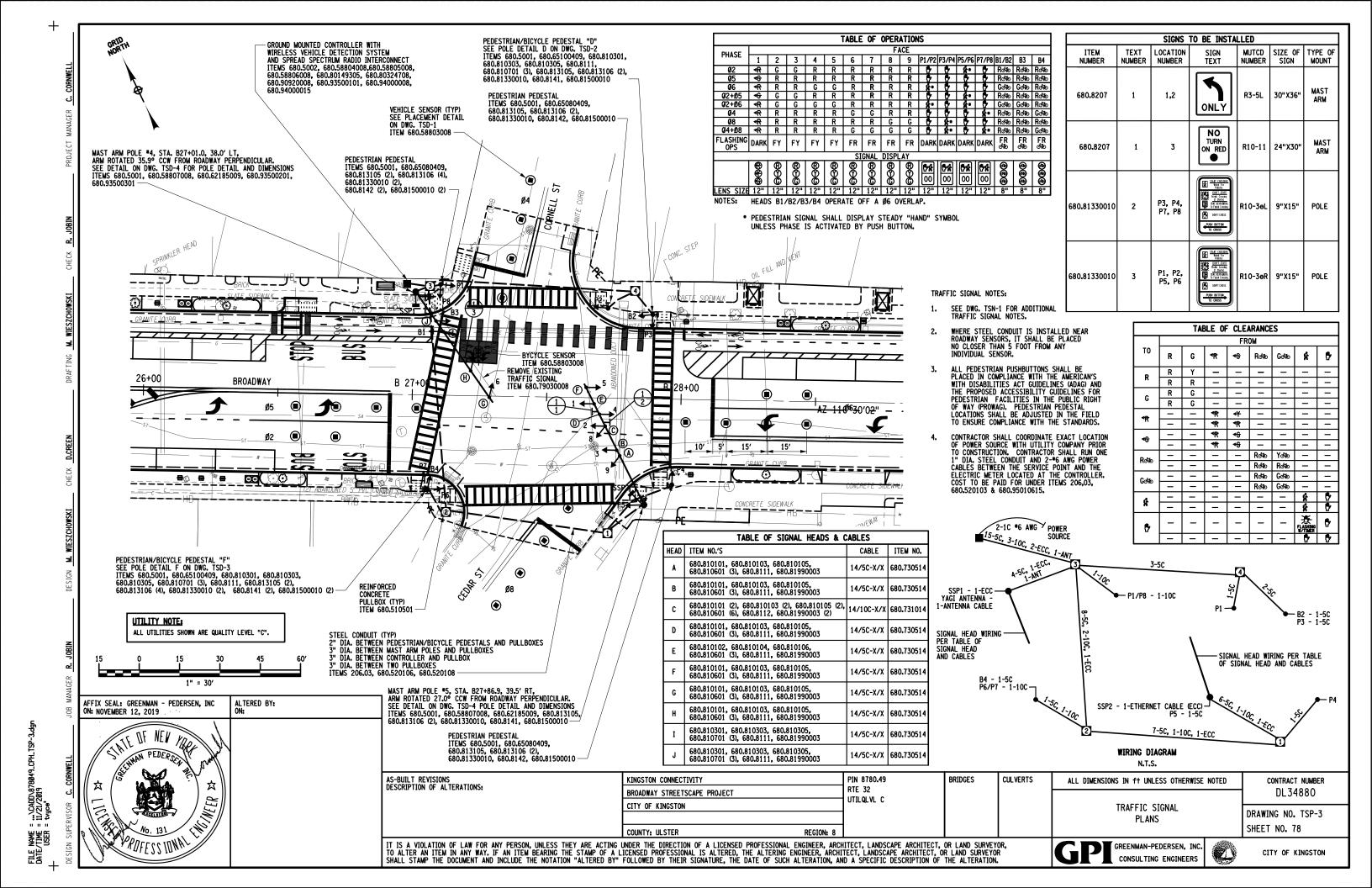
CONTRACT NUMBER

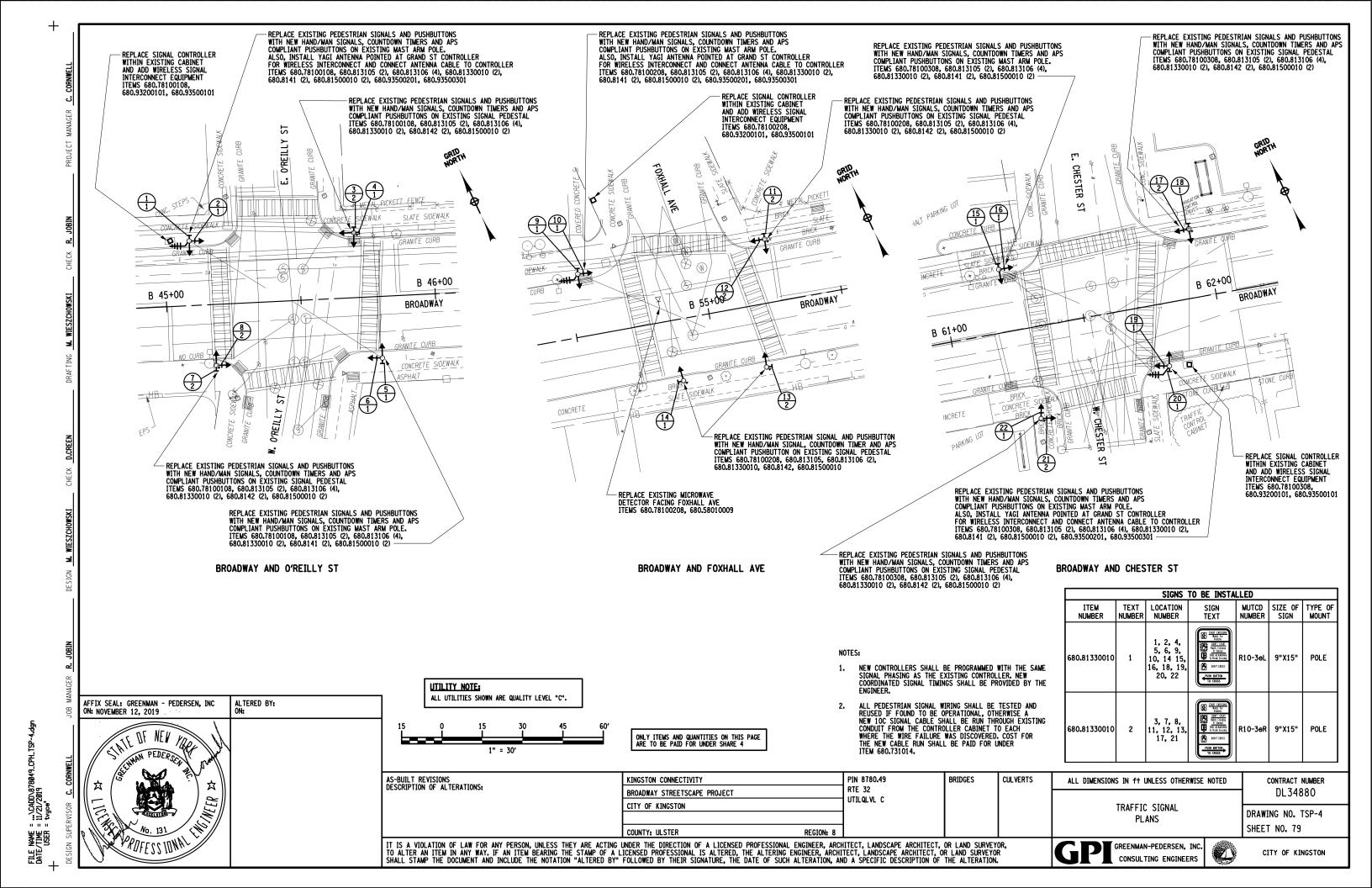
DL34880





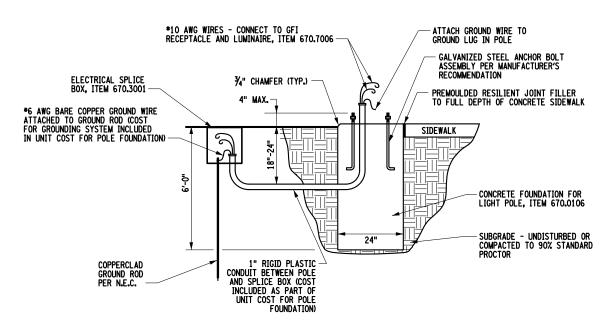




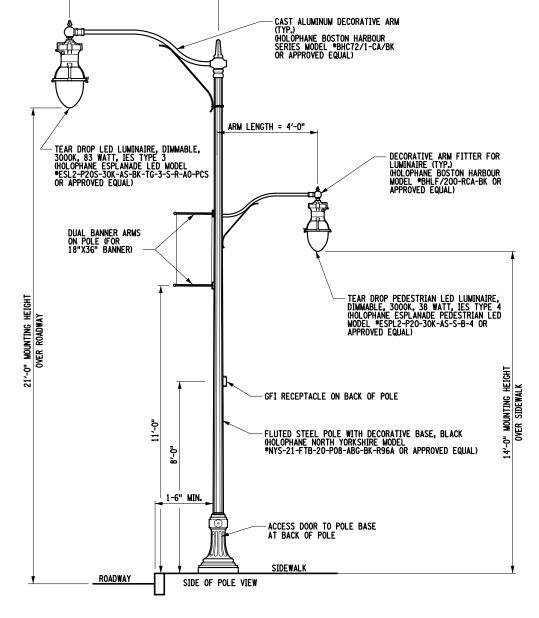


- 1. ALL WORK PERFORMED UNDER THIS CONTRACT SHALL BE IN CONFORMANCE
 WITH THE CURRENT NEW YORK STATE DEPARTMENT OF TRANSPORTATION (NYSDOT) STANDARD
 SPECIFICATIONS AS PROVIDED ON THE NYSDOT WEBSITE, UNLESS MODIFIED IN THE
 CONTRACT PLANS.
- 2. ALL LIGHTING EQUIPMENT SHALL BE INSTALLED IN ACCORDANCE WITH NEW YORK STATE STANDARDS SHEETS 670-01 TO 670-03, UNLESS MODIFIED IN THE CONTRACT PLANS.
- 3. CONTRACTOR SHALL CONFIRM POWER SERVICE REQUIREMENTS AND LOCATION WITH CENTRAL HUDSON GAS & ELECTRIC CORPORATION PRIOR TO CONSTRUCTION.
- 4. UNDER NO CONDITION SHALL THE CONTRACTOR MAKE THE SERVICE CONNECTIONS. THE CONTRACTOR SHALL RUN CONDUCTORS UP POLE THROUGH RISER AND PROVIDE ENOUGH SLACK AT THE TOP OF THE POLE FOR CENTRAL HUDSON GAS & ELECTRIC CORPORATION TO MAKE CONNECTION. CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH POWER COMPANY FOR THIS CONNECTION.
- 5. ANY FOOTINGS PLACED IN SIDEWALKS WILL BE CONSTRUCTED FLUSH TO THE SIDEWALK SURFACE AND BITUMINOUS JOINT FILLER SHALL BE PLACED BETWEEN THE SIDEWALK AND FOUNDATION.
- 6. EACH LIGHT SHALL BE INSTALLED WITH THE CAPABILITY OF BEING DIMMED. CONTRACTOR SHALL ADJUST DIMMING LEVEL TO THE SATISFACTION OF THE ENGINEER IN CHARGE AND CITY REPRESENTATIVE PRIOR TO FINAL INSPECTION. DIMMER CONTROLS AT EACH POLE SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 670.15100108.

	ESTIMATE OF ROADWAY LIGHTING QUANTITIES		
ITEM NO.	DESCRIPTION	UNIT	GRAND TOTAL
206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	LF	6,100
670.0106	FOUNDATION FOR LIGHT POLE, 6 FOOT LONG	EA	45
670.15100108	DECORATIVE STREET LIGHT POLE AND LUMINAIRE ASSEMBLY, TYPE A	EA	45
670.2602	RIGID PLASTIC CONDUIT, 2" DIA.	LF	6,100
670.3001	LIGHTING PULLBOX - LESS THAN 5 CF INSIDE VOLUME	EA	40
670.3010	LIGHTING PULLBOX - 7.5 TO 10 CF INSIDE VOLUME	EA	9
670.53070104	POWER POINT DISCONNECT ASSEMBLY - LOCATION A	EA	1
670.53070204	POWER POINT DISCONNECT ASSEMBLY - LOCATION B	EA	1
670.53070304	POWER POINT DISCONNECT ASSEMBLY - LOCATION C	EA	1
670.7002	SINGLE CONDUCTOR CABLE, *2 AWG	LF	800
670.7004	SINGLE CONDUCTOR CABLE, *6 AWG	LF	33,800
670.7006	SINGLE CONDUCTOR CABLE, *10 AWG	LF	4,600
670.7501	GROUND WIRE #6 AWG	LF	6,800
670.77000105	MODIFY LIGHTING INSTALLATION, TYPE 1 (REMOVE LIGHT POLE)	EA	44
670.77000205	MODIFY LIGHTING INSTALLATION, TYPE 2 (CIRCUIT TESTING)	EA	5
670.77000305	MODIFY LIGHTING INSTALLATION, TYPE 3 (BILLBOARD CONNECTION)	EA	2
680.700606	RISER ASSEMBLY, 2 INCH DIAMETER	EA	3
680.90920008	ELECTRIC METER SOCKET, 200 AMP, SINGLE PHASE 240 VOLT	EA	3

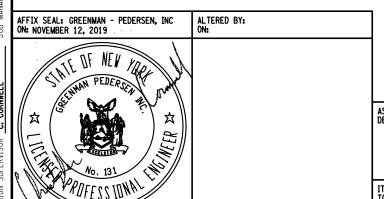


POLE FOUNDATION DETAIL
NOT TO SCALE



TYPE A - LIGHT POLE DETAIL DOUBLE ARM ASSEMBLY

NOT TO SCALE



PIN 8780.49
RTE 32

BRIDGES CULVERTS

ARM LENGTH = 6'-0"

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

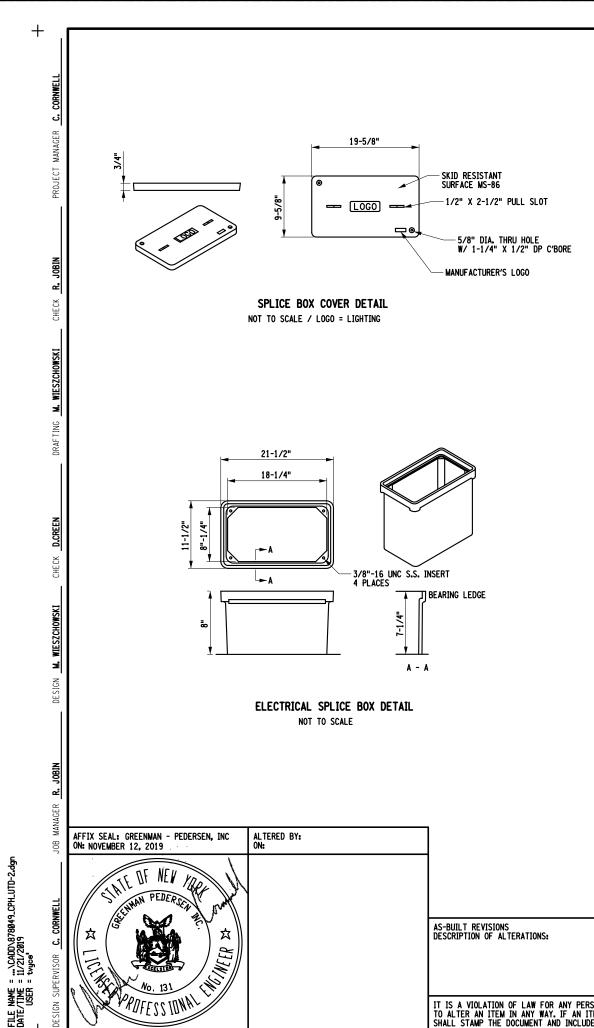
CONTRACT NUMBER
DL34880

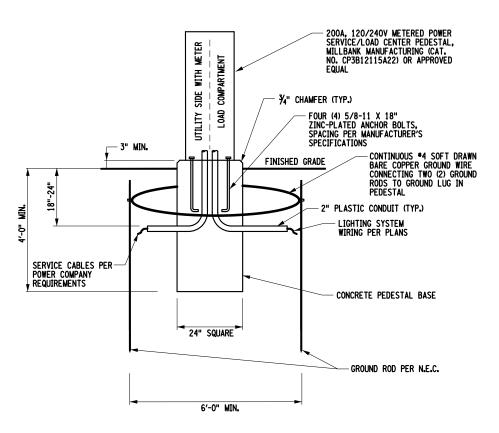
DRAWING NO. UTD-1 SHEET NO. 80

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

GREENMAN-PEDERSEN, INC.
CONSULTING ENGINEERS

LIGHTING DETAILS





POWER SERVICE DETAIL NOT TO SCALE

POWER SERVICE NOTES:

- ALL COSTS ASSOCIATED WITH THE POWER SERVICE, TO INCLUDING CABINET, FOUNDATION, GROUNDING SYSTEM AND ANCILLARY EQUIPMENT SHALL BE PAID FOR UNDER ITEM 670.5307XX04, EXCEPT FOR THE ELECTRIC METER SOCKET (IF REQUIRED), WHICH WILL BE PAID FOR SEPARATELY UNDER ITEM 680.90920008.
- 2. EACH POWER SERVICE SHALL INCLUDE AN 8 CIRCUIT LOADCENTER WITH CIRCUIT BREAKERS AS SHOWN
- INSTALLATION SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS LISTED IN THE CONTRACT DOCUMENTS, THE NEC, AND ALL APPLICABLE CODES AND CENTRAL HUDSON GAS & ELECTRIC CORP. REQUIREMENTS.
- INSTALLATION SHALL BE INSPECTED BY CENTRAL HUDSON GAS & ELECTRIC CORP. PRIOR TO SERVICE CONNECTION.

	F							
DESCRIPTION	BREAKER (AMPS/POLES)	NO.	NO. PHASE NO. (BREAKER (AMPS/POLES)	DESCRIPTION		
LIGHT POLES	40/2	1	A	2	20/1	RECEPTACLES **		
LIGHT POLES	4072	3	В	4	20/1	RECEPTACLES **		
	SPACE	5	A	6	SPACE			
	SPACE	7	В	8	SPACE			

- * PANEL BOX SET-UP SHALL BE THE SAME FOR ALL THREE SERVICES; "A", "B" & "C".
- ** RECEPTACLES ASSOCIATES TO EACH CIRCUIT ARE CALLED OUT IN THE LIGHT POLE SCHEDULE.

PIN 8780.49

RTE 32

BRIDGES

CULVERTS

RO	ROADWAY LIGHTING POLE SCHEDULE										
				POLE	C	IRCUITS					
POLE NO.	STATION	OFFSET	SIDE	TYPE	LIGHT	RECEPTACLE					
A-1	B 06+32	29.2'	RT	A	A1	A4					
A-2	B 06+91	21.5'	LT	Α	A1	A2					
A-3	B 07+81	30.1′	RT	A	A1	A2					
A-4	B 08+45	23.2'	LT	A	A1	A4					
A-5	B 09+31	31.1'	RT	A	A1	A4					
A-6	B 09+93	26.9'	LT	A	A1	A2					
A-7	B 10+83	34.4'	RT	A	A1	A2					
A-8	B 11+36	30.4'	LT	A	A1	A4					
A-9	B 12+40	33.2′	RT	A	A1	A4					
A-10	B 12+70	32.4'	LT	A	A1	A2					
A-11	B 13+77	33.6′	RT	A	A1	A2					
A-12	B 14+17	32.8′	LT	A	A1	A4					
A-13	B 15+09	33.1′	RT	A	A1	A4					
A-14	B 15+72	32.4'	LT	A	A1	A2					
A-15	B 16+59	32.5′	RT	A	A1	A2					
A-16	B 17+11	32.4'	LT	A	A1	A4					
A-17	B 18+14	32.4'	RT	A	A1	A4					
A-18	B 18+20	32.2'	LT	A	A1	A2					
A-19	B 19+11	33.5′	RT	A	A1	A2					
A-20	B 19+42	32.4'	LT	A	A1	A4					
B-1	B 20+51	32.7'	RT	A	B1	B2					
B-2	B 20+39	32.6'	LT	A	B1	B4					
B-3	B 21+69	33.0′	RT	A	B1	B4					
B-4	B 21+88	32.2'	LT	A	B1	B2					
B-5	B 22+98	31.4'	RT	A	B1	B2					
B-6	B 22+97	32.0'	LT	A	B1	B4					
B-7	B 24+06	31.5'	RT	A	B1	B4					
B-8	B 24+06	31.9'	LT	A	B1	B2					
B-9	B 25+35	32.4'	RT	A	B1	B2					
B-10	B 25+41	32.3′	LT	A	B1	B4					
B-11	B 26+65	32.2'	RT	Ā	B1	B4					
B-12	B 26+72	31.8'	LT	Ā	B1	B2					
B-13	B 28+11	28.1'	RT	Ā	B1	B2					
B-14	B 28+07	28.9'	LT	Ā	B1	B4					
B-15	B 29+45	28.6'	RT	Ā	B1	B4					
B-16	B 29+25	29.4'	LŤ	A	B1	B2					
B-16	B 30+83	27.8	RT		B1	B2					
B-18	B 30+83	29.1'	LT	A	B1	B4					
C-1	B 33+09	27.8′	RT	A	C1	C4					
C-2	B 32+94		-		C1	C2					
C-3	B 34+61	29.1′ 27.9′	LT PT	A	C1	C2					
C-4	B 34+18	29.5'	RT	A	C1	C4					
C-5		27.7'	LT RT	A	C1	C4					
C-6	B 36+15										
	B 35+75	28.7′	LT	A	C1	C2					
C-7	B 37+68	27.8′	RT	A	C1	C2					

	CITY OF KINGSTON				
	COUNTY: ULSTER	REGION: 8	1		
IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING U	INDER THE DIRECTION OF A LICENSED PROFESSIONAL	ENGINEER. ARCH	HITECT, LANDSCAPE	ARCHITECT.	OR LAND

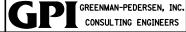
BROADWAY STREETSCAPE PROJECT

KINGSTON CONNECTIVITY

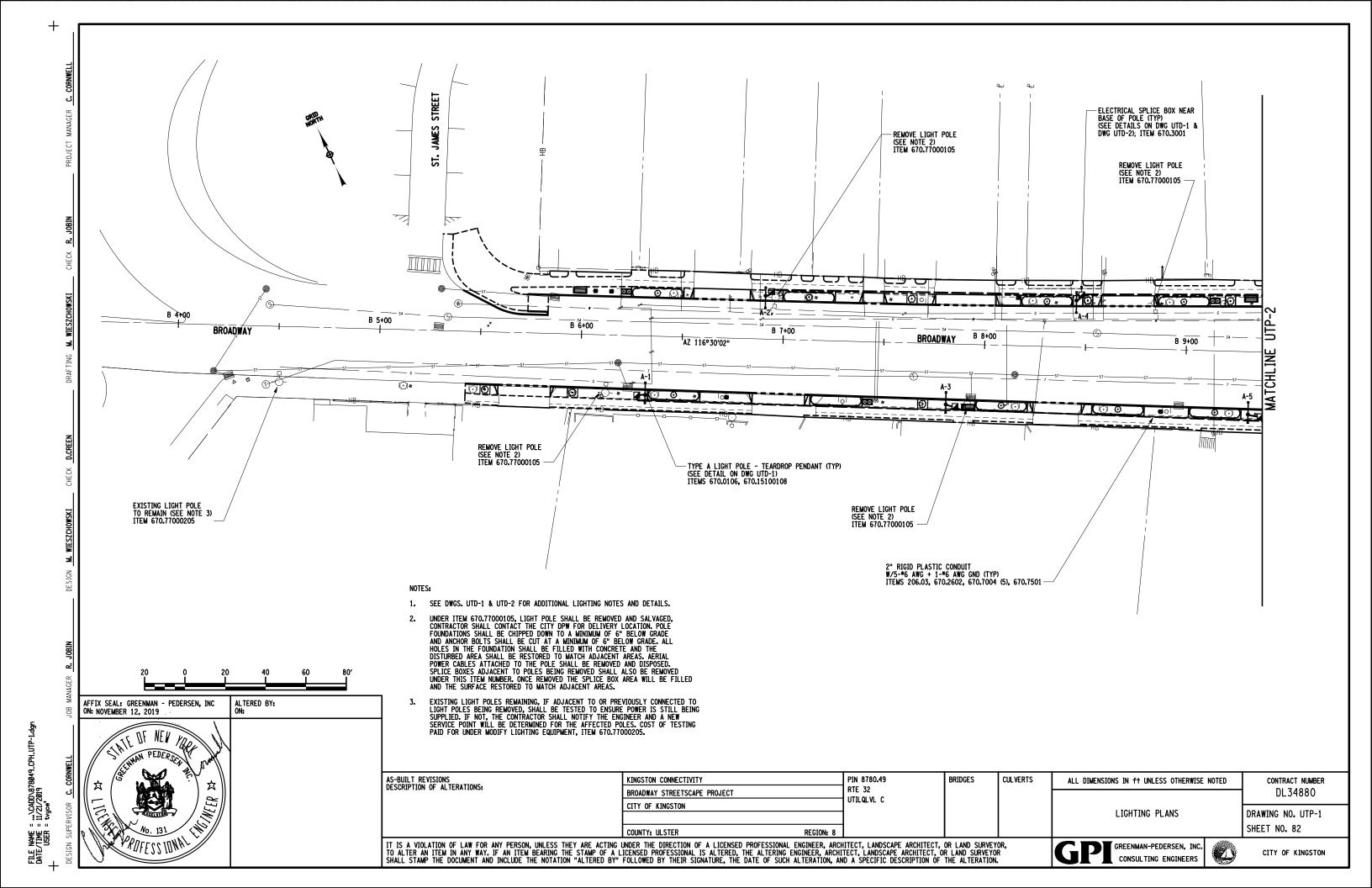
ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED LIGHTING DETAILS

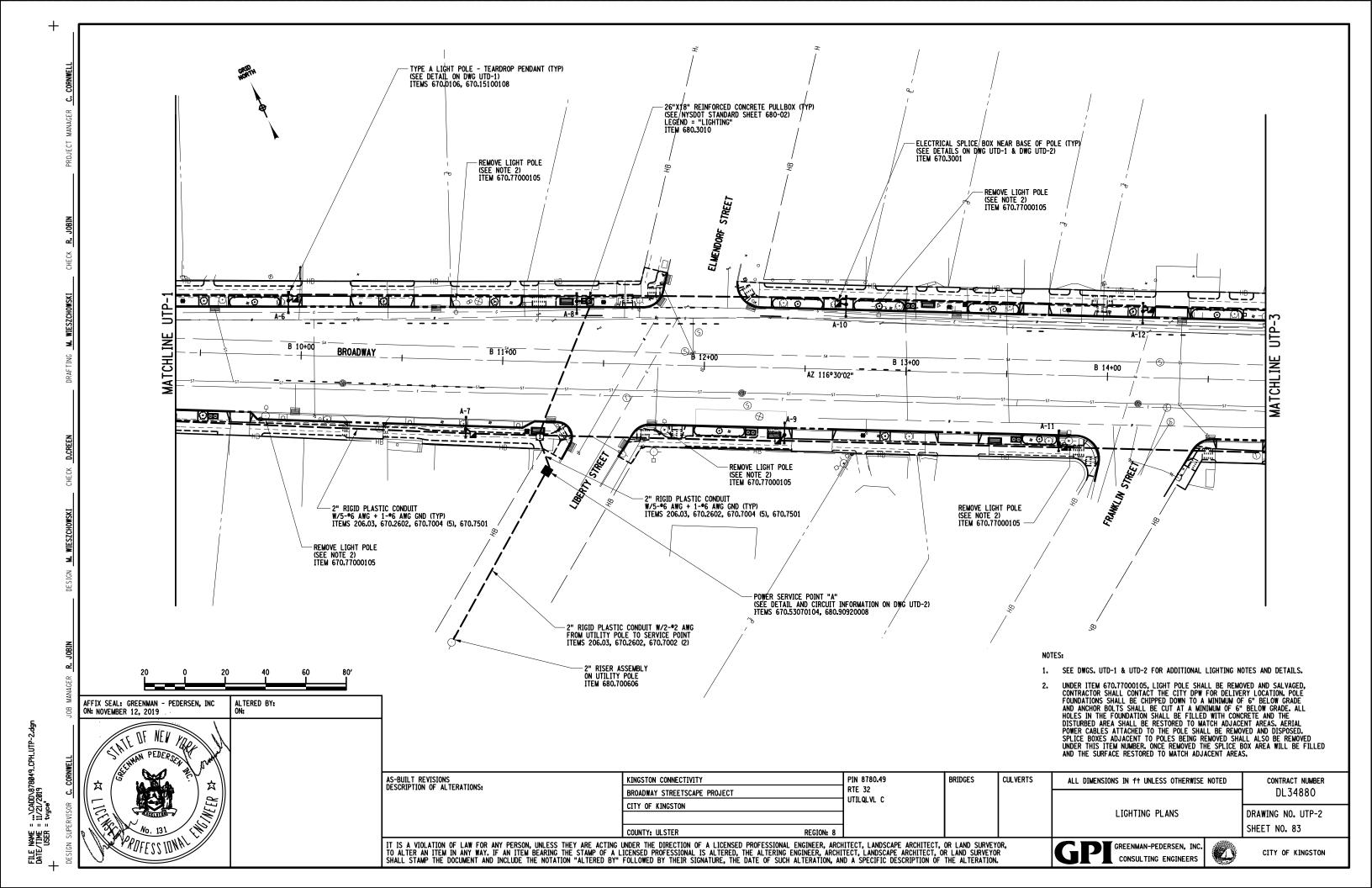
CONTRACT NUMBER DL34880 DRAWING NO. UTD-2

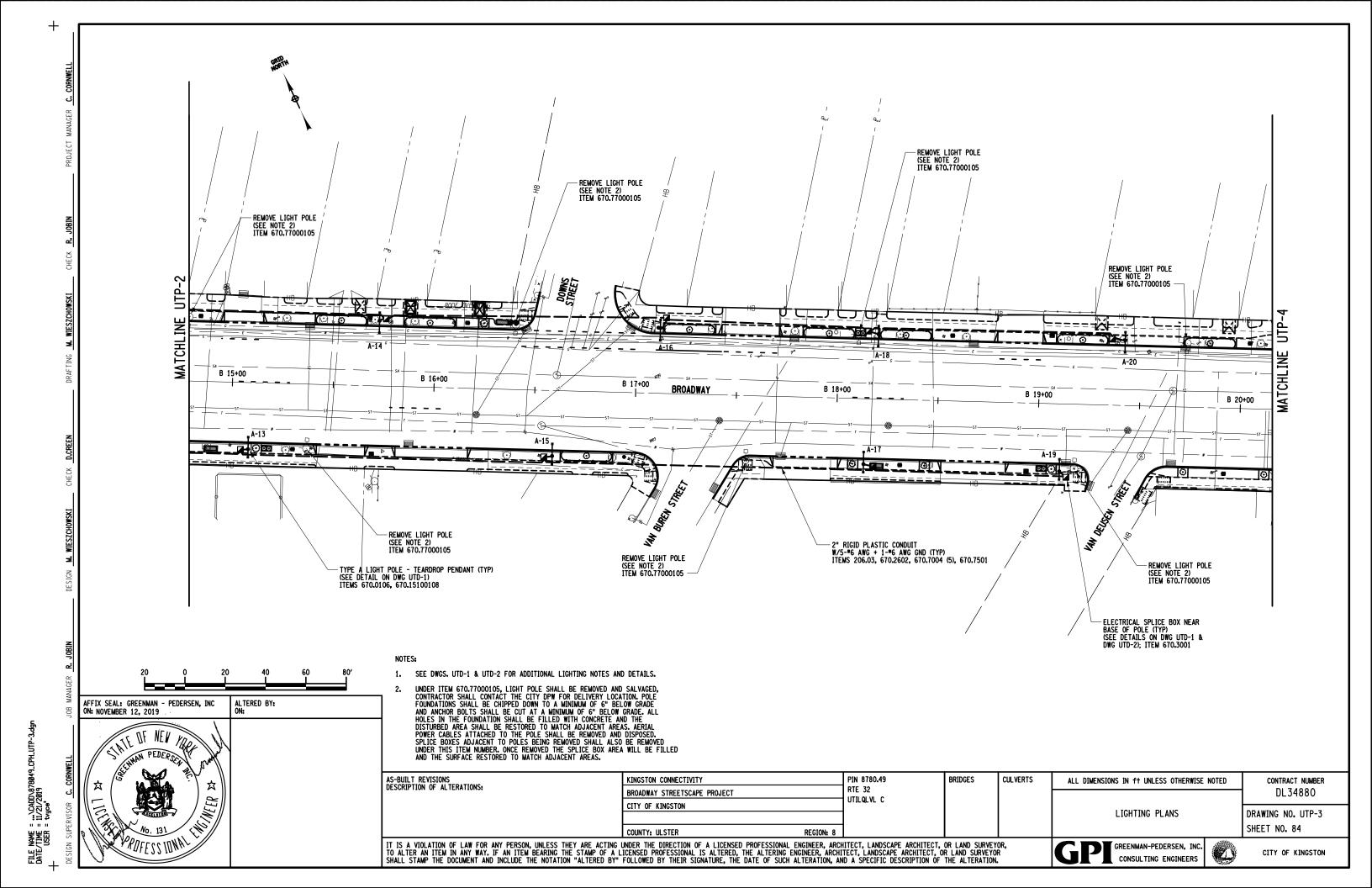
SHEET NO. 81

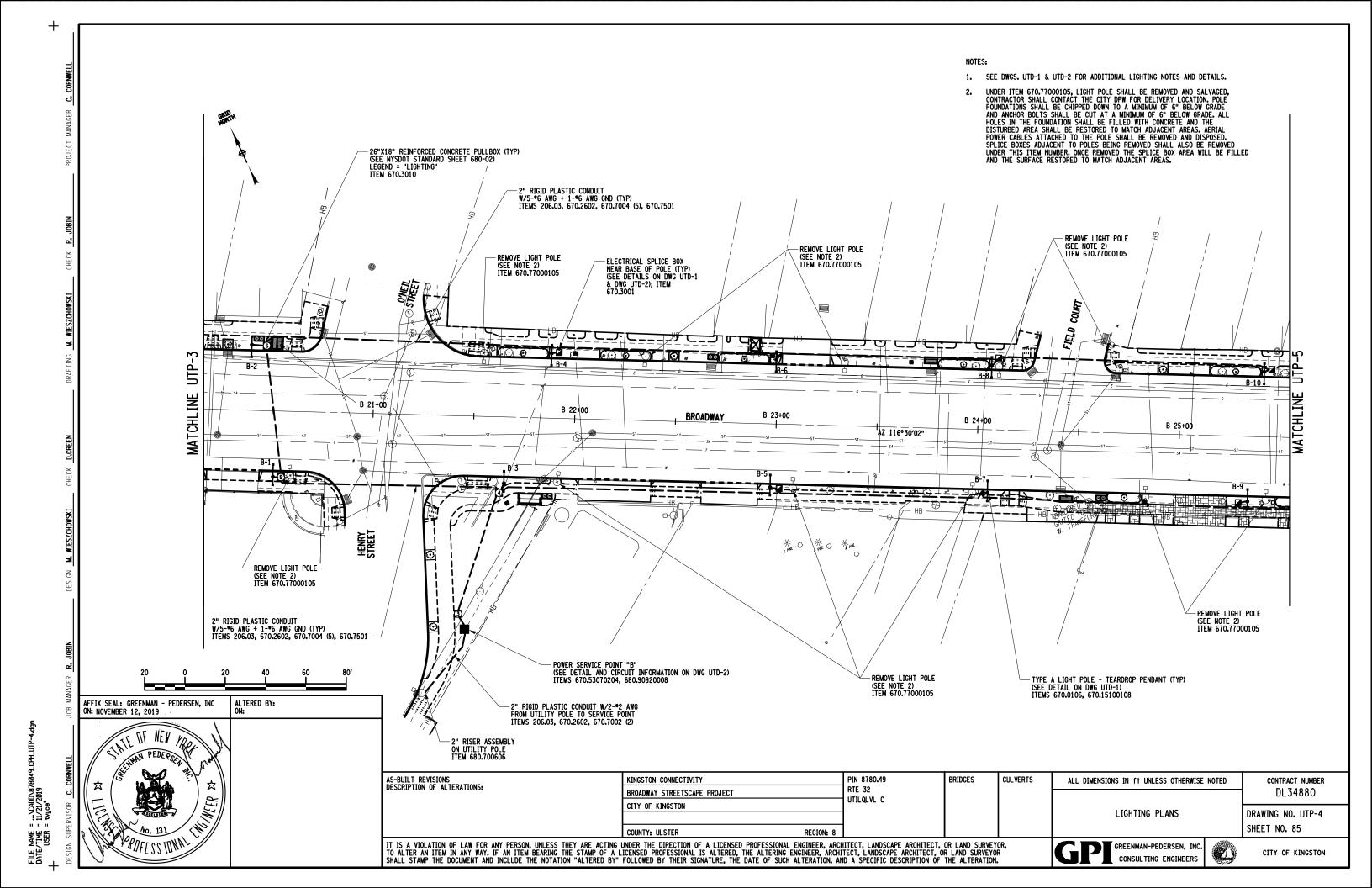


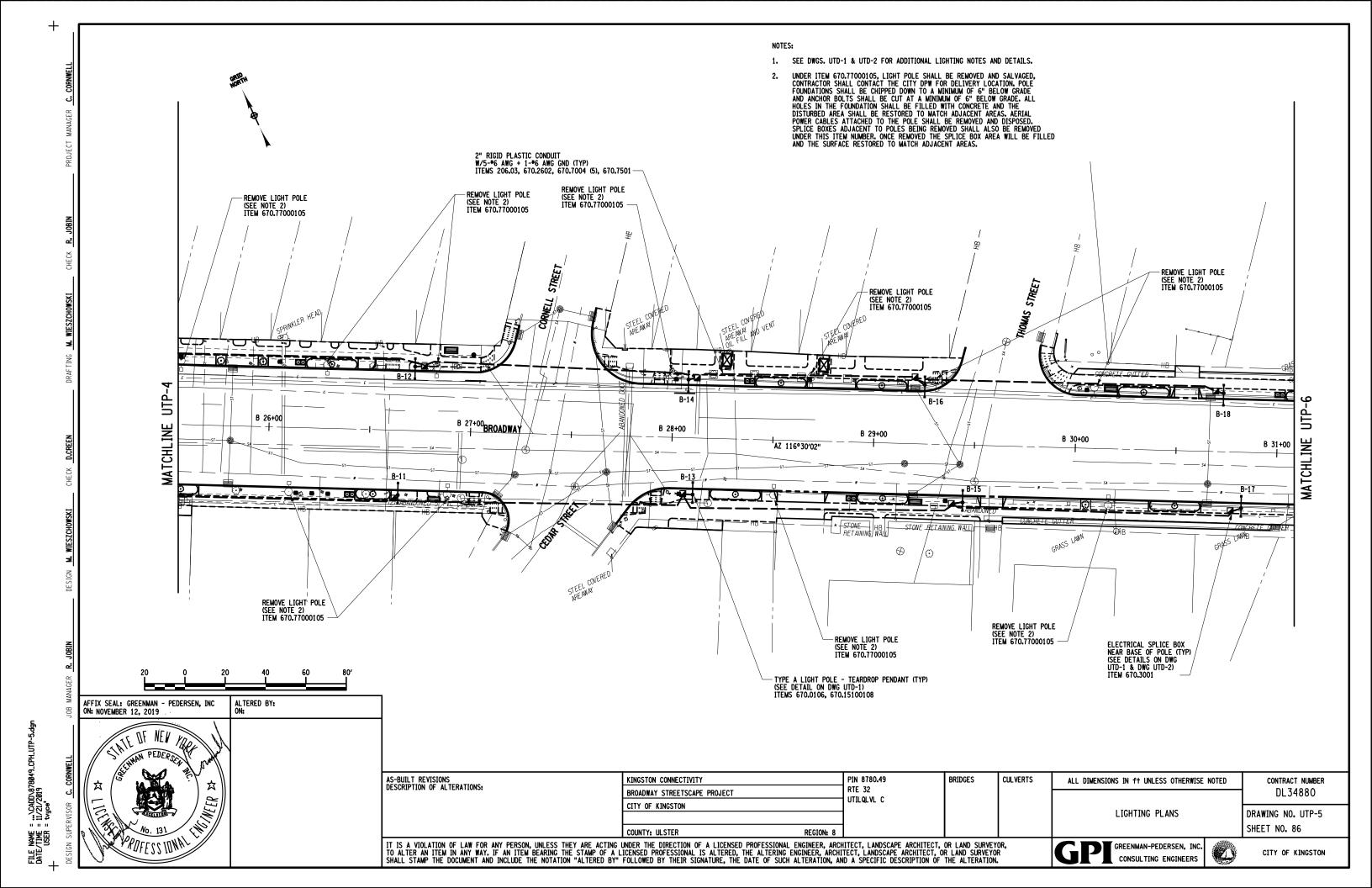


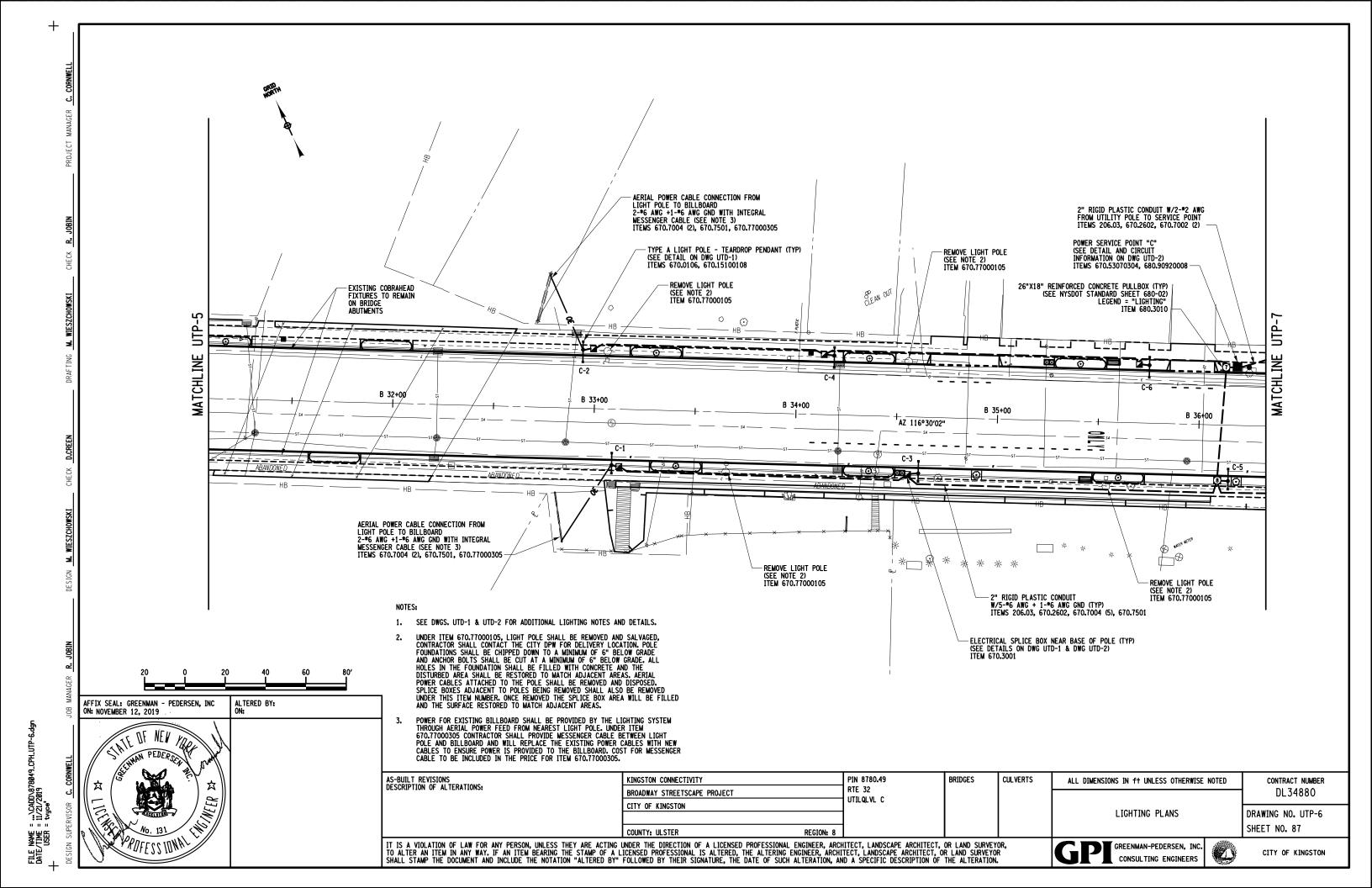


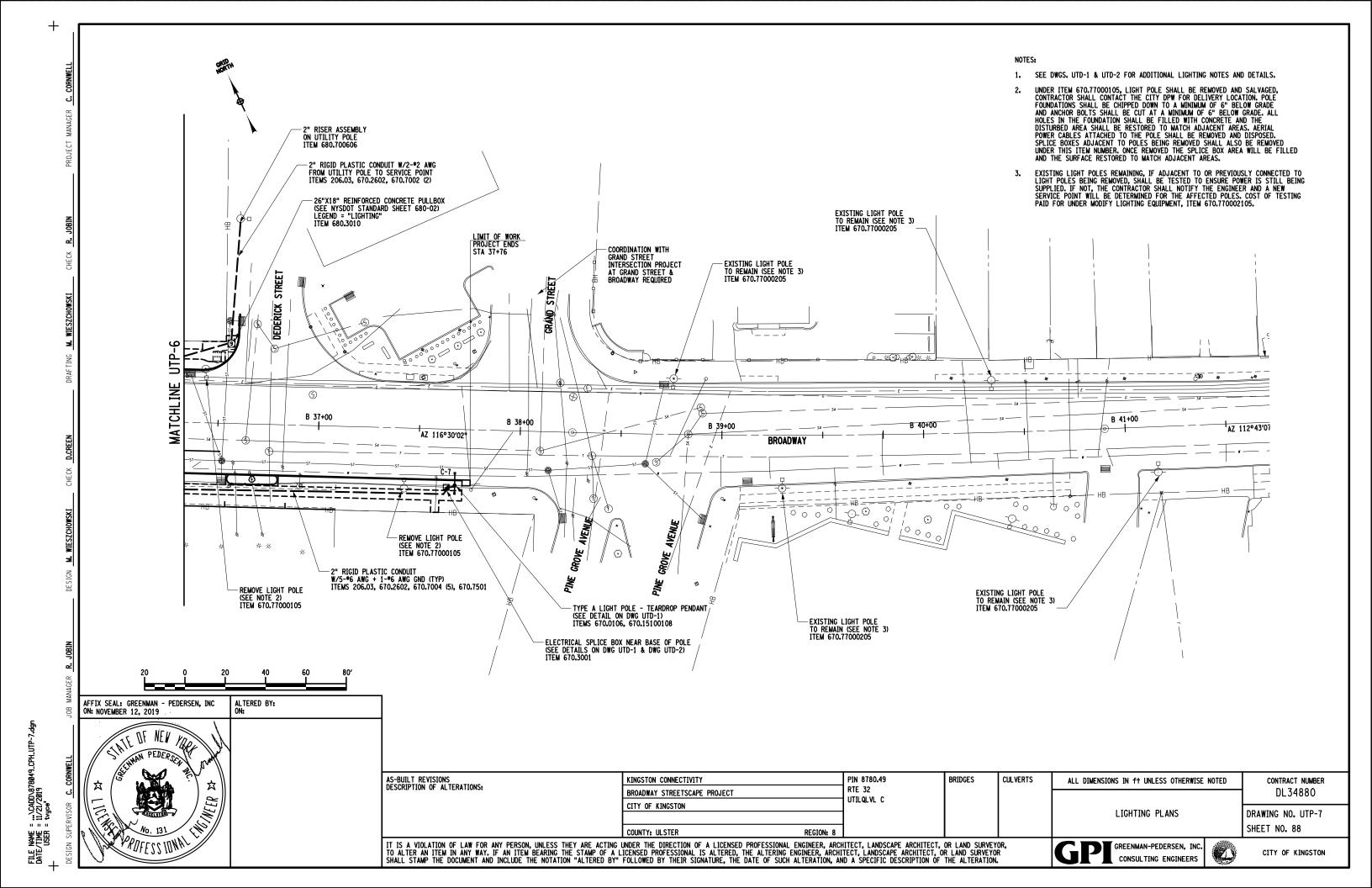


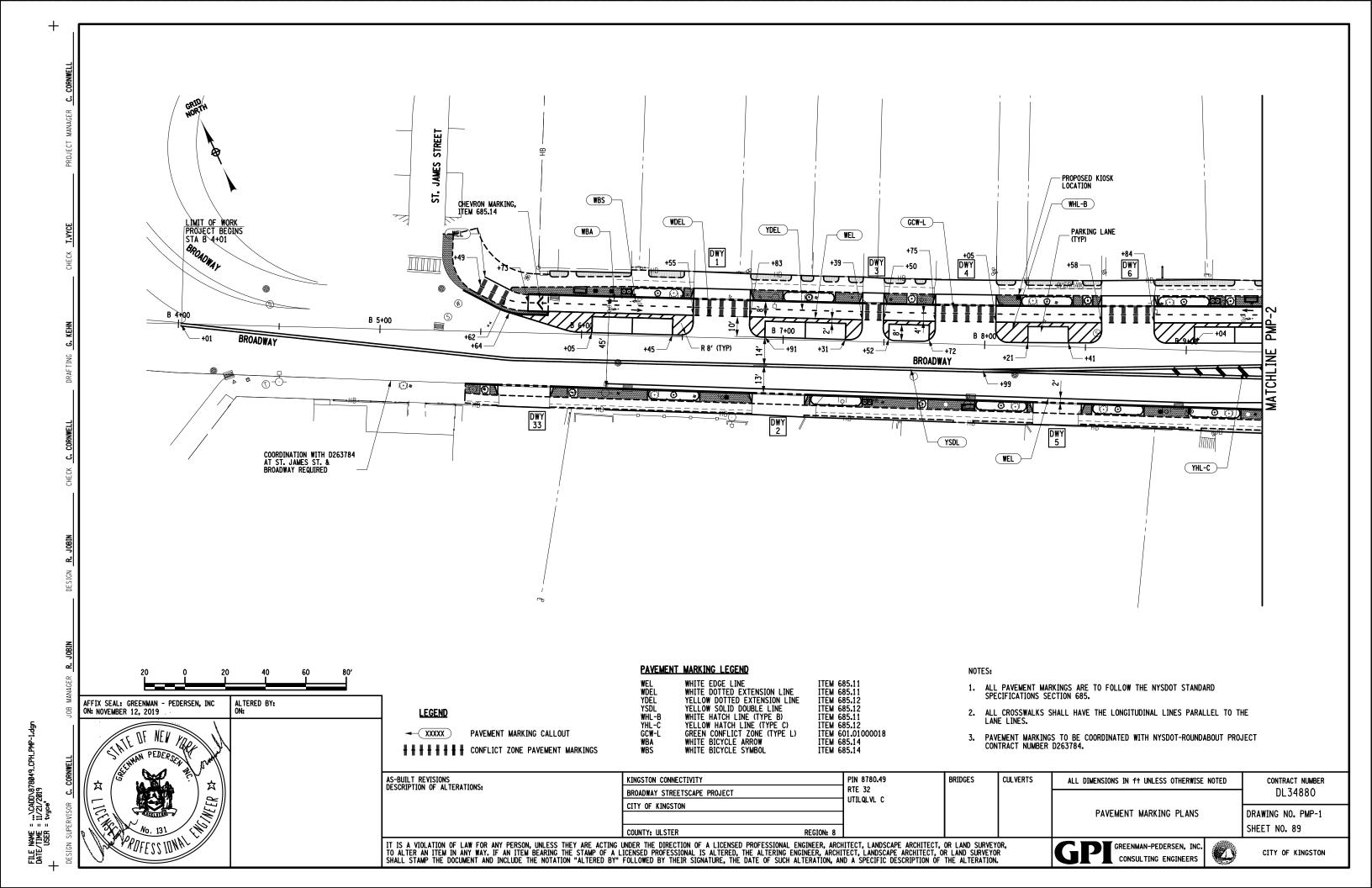


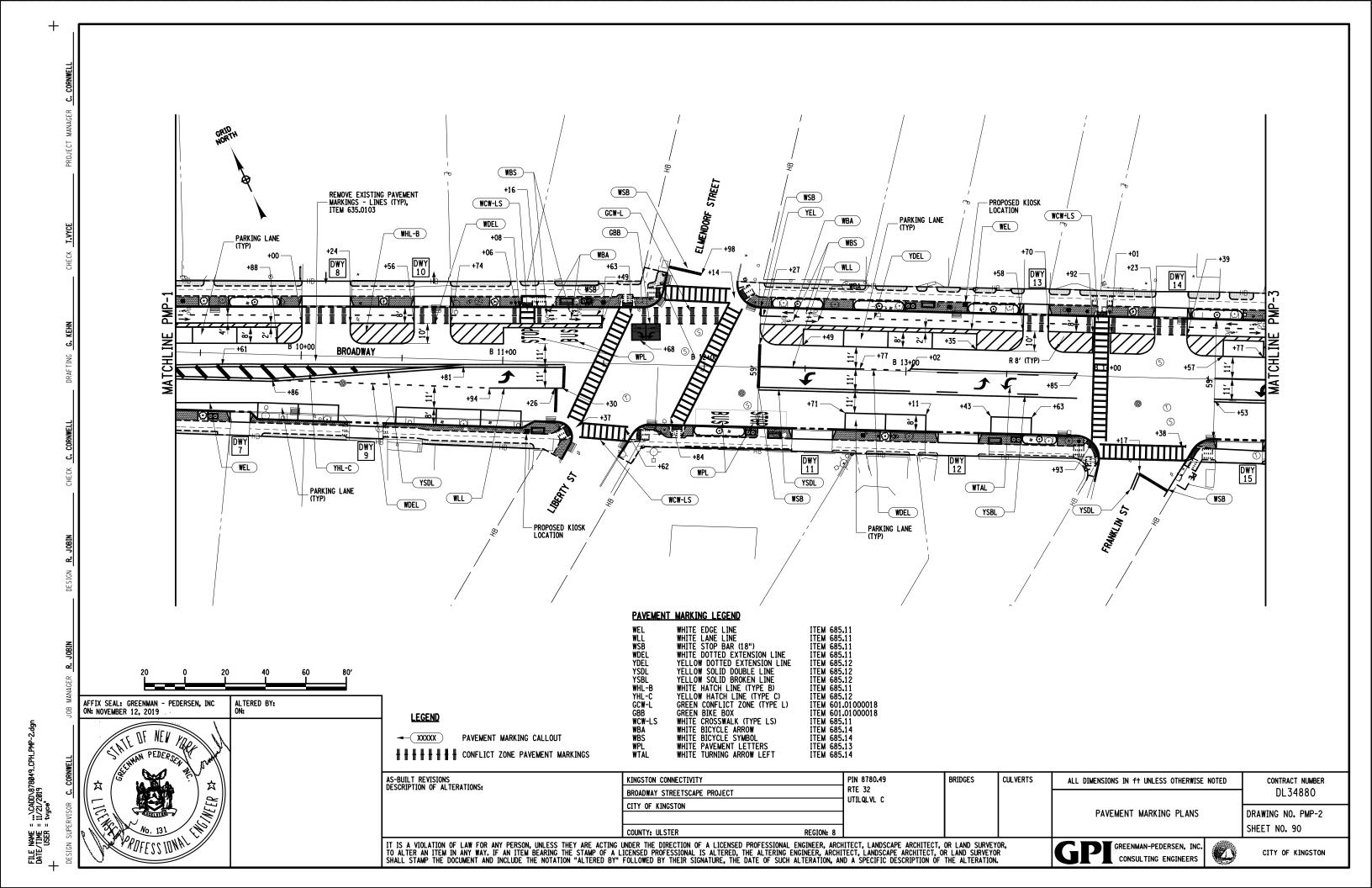


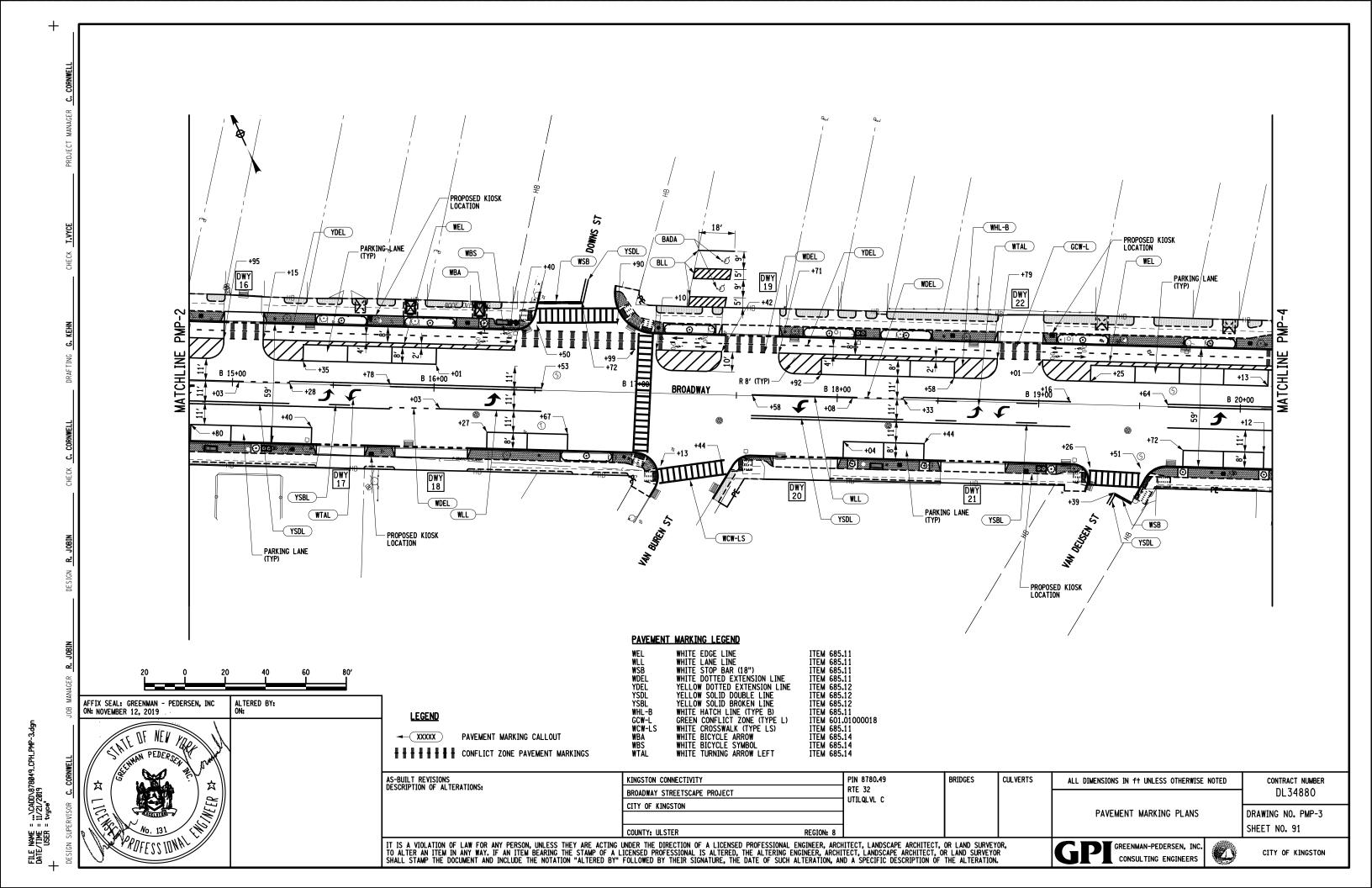


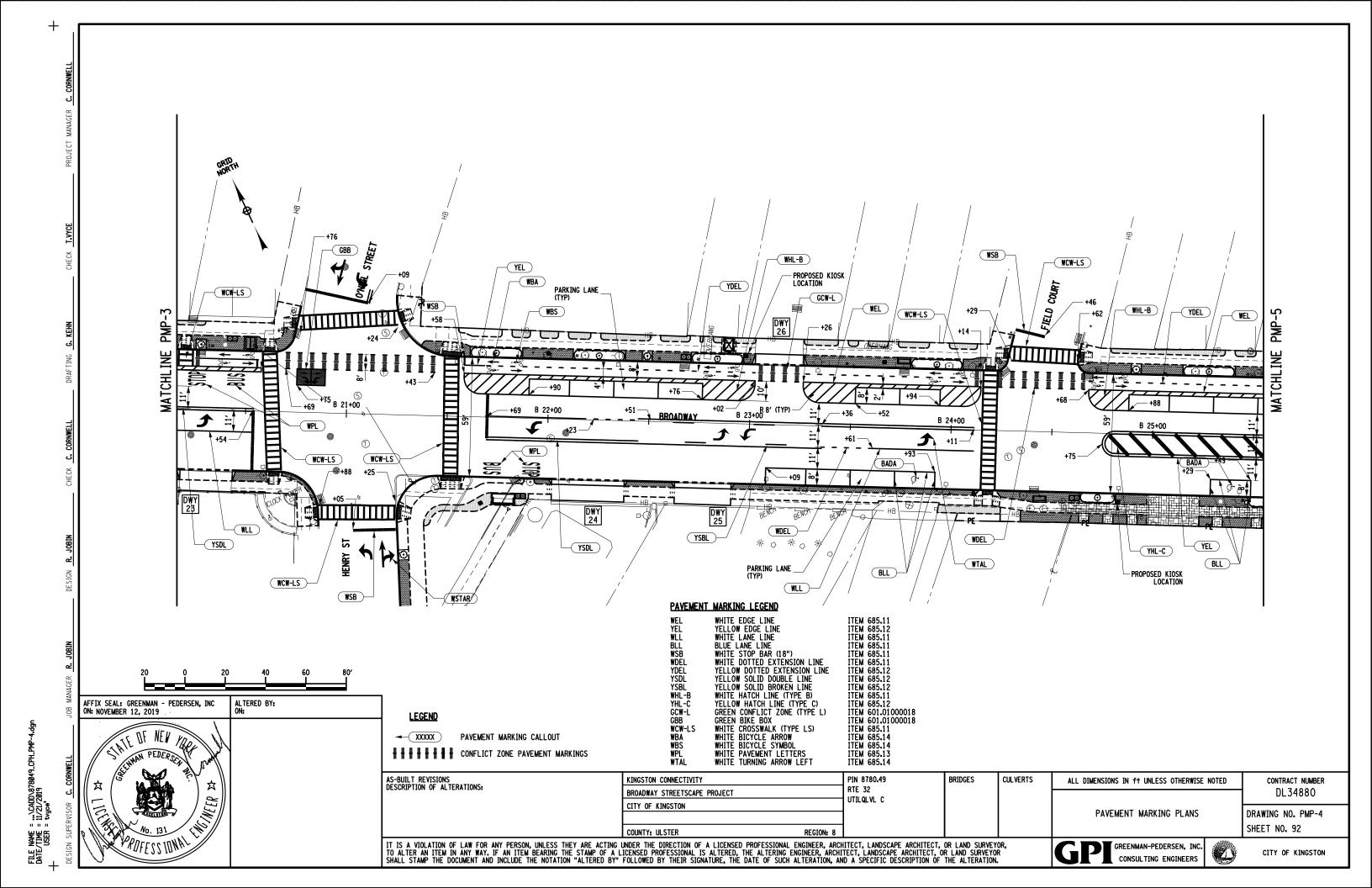


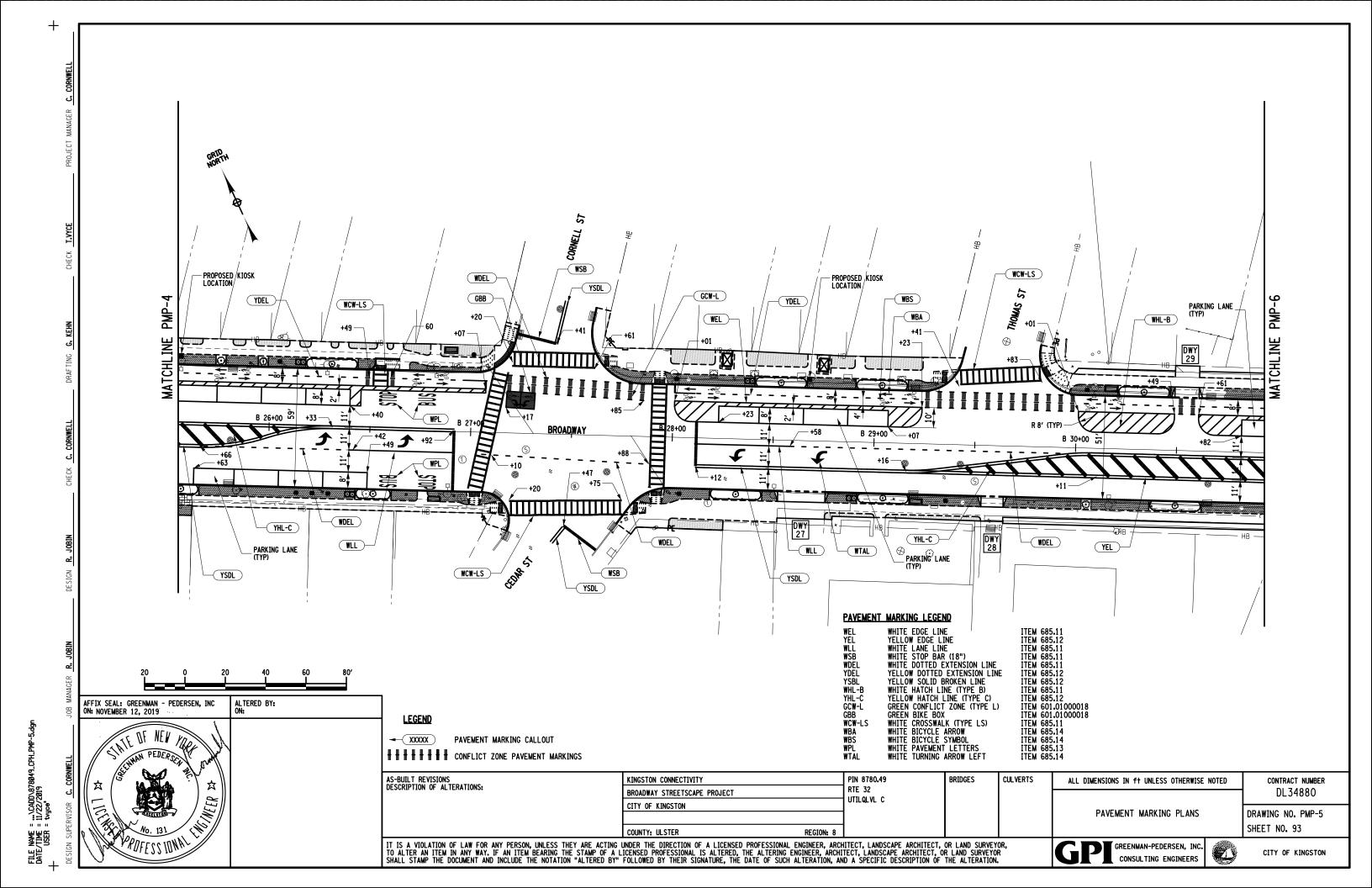


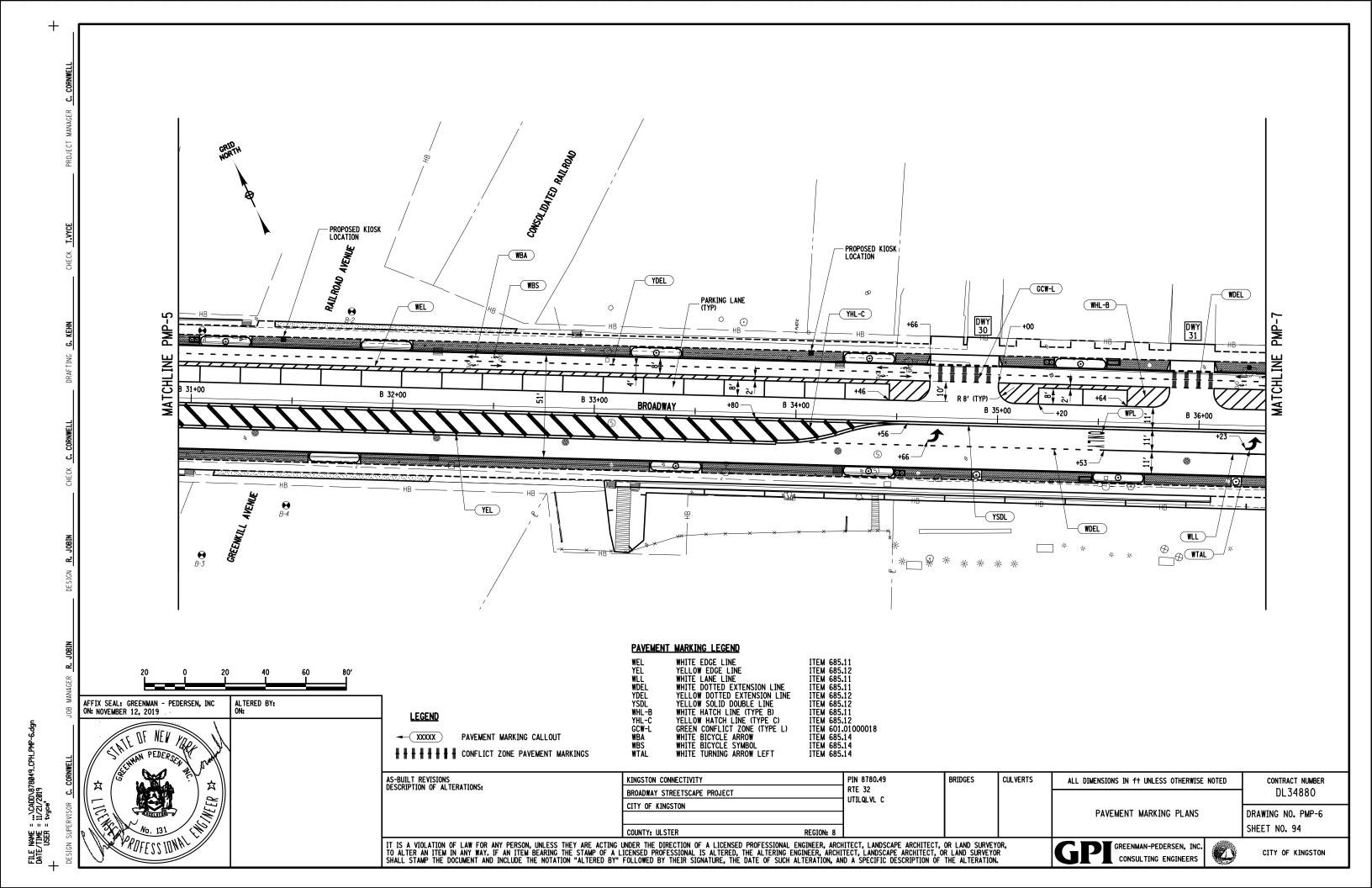


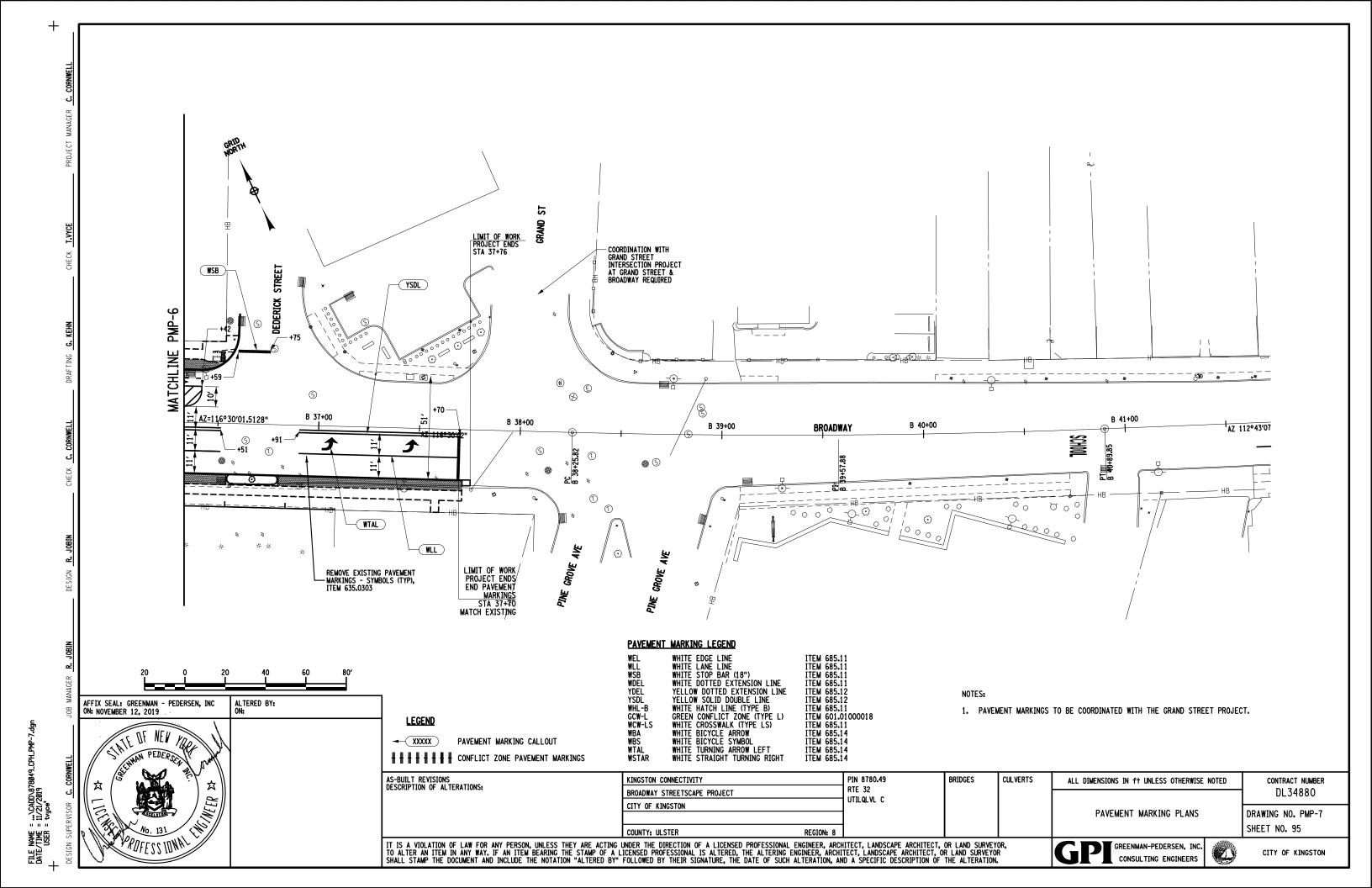


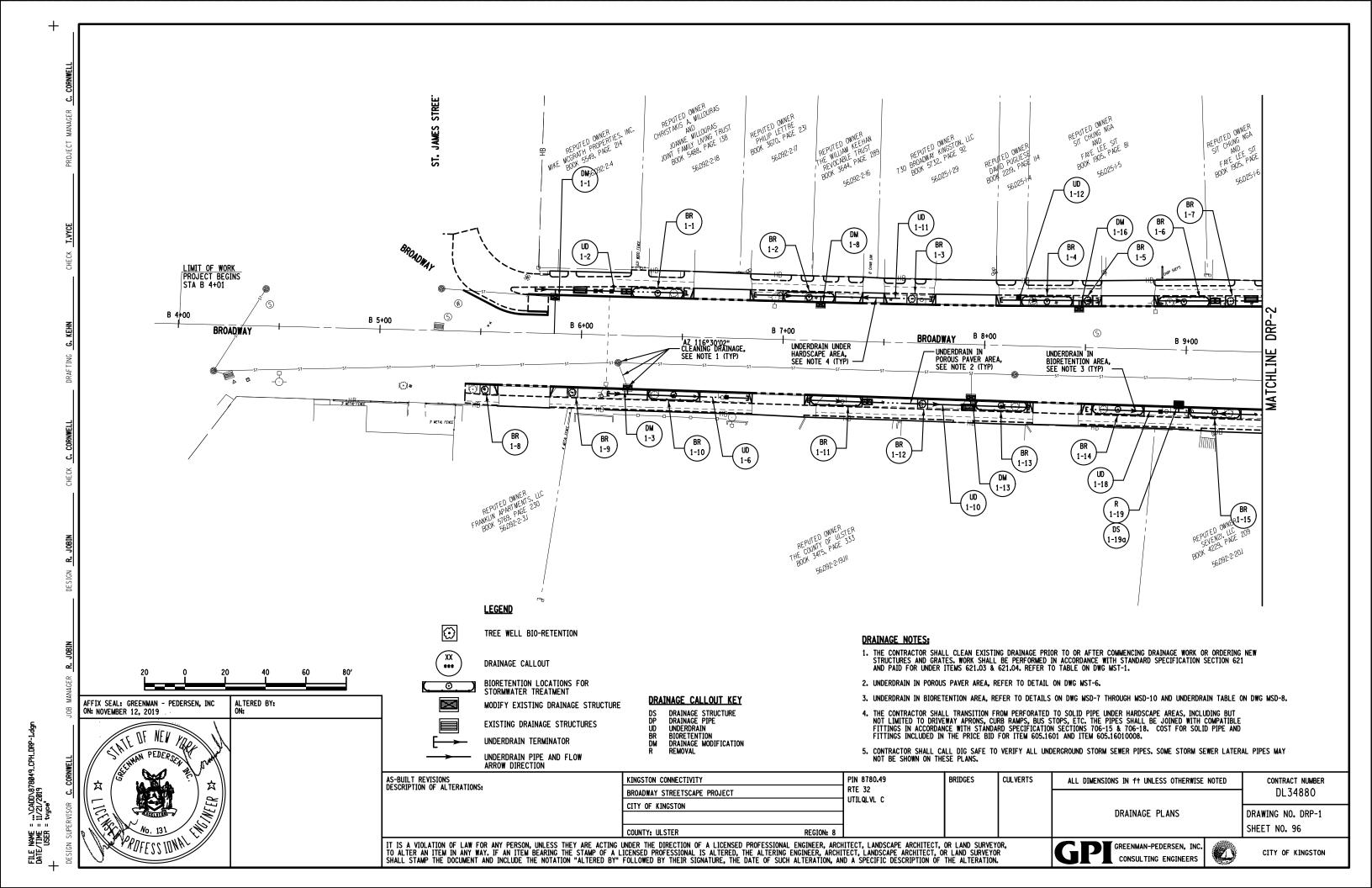


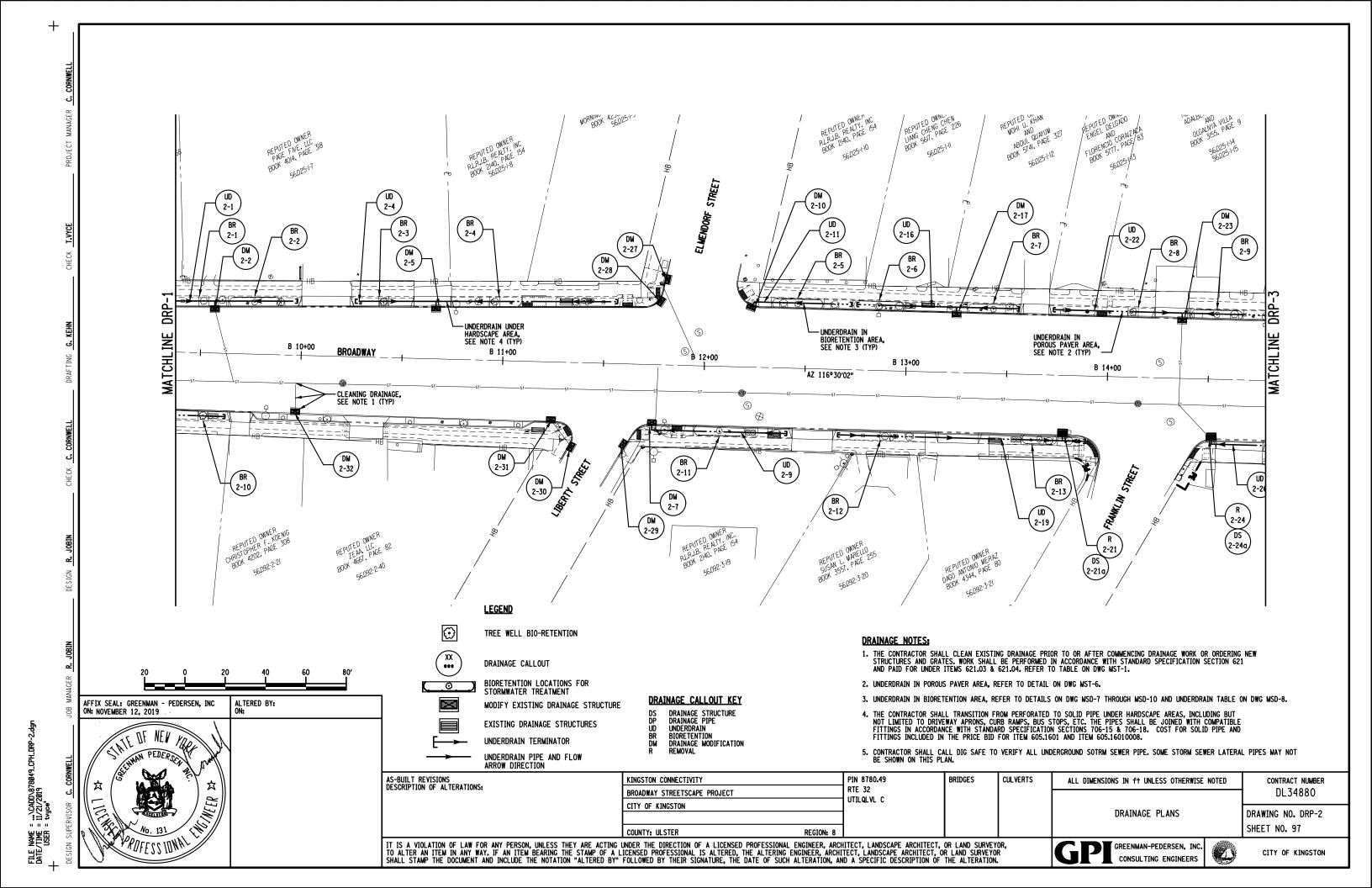


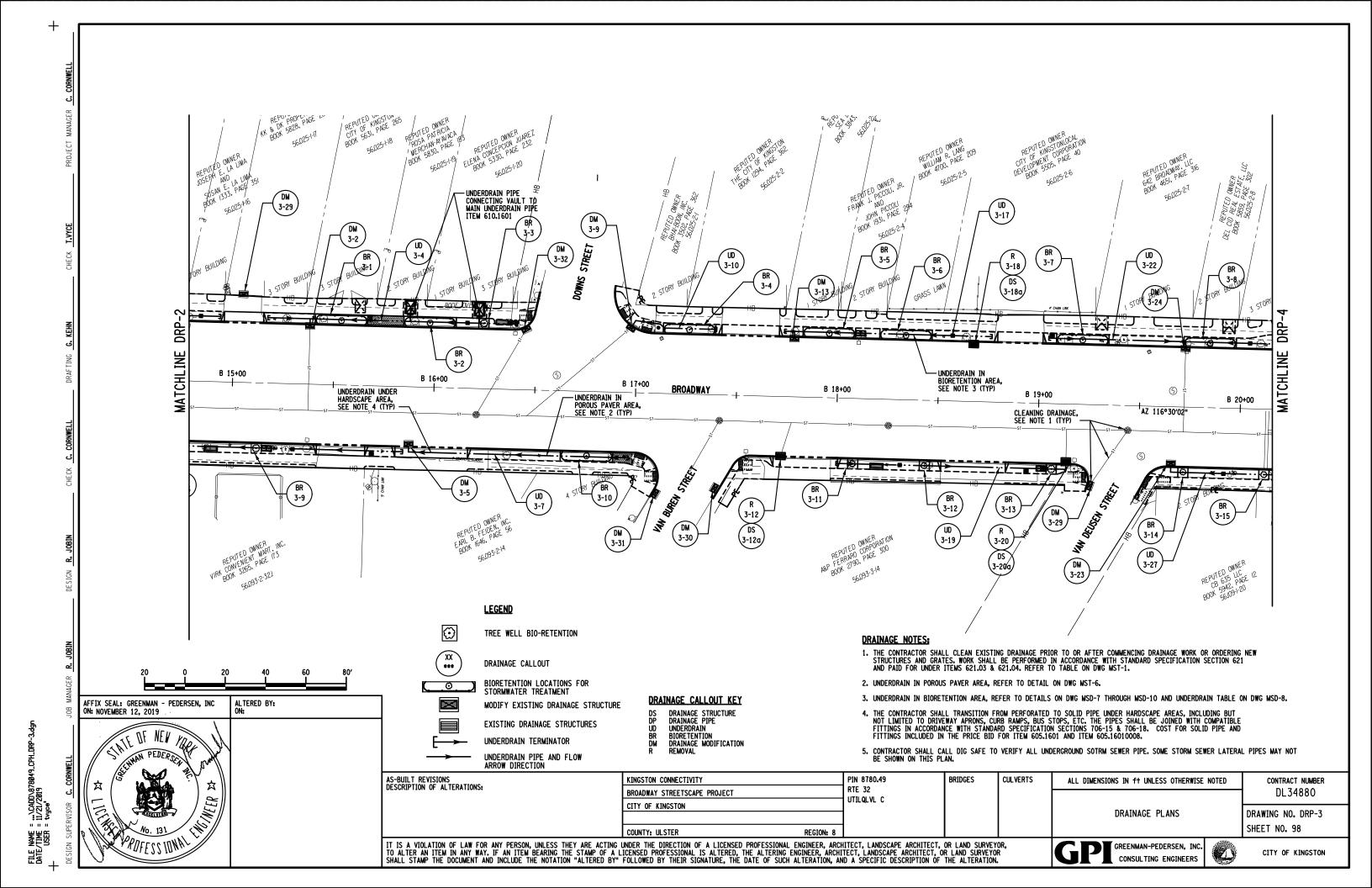


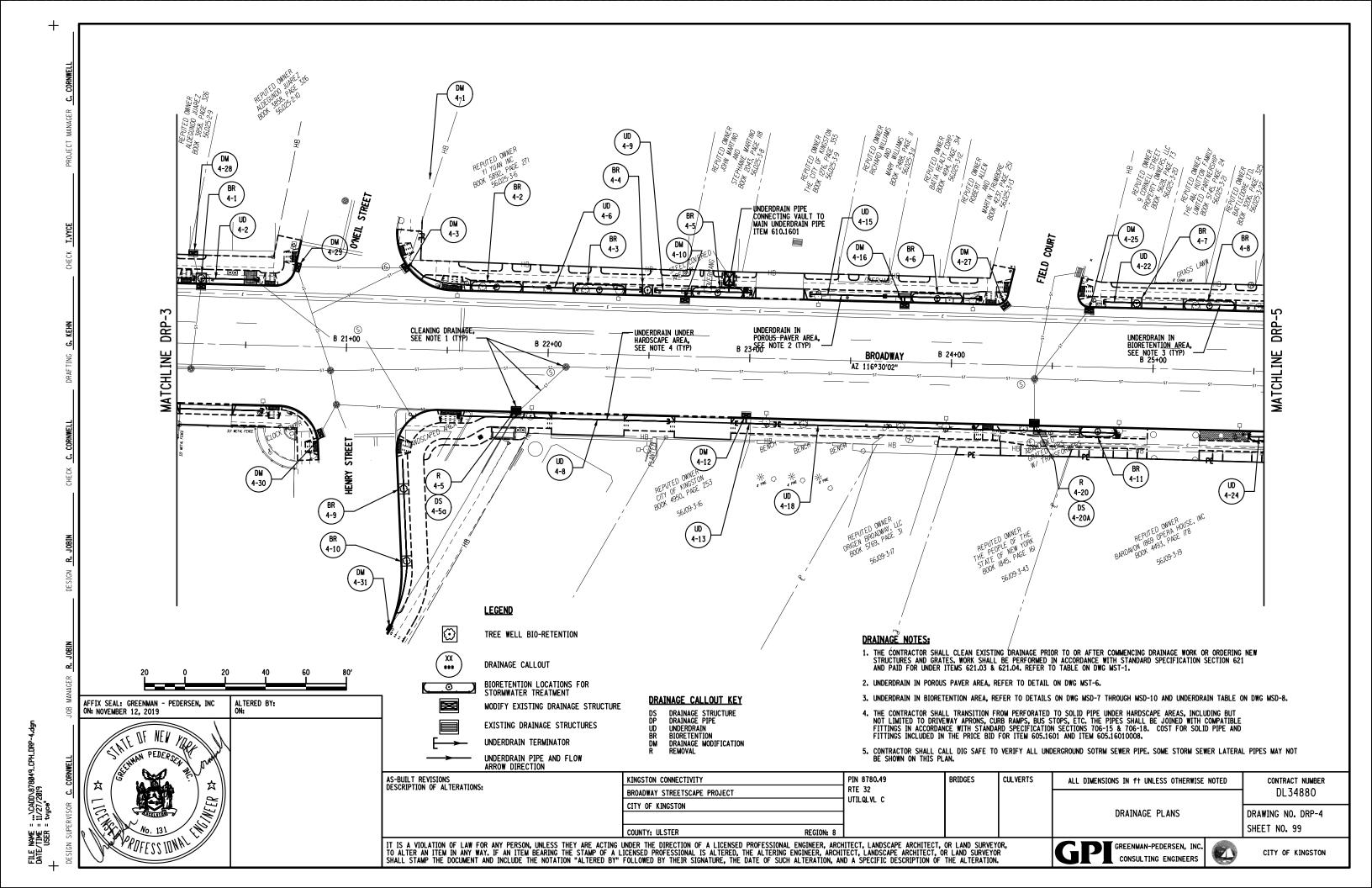


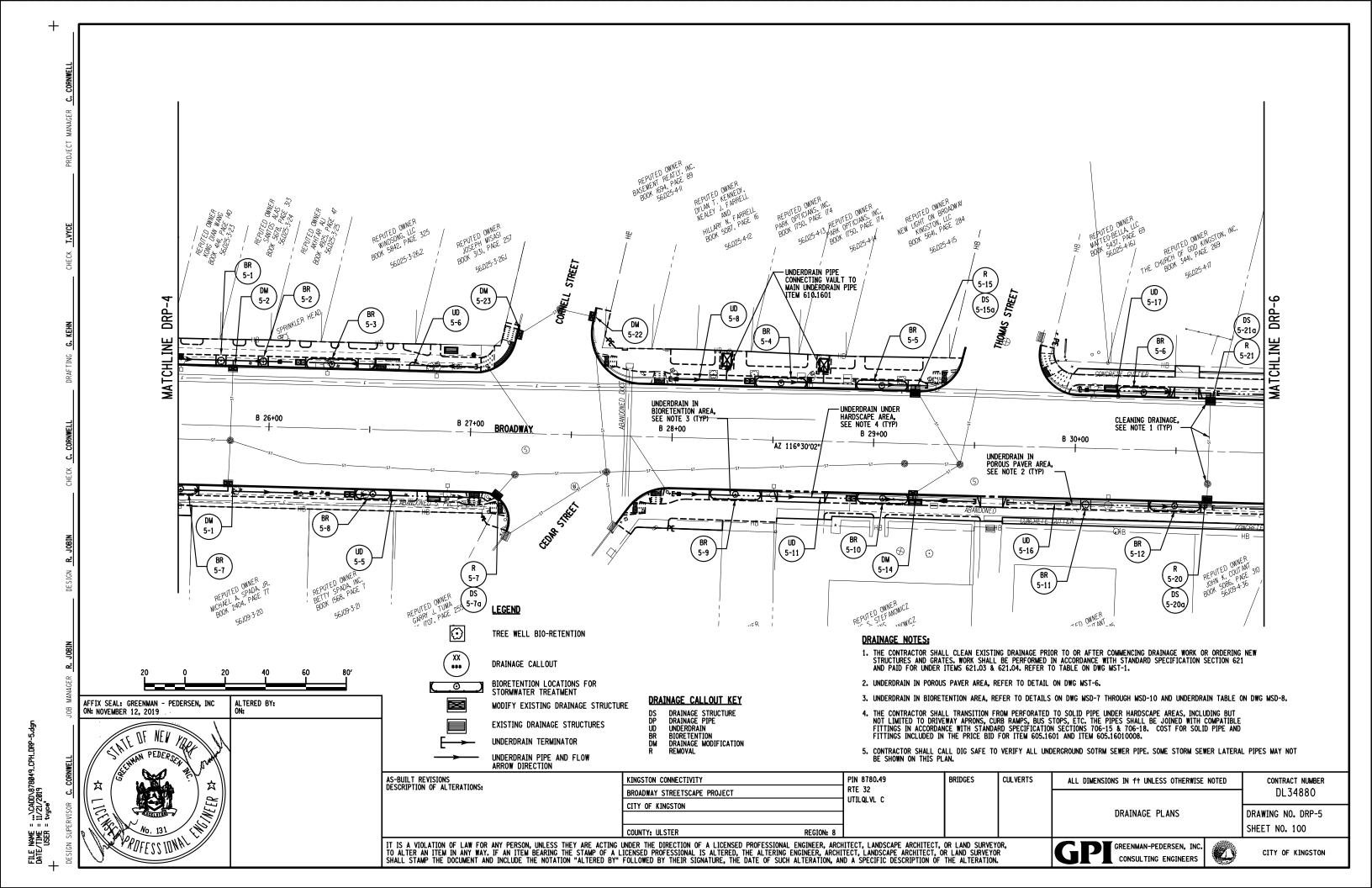


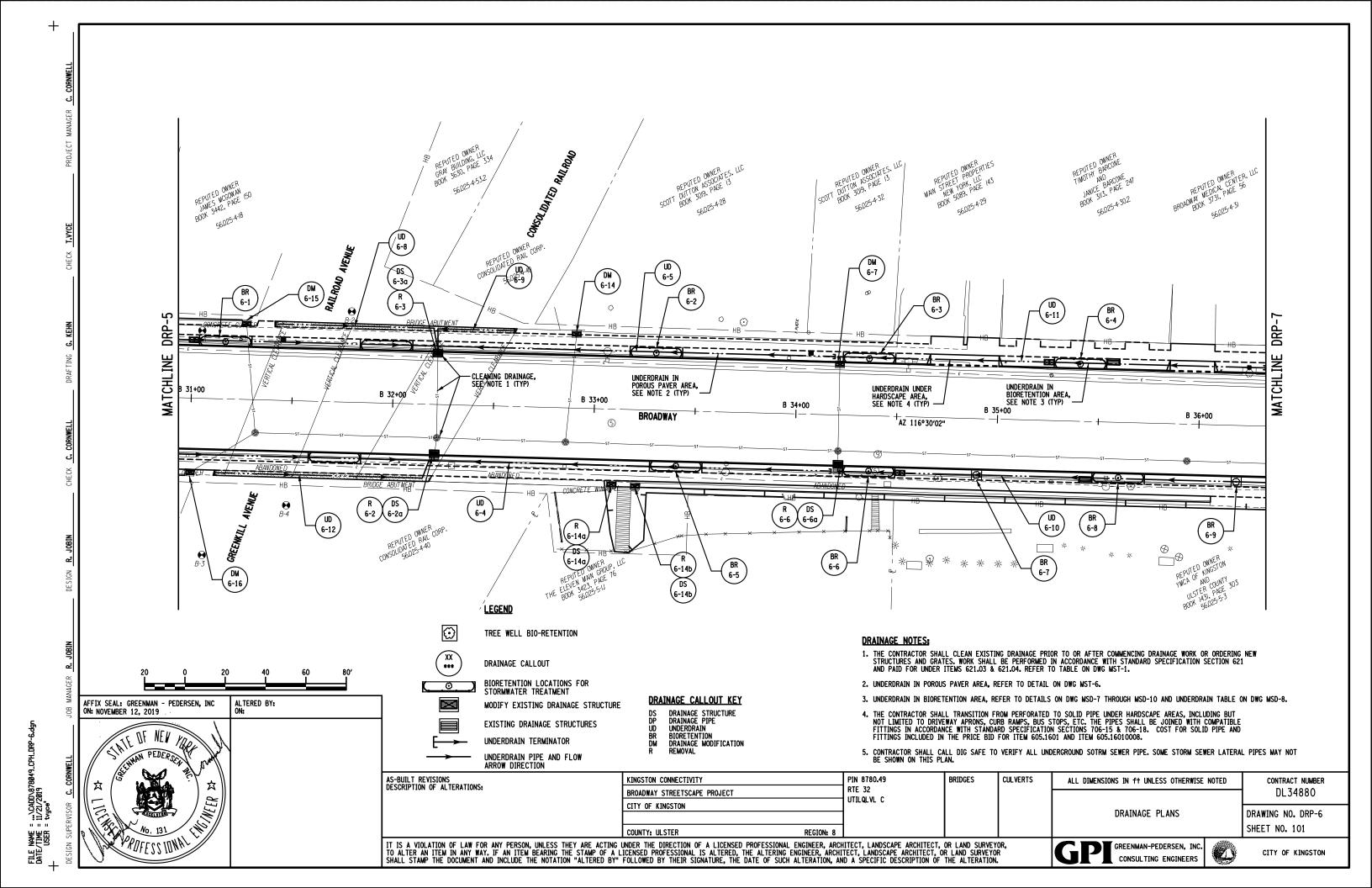


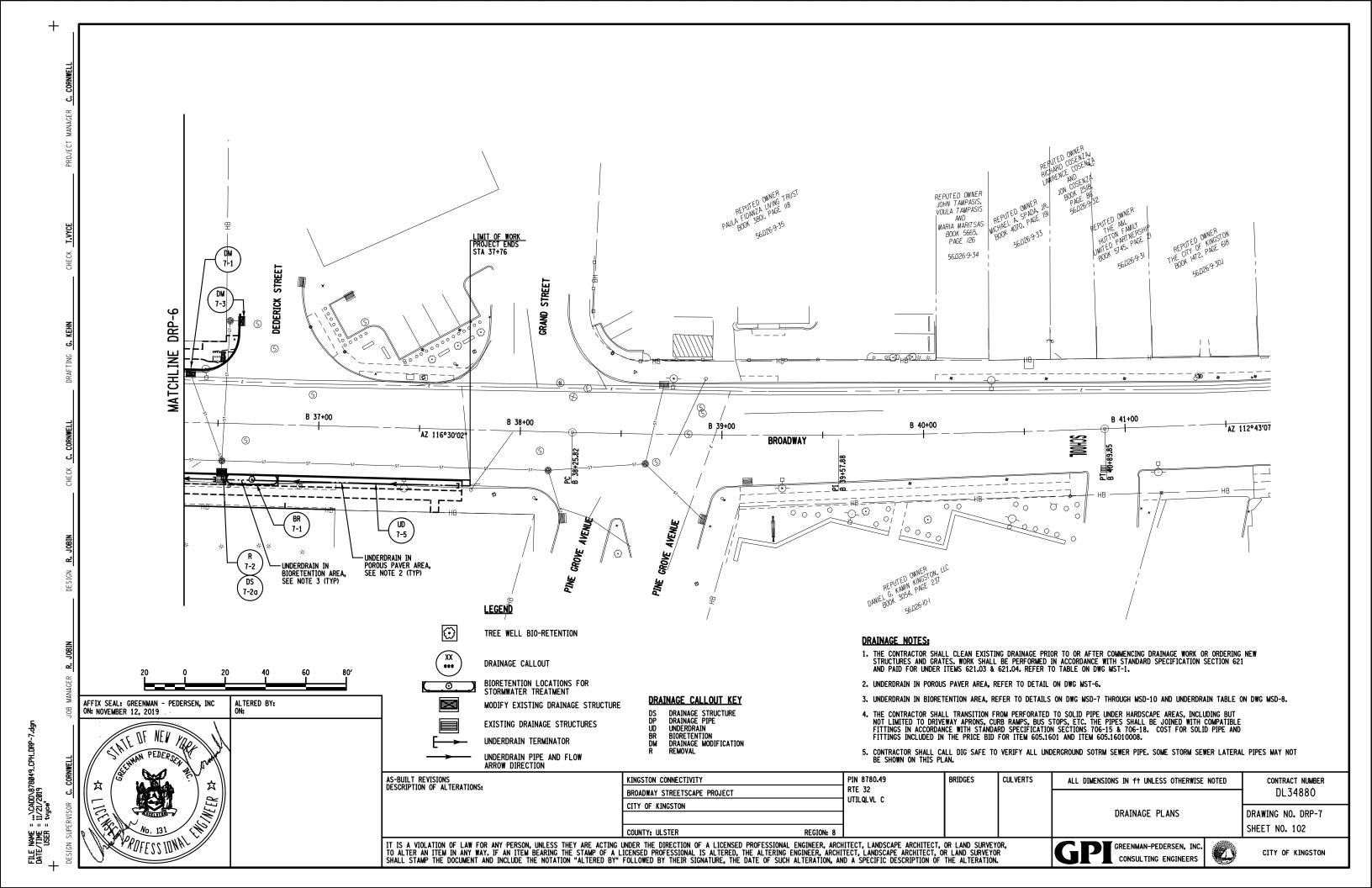












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H_E00-1.dg
CADD\878849_CPH_E00-1.d /22/2019 yoe'
=\CADD\ = 11/22/20 = tvyce'
FILE NAME DATE/TIME USER -
+

ITEM NUMBER

DISPOSAL OF BULDINGS 8H-578

DISPOSAL OF BULDINGS 8H-582

DISPOSAL OF BULDINGS BH-618

DISPOSAL OF BULDINGS BH-640

DISPOSAL OF BUILDINGS 8H-642

DISPOSAL OF BULDINGS 8H-666

DISPOSAL OF BUILDINGS 8H-670

DISPOSAL OF BULDINGS BH-672

REMOVAL OF SUBSTRUCTURES

EM8ANKMENT IN PLACE

SELECT GRANULAR FILL

SELECT STRUCTURAL FILL

TEST PIT EXCAVATION

GEOTEXTILE SEPARATION

SUBBASE COURSE (MODIFIED)

TACK COAT (STRAIGHT)

SHIELDS AND SHORING

POROUS PAVING SURFACE, TYPE NN

UNCLASSIFIED EXCAVATION AND DISPOSAL

LIGHTWEIGHT CONCRETE FILL (TYPE A)

TRENCH AND CULVERT EXCAVATION

BIORETENTION AND DRY SWALE SOIL

CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION

DRAINAGE STRUCTURE INLET PROTECTION, PREFABRICATED-TEMPORARY

ROLLED EROSION CONTROL PRODUCT, CLASS I TYPE C, SHORT TERM

TRUE & LEVELING F9, SUPERPAVE HMA, 70 SERIES COMPACTION

12.5 F2 TOP COURSE HMA, 70 SERIES COMPACTION

25 F9 BINDER COURSE HMA, 80 SERIES COMPACTION

37.5 F9 BASE COURSE HMA, 80 SERIES COMPACTION

PRODUCTION COLD MILLING BITUMINOUS CONCRETE

PCC FOUNDATION FOR PAVEMENT, CLASS C

MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE

202.010001

202.030002

202.010003

202.010004

202.010005

202.010006

202.010007

202.030008

202.19

203.02

203.03

203.07

203.21

206.0201

206.03

206.05

207.21

209.1703

209.1803

304.110000008

402.017903

402.127203

402.258903

402.378903

407.0103

490.10

490.30

503.1010

552.17

420.98010104

208.01030022

555.0105 CONCRETE FOR STRUCTURES, CLASS A ENGINEERING SHARE 1 - ROADWAY AND SIDEWALK IMPROVEMENTS (NYSDOT AND NYSDEC)
ENGINEERING SHARE 2 - GREEN INFRASTRUCTURE (NYSEFC)
ENGINEERING SHARE 3 - BROADWAY TRAFFIC SIGNALIZATION WITHIN PROJECT LIMITS
ENGINEERING SHARE 4 - BROADWAY TRAFFIC SIGNALIZATION (NYSERDA FUNDING)

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

ESTIMATE OF QUANTITIES

UNIT

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620

1,797

2,100

19,295

1,758

101

1,456

19

1,309

689

50

5,461

536

7,600

SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL

ITEM DESCRIPTION

KINGSTON CONNECT **BROADWAY STREETS** CITY OF KINGSTON COUNTY: ULSTER

ESTIMATE QUANTITIES

DRAWING NO. EOQ-1

SHEET NO. 103

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

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7,600

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2,100

1,309

19,295 1,758

101

1,456

19

GREENMAN-PEDERSEN, INC CONSULTING ENGINEERS

CITY OF KINGSTON

AS-BUILT QUANTITY	ITEM NUMBER		ITEM DESCR	PTION			UNIT	SHARE 1	SHARE 2	SHARE 3	SHARE 4	TOTAL	AS-BUILT
	555.09	CONCRETE FOR STRUCT	TURES, CLASS HP				CY	23				23	2-101111
	556.0201	UNCOATED BAR REINF	ORCEMENT FOR CONCRETE STUI	RCTURES			LB	96				96	
·-	601.01000018	SURFACE TREATMENT I	FOR PAVEMENTS TYPE 2, TRAFFI	C GRADE		***************************************	SY	272				272	
	603.77	CONCRETE COLLARS				***************************************	EΑ	41				41	
	603.98040007	POLYVINYL CHLORIDE ((PVC) SEWER PIPE AND FITTINGS	5, 4"		***************************************	l,F		468			468	
	604.070801	ALTERING DRAINAGE S	TRUCTURES, LEACHING BASINS	AND MANHOLES	TOP REBUILD		EΑ	30				30	
	604.070802	ALTERING DRAINAGE S	TRUCTURES, LEACHING BASINS	AND MANHOLES	TOP REBUILD AND	WALL REPAIR	ΕA	20				20	
	604.300611	RECTANGULAR DRAINA	AGE STRUCTURE TYPE F FOR #11	WELDED FRAME		*	LF	54				54	
	604.301411	RECTANGULAR DRAINA	AGE STRUCTURE TYPE N FOR #11	UCTURE TYPE N FOR #11 WELDED FRAME				54		,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,		54	
	605.1001	UNDERDRAIN FILTER, T	TYPE 2	2					1.92		j	192	
	605.1601	PERFORATED POLYVIN	ORATED POLYVINYL CHLORIDE UNDERDRAIN PIPE, 4 INCHDIAMETER						6,294			6,294	
	605.16010008	NON-PERFORATED PV	PERFORATED PVC PIPE, 4" DAIMETER						539			539	
	608.0101	CONCRETE SIDEWALKS AND DRIVEWAYS						721				721	
	608.020102	HOT MIX ASPHALT (HM	F MIX ASPHALT (HMA) SIDEWALKS, DRIVEWAYS, AND BICYCLE PATHS									67	
	608.12	PRECAST CONCRETE BL	OCK PAVED SIDEWALKS AND DE	ŞY	68				68				
	608.13000039	PRECAST PERMEABLE F	AVERS FOR SIDEWALKS, DRIVE	ERS FOR SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS					1,929			1,929	
	608.21000003	CASTIRON EMBEDDED	DETECTABLE WARNING UNITS	TECTABLE WARNING UNITS				51		·		51	
	608.40010008	BLUESTONE SIDEWALK	S (STONE DUST SETTING BED)				ŞF	966				966	
	608.72113108	NON-SECURITY BOLLAR 18 INCHES TO 24 INCHE	RD-FIXED IN GROUND WITHOUT ES	LIGHTING WIDTH	UP TO 3 INCHES H	EIGHT OVER	EA		192			192	
	609.0201	STONE CURB, GRANITE	i, (TYPE A)				ĻF	6,433				6,433	
	609.05	CAST-IN-PLACE CONCE	RETE CURB & GUTTER (AS DETAIL	ED)			l,F		1,798			1,798	-
	609.15	RESETTING EXISTING C	URB				LF	363				363	
	609.15100011	STEEL EDGING					l.F		1,203			1,203	
	610.1101	MULCH FOR PLANTING	TYPE A, B & D - WOOD CHIPS AN	ND SHREDDED BA	RK		CY	·	29			29	
	610.1403	TOPSOIL - LAWNS					CY	50				50	
	610.1602	TURF ESTABLISHMENT	- LAWNS				SY	448				448	
	610.19	WATERING VEGETATIO	N				MGAL		73	1.1.		73	
	611.0171	PLANTING - MAJOR DE	CIDUOUS TREE - 3 INCH CALIPER	BALL & BURLAP,	FIELD POTTED OR I	FIELD BOXED	EA		76			76	
	611.0442	PLANTING - DECIDUOL	JS SHRUBS - AS SPECIFIED CONTA	AINER OR BOX GR	OWN - 2FT		EΑ		37			37	
	611.0452	PLANTING - DECIDUOUS SHRUBS - AS SPECIFIED CONTAINER OR BOX GROWN - 3FT					EA		36	*		36	
	611.0461	PLANTING - DECIDUOL	IS SHRUBS - AS SPECIFIED CONTA	AINER OR BOX GR	OWN - 4FT		ĒΑ		110			110	
	611.0462	PLANTING - DECIDUOUS SHRUBS - AS SPECIFIED CONTAINER OR BOX GROWN - 4FT							52			52	
	611.0751	PLANTING - HERBACEOUS PLANTS - NUMBER 1 CONTAINER - CONTAINER GROWN EA 1,470								1,470			
NECTIVITY			PIN 8780.49	BRIDGES	CULVERTS	ALL DIMENS	IONS IN	f† UNLESS	OTHERWIS	E NOTED			T NUMBER
REETSCAPE PROJECT			RTE 32		F							DL34	4880

ESTIMATE OF QUANTITIES

645,48020001 STEEL EXHIBIT PEDESTAL

ITEM NUMBER

611.18

611.19

614.060304

614.0701

615.01010110

615.01010210

615.08030005

615.27020010

619.01

619.04

619.0701

619.080201

619.100101

619.100201

619.110511

620.10

621.03

621.04

623.10

624.01

625.01

633.14

627.50140008

634.06010007

634.06030007

634.10010039

634.99010017

635.0103 635.0303

637.34

645.48010001

PORTABLE DRIP IRRIGATION SYSTEM

LITTER (TRASH) RECEPTACLE TYPE 1

LITTER (RECYCLE) RECEPTACLE TYPE 2

BASIC WORK ZONE TRAFFIC CONTROL

TYPE III CONSTRUCTION BARRICADE

REMOVE PAVEMENT MARKING LETTERS OR SYMBOLS, TRAFFIC PAINT

INTERIM PAVEMENT MARKINGS, STRIPES (TRAFFIC PAINT)

INTERIM PAVEMENT MARKINGS, SYMBOLS (TRAFFIC PAINT)

TEMPORARY BUSINESS SIGNS

GALVANIZED GABIONS

SURVEY OPERATIONS

CUTTING PAVEMENT

RELOCATE PARKING METERS

UNIT STAIR ASSEMBLY SYSTEM

REMOVE AND STORE PARKING METERS

OFFICE TECHNO; OGY AND SUPPLIES

HIGH PRESSURE LAMINATE GRAPHIC SIGN

BUILDING CONDITION SURVEY (8 BUILDINGS)

CLEANING CLOSED DRAINAGE SYSTEMS

CRUSHED STONE (IN PLACE MEASURE) MODIFIED

CONVENTIONALLY FORMED OR MACHINE FORMED GUTTERS

REMOVAL AND REPAIR OF DETERIORATED HMA PAVEMENT

CLEANING AND PREPERATION OF PAVEMENT SURFACES - LINES

CLEANING AND PREPERATION OF PAVEMENT SURFACES - SYMBOLS

CLEANING DRAINAGE STRUCTURES

POST-PLANTING CARE

BENCH TYPE 01

BICYCLE RACK

REMOVAL OF PORTABLE DRIP IRRIGATION SYSTEM

ENGINEERING SHARE 1 - ROADWAY AND SIDEWALK IMPROVEMENTS (NYSDOT AND NYSDEC)
ENGINEERING SHARE 2 - GREEN INFRASTRUCTURE (NYSEFC)
ENGINEERING SHARE 3 - BROADWAY TRAFFIC SIGNALIZATION WITHIN PROJECT LIMITS
ENGINEERING SHARE 4 - BROADWAY TRAFFIC SIGNALIZATION (NYSERDA FUNDING)

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LF.

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7,357

312

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63

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GREENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

	COUNTY: ULSTER	REGION: 8		
T IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING L O ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A L HALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	ICENSED PROFESSIONAL IS ALTERED, THE ALTERING E	ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT,	OR LAND SURVEYOR	

					COL	JNTY: ULS	TER	_	REGION: 8									SHEE	T NO. 10	4
					CII	Y OF KING	25 I UN						[ESTIMAT	E QUANT	TITIES			ING NO.	
						REETSCAPE F	PROJECT		NIC 32								DL34880			
AS-BUILT REVISIONS DESCRIPTION OF ALTERATION	S:						NNECTIVITY			PIN 8780.49 RTE 32	BRIDGES	CULVERTS	ALL DIMENS	IONS IN f	t UNLESS	OTHERWISE	NOTED		CONTRACT	
	EΑ		2:			2		670.53070304	POWER POINT DISCO	NNECT ASSEMBLY - LOCATION (;			EА	1				1	
	EΑ		2			2		670.53070204	POWER POINT DISCO	NNECT ASSEMBLY - LOCATION E	3			EА	1				1	
	DC	5,000				5,000		670.53070104	POWER POINT DISCO	NNECT ASSEMBLY - LOCATION A	4			EА	1				1	
	ŧΑ	23				23		670.3010	670.3010 PULL BOXES 7.6 CUBIC FEET TO 10 CUBIC FEET, INSIDE VOLUME(LIGHTING)					EА	9				9	
	ίε	23,000				23,000		670.3001	PULL BOXES LESS THA	N 5 CUBIC FEET, INSIDE VOLUM	E(LIGHTING)			EА	40				40	
	1.5	1				1		670.2602	RIGID PLASTIC CONDI	JIT, 2"				L.F	6,100				6,100	

۱F 5,397 5,397 ŧΑ 96 96 CY 511 511 SF 1,662 1,662

2 2 CY 134 134

10,000 10,000 40 40 (PVMS) STANDARD SIZE - FULL MATRIX (LED) NO OPTIONAL EQUIPMENT SPECIFIED, NO CELLULAR COMN

SF 200 ŧΑ 2 2

ŧΑ 45 45 200

ŧΑ 22 22 ŧΑ 28 28 LS.

£Α 23 23 ŧΑ 23 23

ŧΑ 14

ŧΑ TREE REMOVALIOVER 12 INCHES TO 18 INCHES DIAMETER BREAST HEIGHT - STUMPS GRUBBED. 72

ESTIMATE OF QUANTITIES

ITEM DESCRIPTION

PRE-EXISTING STUMP REMOVAL UP TO 24 INCH DIAMETER AT 6 INCHES ABOVE GRADE

£Α 78

£Α 78 78 78 72

UNIT SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL

£Α

AS-BUILT QUANTITY

14

645.830102

647.31

647.41

647.51

1

7,357

312

63

GROUND MOUNTED SIGN PANELS WITH Z BARS - HIGH VIS SHEETING 545.5202 645.81 645.81020003

647.61

655.0704

655.0705

655.1103

655.1106

655.16000011

655.25010005

660,65000101

660.65000201

660.65000303

660.65000401

660.65000501

660.65000601

665.20040004

670.15100108

663.1301

663.33

670.0106

645.5102

ITEM NUMBER

TYPE A SIGN POSTS RETROREFLECTIVE SIGN POST STRIP

GROUND MOUNTED SIGN PANELS WITHOUT Z BARS

GROUND MOUNTED SIGN PANELS WITHOUT Z BARS - HIGH VIS SHEETING

RELOCATE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE I (UNDER 30 SQUARE FEET)

CAST FRAME F1, UNMOUNTABLE CURB BOX AND RETICULINE GRATE G1

CAST FRAME F2, UNMOUNTABLE CURB BOX AND RETICULINE GRATE G2

FURNISH AND/OR INSTALL INLET ASSEMBLY, REINSTALL EXISTING

WELDED FRAME AND RETICULINE GRATE 3

WELDED FRAME AND RETICULINE GRATE 6

ALTERING UTILITY MANHOLES

REMOVE AND DISPOSE OF FRAMES AND GRATES

ALTERING UTILITY MANHOLES AND VAULTS 8H-578

ALTERING UTILITY MANHOLES AND VAULTS 8H-582

ALTERING UTILITY MANHOLES AND VAULTS 8H-618

ALTERING UTILITY MANHOLES AND VAULTS 8H-666

ALTERING UTILITY MANHOLES AND VAULTS 8H-670

FOUNDATION FOR LIGHT STANDARDS, 6 FEET LONG.

FURNISH AND INSTALL DECORATIVE STREET LIGHT POLE AND LUMINAIRE ASSEMBLIES TYPE-1

ADJUST EXISTING VALVE BOX ELEVATION

ALUMINUM ACCESS DOORS

REMOVE AND DISPOSE SIGN PANEL. SIGN PANEL ASSEMBLY SIZE I(UNDER 30 SQUARE FEET)

REMOVE AND DISPOSE GROUND MOUNTED TYPE A SIGN SUPPORT(S) FDNS AND ANY ATTACHED SIGNS

TYPE B SIGN POST, GALVANIZED \$3X5.7 SECTION, BI-DIRECTIONAL BREAKAWAY BASE

ITEM DESCRIPTION

ESTIMATE OF QUANTITIES

ĘΑ 20 EΑ 11 EΑ 31 EΑ 18 REMOVE AND STORE SIGN PANEL. SIGN PANEL ASSEMBLY SIZE I (UNDER 30 SQUARE FEET)

138

81

165

UNIT

SF

SF

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11 31 18

SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL

25 25

AS-BUILT

OUANTITY

138

81

165

20

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18 18 27 27 50 50

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CITY OF KINGSTON

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JPH_E00-3.
\CADD\878@49_C /22/2@19 yce
=\CADD = 11/22/2 = tvyce'
TLE NAME NATE/TIME USER
E8 +

ITEM NUMBER

SINGLE CONDUCTOR CABLE #2 AWG

SINGLE CONDUCTOR CABLE #6 AWG

SINGLE CONDUCTOR CABLE #10 AWG

MODIFY LIGHTING INSTALLATION, TYPE 1 (REMOVE LIGHT POLE)

MODIFY LIGHTING INSTALLATION, TYPE 3 (BILLBOARD CONNECTION)

PULL BOX-RECTANGULAR, 26 X 18 INCH, REINFORCED CONCRETE

WIRELESS VEHICLE DETECTION SYSTEM INTERSECTION SENSOR

WIRELESS VEHICLE DETECTION SYSTEM ISOLATOR MODULE (ISO)

FLUTED TRAFFIC SIGNAL POLE - TOP MOUNT - PAINTED, 8 FEET LENGTH

FLUTED TRAFFIC SIGNAL POLE - TOP MOUNT - PAINTED, 10 FEET LENGTH

MODIFY AND REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 1

MODIFY AND REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 2

MODIFY AND REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 3

REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 1

MICROWAVE VEHICLE DETECTOR WITH 12 VOLT AC POWER SUPPLY DETECTOR CARD

WIRELESS VEHICLE DETECTION SYSTEM CONTACT CLOSURE INTERFACE (APCC)

WIRELESS VEHICLE DETECTION SYSTEM EXTENSION CONTACT CLOSURE CARD

WIRELESS VEHICLE DETECTION SYSTEM SERIAL PORT PROTOCOL DIGITAL RADIO (SPP)

FLUTED TRAFFIC SIGNAL POLE, MAST ARM, 18 FEET MOUNTING HEIGHT, 46 FEET MAST ARM LENGTH FLUTED TRAFFIC SIGNAL POLE, MAST ARM, 18 FEET MOUNTING HEIGHT, 48 FEET MAST ARM LENGTH

FLUTED TRAFFIC SIGNAL POLE, MAST ARM, 18 FEET MOUNTING HEIGHT,50 FEET MAST ARM LENGTH

MODIFY LIGHTING INSTALLATION, TYPE 2 (CIRCUIT TESTING)

DECORATIVE WRAP FOR TRAFFIC SIGNAL CABINETS

POLE EXCAVATION AND CONCRETE FOUNDATION

CONCRETE BASE FOR CONTROLLER CABINET

CONDUIT, METAL STEEL, ZINC COATED, 1"

CONDUIT, METAL STEEL, ZINC COATED, 2'

CONDUIT, METAL STEEL, ZINC COATED, 3"

GROUND WIRE #6 AWG

670.7002

670.7006

670,7501

670.77000105

670.77000205

670,77000305

680.06010006 680.5001

680,5002

680.510501

680.520103

680.520106

680.58010009

680.58803008 680.58804008

680.58805008

680.58806008

680.58807008

680.62184609

680.62185009

680.65100409

680,700606

680.730514

680.731014

680.78100108

680.78100208

680.78100308

680,79010008

680,79020008 REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 2 ENGINEERING SHARE 1 - ROADWAY AND SIDEWALK IMPROVEMENTS (NYSDOT AND NYSDEC)
ENGINEERING SHARE 2 - GREEN IMFRASTRUCTURE (NYSEFC)
ENGINEERING SHARE 3 - BROADWAY TRAFFIC SIGNALIZATION WITHIN PROJECT LIMITS
ENGINEERING SHARE 4 - BROADWAY TRAFFIC SIGNALIZATION (NYSERDA FUNDING)

RISER ASSEMBLY, 2 INCH DIAMETER

SIGNAL CABLE 5 CONDUCTORS, 14 AWG

SIGNAL CABLE 10 CONDUCTORS, 14 AWG

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

ESTIMATE OF QUANTITIES

ITEM DESCRIPTION

UNIT

LF

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LF

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1.5

1.5

33,800

4,600

6,800

44

2

143

SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL

26

12

67

6450

1700

KINGSTON CONNECTIVITY RTE 32 BROADWAY STREETSCAPE PROJECT

AS-BURLT

QUANTITY

33,800

4,600

6,800

44

143

26

12

350

220

67

6.450

2,500

800

ITEM NUMBER

680.79030008

680.810101

680.

680

680

680.

680.

680

680

680.

680

680

680.

ESTIMATE QUANTITIES

GREENMAN-PEDERSEN, INC

CONSULTING ENGINEERS

DL34880 DRAWING NO. EOQ-3

CITY OF KINGSTON

AS-BUILT

OUANTITY

22

SHEET NO. 105

CITY OF KINGSTON		
COUNTY: ULSTER	REGION: 8	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICE TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTER	RED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND	SURVEYOR
SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE,	THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALT	ERATION.

		PIN 8780.49	BRIDGES	CULVERTS	ALL DIMENSI	ONS IN f	UNLESS	OTHERWISE	NOTED		CONTRACT	NUMBER
0.95010615	SERVICE CABLE 1 CON	IDUCTOR, NO. 06 AWG				LF			800		800	
0.94000015	RAINTIGHT DISCONN	ECT BOX				EА			3		3	
0.94000008	TRAFFIC SIGNAL SERV	VICE ENTRANCE				EΑ			3		3	
0.93500301	REMOTE ANTENNA CA	ABLE FOR SPREAD SPECTRUM WI	RELESS TRAFFIC (CONTROL		Ļ.F				300	300	
0.93500201	DIRECTIONAL YAGI AI	NTENNA FOR SPREAD SPECTRUM	1 WIRELESS TRAFF	IC CONTROL		EА				6	6	
0.93500101	SPREAD SPECTRUM W	/IRELESS TRAFFIC CONTROL				EА				6	6	
0.93200101	NEMA 152 TYPE A2 CC	DNTROLLER - BUS PRIORITY CAPA	ABILITY			EА				3	3	
0.90920008	ELECTRIC METER SOCI	KET, 200 AMP, SINGLE PHASE, 24	0/120 VOLT W/B	YPASS SWITCH FO	R SIGNAL INSTA	EА			6		6	
0.82254310	RECTANGULAR RAPID	FLASHING BEACON (RRFB) ASSE	EMBLY - FOUR BEA	ACONS, SOLAR PO)WERED	EΑ			6		6	
0.8207	OVERHEAD SIGN ASSI	EMBLY, TYPE G			1	EА			8		8	
0.81990003	TRAFFIC SIGNAL BACK	KPLATES WITH YELLOW REFLECTI	VE TAPE			EΑ			27		27	
0.81500010	PEDESTRIAN COUNT-	DOWN TIMER MODULE				EА			24	22	46	
0.8142	PEDESTRIAN SIGNAL I	POST TOP MOUNT ASSEMBLY				EΑ			9	12	21	
0.8141	PEDESTRIAN SIGNAL BRACKET MOUNT ASSEMBLY					EΑ			15	10	25	
0.81330010	AUDIBLE PEDESTRIAN SIGNAL								24	22	46	
0.813106	PEDESTRIAN SIGNAL SECTION - POLYCARBONATE, TYPE I, 12 INCH								48	44	92	
0.813105	PEDESTRIAN SIGNAL I	MODULE - 12 INCH BI-MODAL, HA	AND/MAN SYMBO	DLS LED		EА			24	22	46	
0.8112	TRAFFIC SIGNAL BRAC	CKET ASSEMBLY - 2 WAY				ĘΑ			5		5	
0.8111	TRAFFIC SIGNAL BRAC	CKET ASSEMBLY - 1 WAY				ĘΑ			25		25	
0.810701	TRAFFIC SIGNAL SECT	ION - POLYCARBONATE, TYPE I,	8 INCH			ĘΑ			30		30	
0.810601	TRAFFIC SIGNAL SECT	ION - POLYCARBONATE, TYPE I,	12 INCH			EΑ			75		75	
0.810305	TRAFFIC SIGNAL MOD	DULE - 8 INCH, GREEN BICYCLE, LE	:O			EΑ			10		10	
0.810303	TRAFFIC SIGNAL MOD	DULE - 8 INCH, YELLOW BICYCLE, I	.ED			ĘΑ			10		10	
0.810301	TRAFFIC SIGNAL MOD	ULE - 8 INCH, RED BICYCLE, LED				EΑ			10		10	
0.810106	TRAFFIC SIGNAL MOD	DULE - 12 INCH GREEN ARROW, LI	ED			ĘΑ			3		3	
0.810105	TRAFFIC SIGNAL MOD	DULE - 12 INCH, GREEN BALL, LED				ĘΑ			22		2.2	
0.810104	TRAFFIC SIGNAL MOD	DULE - 12 INCH, YELLOW ARROW,	LED			EΑ			3	***************************************	3	
0.810103	TRAFFIC SIGNAL MOD	DULE - 12 INCH, YELLOW BALL, LEI	D			ĘΑ			22		22	
0.810102	TRAFFIC SIGNAL MOD	DULE - 12 INCH, RED ARROW, LED				EΑ			3		3	
	<u> </u>						L		1		L	

ESTIMATE OF QUANTITIES

UNIT

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SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL

22

ITEM DESCRIPTION

NEMA 1S 2 SOLID STATE MENU DRIVEN ACTUATED TRAFFIC SIGNAL CONTROLLER AND CABINET

REMOVE TRAFFIC SIGNAL EQUIPMENT - LOCATION 3

MICROCOMPUTER CABINET BASE (ALUMINUM) TRAFFIC SIGNAL MODULE - 12 INCH, RED BALL, LED

FILE NAME = ...\CADD\878849_CPH_E00-4.dgn
DATE/TIME = 11/22/2819

USER = tvyoe*

DESIGN SUPERVISOR C. CORNWELL

ITEM NUMBER

685.12

685.13

685.14

697.03

698.04

698.05

699.040001

	CITY OF KINGSTON			1
				1
	COUNTY: ULSTER	REGION: 8		
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UT OF ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LIGHTLAND SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY"	ICENSED PROFESSIONAL IS ALTERED, THE ALT	TERING ENGINEER, ARCHI	TECT, LANDSCAPE ARCHITECT, OF	R LAND SURVEYOR [*]

GPERENMAN-PEDERSEN, INC. CONSULTING ENGINEERS

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED

ESTIMATE QUANTITIES

BRIDGES

CULVERTS

SHEET NO. 106 CITY OF KINGSTON

CONTRACT NUMBER

DL34880

DRAWING NO. EOQ-4

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	KINGSTON CONNECTIVITY	PIN 8780.49
	BROADWAY STREETSCAPE PROJECT	RTE 32
	CITY OF KINGSTON	
	COUNTY: ULSTER REGION	: 8
	E ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER,	

ESTIMATE OF QUANTITIES

UNIT

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15

13,998

46

128

8,712

1,756

1

195,000 60,000

SHARE 1 SHARE 2 SHARE 3 SHARE 4 TOTAL

30,000 5,000

QUANTITY

13,998

46

128

290,000

8,712

1,756

1

ITEM DESCRIPTION

WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS

YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS

WHITE EPOXY REFLECTORIZED PAVEMENT LETTERS - 20 MILS

WHITE EPOXY REFLECTORIZED PAVEMENT SYMBOLS - 20 MILS

ENGINEERING SHARE 1 - ROADWAY AND SIDEWALK IMPROVEMENTS (NYSDOT AND NYSDEC)
ENGINEERING SHARE 2 - GREEN INFRASTRUCTURE (NYSEFC)
ENGINEERING SHARE 3 - BROADWAY TRAFFIC SIGNALIZATION WITHIN PROJECT LIMITS
ENGINEERING SHARE 4 - BROADWAY TRAFFIC SIGNALIZATION (NYSERDA FUNDING)

ASPHALT PRICE ADJUSTMENT (LUMP SUM) [CALCULATED IN ESTIMATOR]

FIELD CHANGE PAYMENT (FCP)

FUEL PRICE ADJUSTMENT

MOBILIZATION 4%